



## DEVELOPMENT APPLICATION

Development Permit - Material Change of Use

# Dual Occupancy

**50 Railway Street, Pittsworth**

PLANNING REPORT

June 2026

TOOWOOMBA | IPSWICH

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Prepared For: Broadacres Super Pty Ltd

Project Name: Dual Occupancy

Project Reference: 2025-364

Date: 29 June 2026

## Document History

Version	Date	Description of Revision	Prepared By	Reviewed By
Draft	25/06/2026	Draft for Client Review	JG	JG
Final	29/06/2026	Lodgement with Council	JG	JG

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## EXECUTIVE SUMMARY

### SITE DETAILS

<b>Address of Site</b>	50 Railway Street, Pittsworth
<b>Real Property Description</b>	Lot 1 RP906054
<b>Area of Site</b>	623m <sup>2</sup>
<b>Road Frontage(s)</b>	Primary: Railway Street Secondary: Short Street
<b>Easements</b>	N/A
<b>Registered Owners</b>	Broadacres Super Pty Ltd

### PLANNING INSTRUMENTS

<b>Regional Plan</b>	Darling Downs Regional Plan
<b>Regional Plan Designation</b>	Priority Living Area
<b>Planning Scheme</b>	<i>Toowoomba Regional Planning Scheme 2012</i>
<b>Zone</b>	Major Centre Zone
<b>Overlays</b>	N/A
<b>Vegetation</b>	N/A
<b>Existing Use</b>	Vacant
<b>Current Approvals</b>	Nil (as available on Council's Development.i)

### APPLICATION OVERVIEW

<b>Application</b>	Development Application
<b>Approval Sought</b>	Development Permit
<b>Development Type</b>	Material Change of Use
<b>Land Use Definition</b>	Dual Occupancy
<b>Level of Assessment</b>	Impact Assessable

<b>Relevant Referral/Technical Agencies</b>	Department of Transport and Main Roads
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## PROPOSED DEVELOPMENT

<b>Development Summary</b>	Dual Occupancy
<b>Gross Floor Area</b>	Unit 1: 167.2m <sup>2</sup> Unit 2: 170.0m <sup>2</sup>  Total: 337.2m <sup>2</sup>
<b>Building Footprint</b>	287.6m <sup>2</sup>
<b>Building Site Cover</b>	46%
<b>Building Setbacks</b>	N: 3.0m E: 2.32m S: 1.5m W: 3.0m
<b>Building Height</b>	Metres: 6.93 Storeys: Two (2)
<b>Impervious Area</b>	371m <sup>2</sup> (59%)
<b>Landscaping Area</b>	251m <sup>2</sup> (40%)
<b>Vehicular Access</b>	Short Street
<b>Carparking</b>	Two spaces per unit

## APPLICANT DETAILS

<b>Applicant</b>	Broadacres Super Pty Ltd c/- Precinct Urban Planning PO Box 3038 <b>TOOWOOMBA QLD 4350</b>
<b>Contact Person</b>	<b>Jess Garratt</b> <b>Phone:</b> 07 4632 2535 <b>Mobile:</b> 0413 276 552 <b>Email:</b> <a href="mailto:jess@precinctplan.com.au">jess@precinctplan.com.au</a>
<b>Our Reference</b>	2025-364

## 1.0 INTRODUCTION

This report has been prepared in support of a Development Application for a for Material Change of Use for a Dual Occupancy on land at 50 Railway Street, Pittsworth, described as Lot 1 RP906054 (**the site**). The proposed development involves the start of a new use of the premises and accordingly comprises a Material Change of Use under Schedule 2 of the *Planning Act 2016* (**the Act**).

This town planning report addresses the merits of the development having regard to the provisions of the *Planning Act 2016* and the *Toowoomba Regional Planning Scheme 2012*. This planning report has been prepared on behalf of the applicant, Broadacres Super Pty Ltd.

This Development Application for a Material Change of Use is Impact Assessable development under the *Toowoomba Regional Planning Scheme 2012*. The following report demonstrates the suitability of the proposal within the locality and its compliance with the relevant provisions of the *Toowoomba Regional Planning Scheme 2012*.

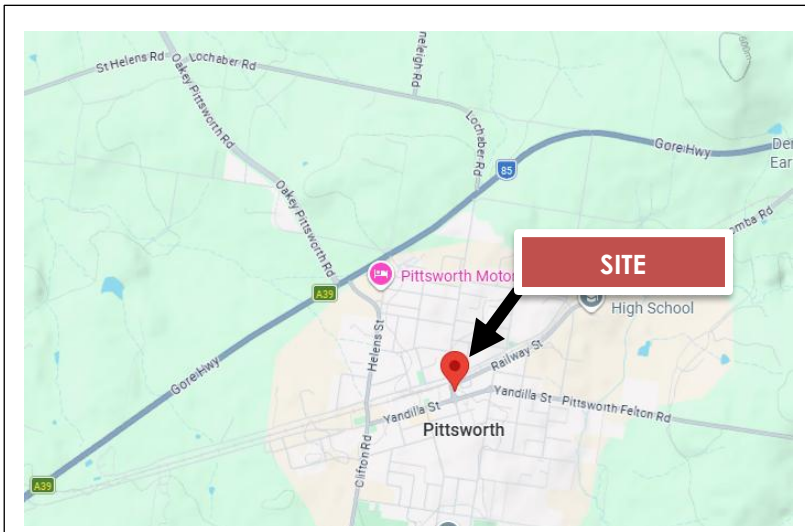
## 2.0 SITE AND LOCALITY

### 2.1 SITE

The site is located at 50 Railway Street, Pittsworth, described as Lot 1 RP906054 (**the site**). The site is located within the suburb of Pittsworth and is within the Pittsworth Town Centre. The location of the site in respect of the surrounding locality is illustrated in **Figure 1**.

The site is of rectangular configuration comprising a single title with a total site area of 623m<sup>2</sup>. The site has primary frontage to Railway Street and secondary frontage to Short Street. The configuration of the site is illustrated in **Figure 2**.

**FIGURE 1 - LOCALITY PLAN**



**FIGURE 2 - CADASTRAL PLAN**



There are no improvements on the site. The features of the site and surrounding land uses are illustrated in **Figure 3**.

**FIGURE 3 - AERIAL IMAGERY**



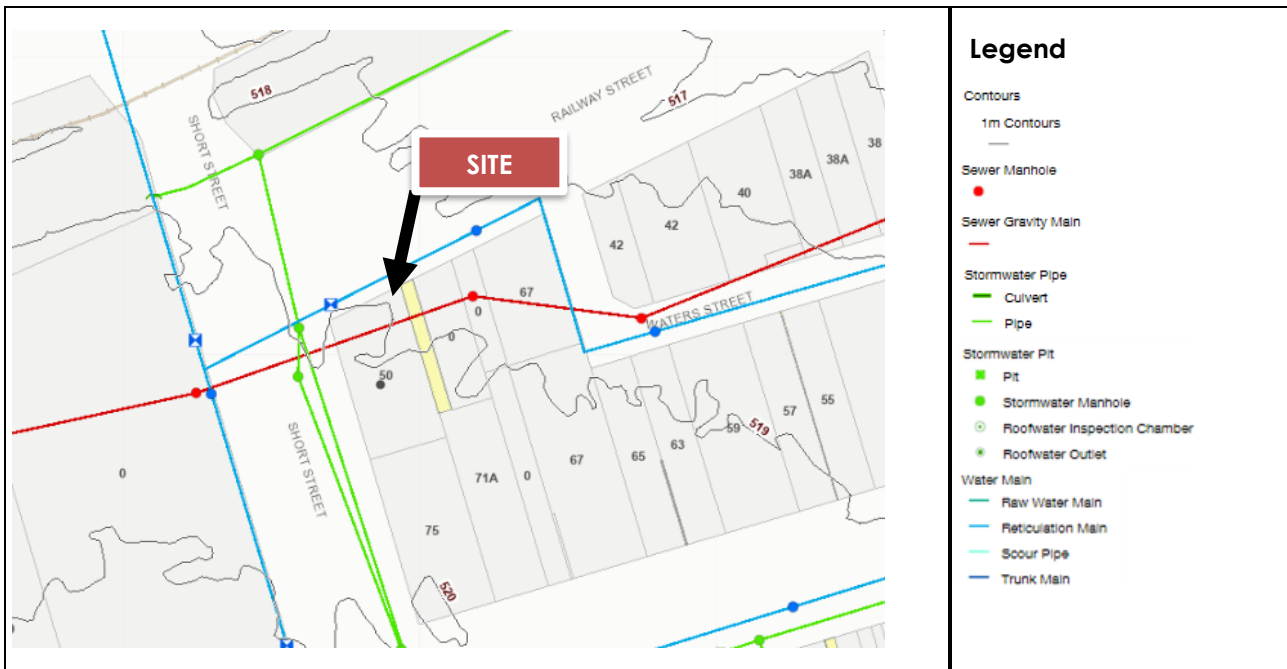
## 2.2 SITE CHARACTERISTICS

The site is characterised by the following features:

- The site has primary frontage to Railway Street and secondary frontage to Short Street.
  - Railway Street comprises a two-lane, unmarked bitumen sealed carriageway with no kerb and channel on either side of the roadway. Railway Street is identified as a Local Road in Council's Road Hierarchy.
  - Short Street comprises a two-lane, line marked bitumen sealed carriageway with kerb and channel and sealed footpaths provided on either side of the roadway. On street parking is provided along the eastern side of the roadway. Short Street is identified as a Collector Road in Council's Road Hierarchy.
- Lands in the locality drain in a south-easterly direction towards Perrier Gully. Perrier Gully is located approximately 1km east of the site and comprises the area's primary drainage feature.

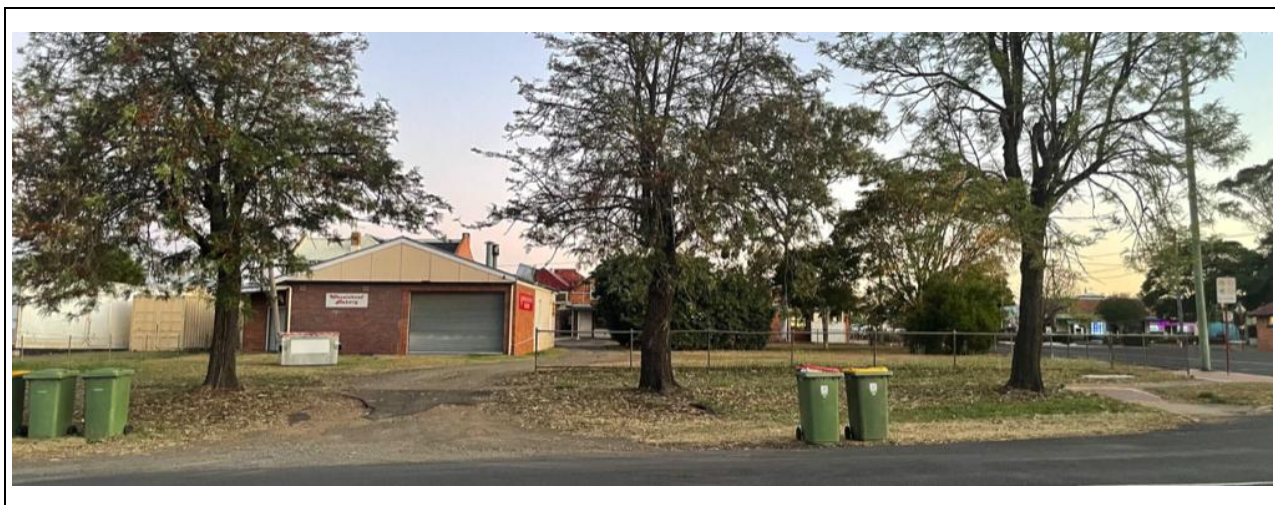
- The site experiences minimal variation in land elevation across the allotment; refer to **Figure 4**. The site contains a single contour line of 519m AHD. This translates to very negligible fall in natural ground levels across the allotment.
- The site is predominately clear of mature vegetation and does not contain mapped areas of remnant vegetation or ecological significance.
- The site is not burdened by, nor does it benefit from any existing easements (including the easement that adjoins the eastern boundary of the site).
- The site is serviced by Council's reticulated water supply, sewerage and stormwater drainage infrastructure; refer to **Figure 4**. The site is also serviced by reticulated electricity and telecommunications supply networks.

**FIGURE 4 - CONTOUR AND INFRASTRUCTURE MAPPING**

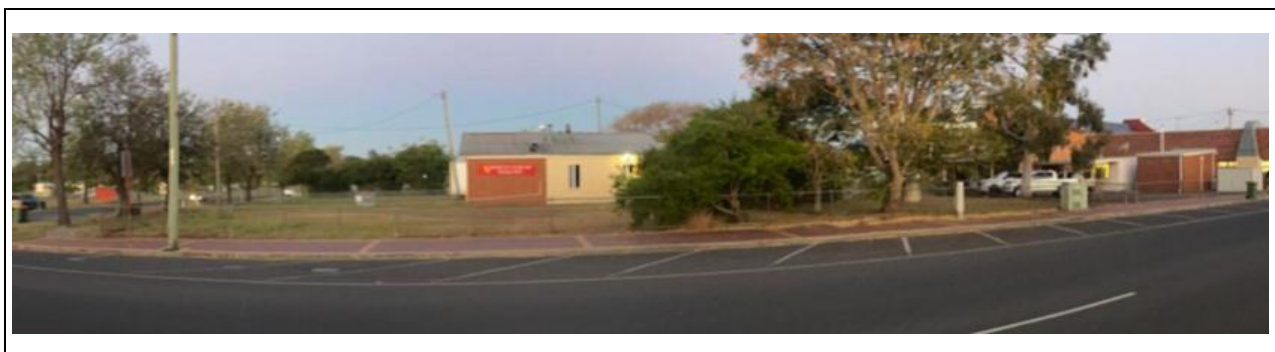


**Figure 4** confirms that the site experiences a negligible fall in elevation across the site. The figure also illustrates the locations of Council's reticulated water, sewerage and stormwater networks in relation to the site.

The features of the site are illustrated in **Photographs 1-2**.



PHOTOGRAPH 1 - View of the site looking south from Railway Street.



PHOTOGRAPH 2 - View of the site looking east from Short Street.

## 2.3 SURROUNDING LAND USE

### 2.3.1 SURROUNDING LOCALITY

The site is situated in a locality that is characterised by commercial land uses. The scale and character of the built form in the locality is of a predominately commercial character.

### 2.3.2 ADJOINING PROPERTIES

The nature and character of land use on surrounding properties may be summarised as follows:

- Land to the **north** of the site, located on the opposite side of Railway Street, comprises railway land containing the South Western Railway System (Millmerran Branch).



**PHOTOGRAPH 3** - View of the railway land to the north of the site from Railway Street.

- Land to the **east** of the site, located at 71A Yandilla Street, comprises a commercial premises containing a bakery.



**PHOTOGRAPH 4** - View of the commercial premises to the east of the site from Railway Street.

- Land to the **south** of the site, located at 75 Yandilla Street, comprises a commercial premises containing a commercial premises.

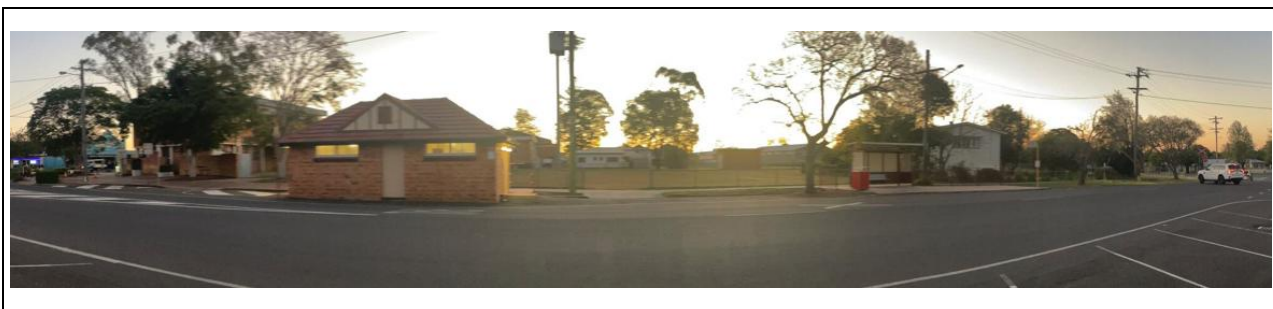


**PHOTOGRAPH 5** - View of the commercial premises to the south of the site from Short Street.



**PHOTOGRAPH 6** - View of the commercial premises to the south of the site from Yandilla Street.

- Land to the **west** of the site, on the opposite side of Short Street, comprises a commercial premises containing the Pittsworth Police Station, Pittsworth Magistrates Court and public toilets located within the road reserve.



**PHOTOGRAPH 7** - View of the commercial premises and public toilets to the west of the site from Short Street.

## **2.4 CURRENT DEVELOPMENT APPROVALS**

The site does not currently benefit for any existing Development Permits as publicly available on Council's website.

## 3.0 PROPOSAL

This Development Application seeks approval for a Material Change of Use – Dual Occupancy on land at 50 Railway Street, Pittsworth, being that land described as Lot 1 RP906054. The proposal seeks to construct a dual occupancy consisting of two detached units on the subject site.

Details regarding the proposed design and operation of the development are provided below.

### 3.1 SITE COMPOSITION AND LAYOUT

The site is currently unimproved, with exception of chain wire mesh boundary fencing along the northern, southern and western boundaries of the site. The proposed development involves the removal of all existing vegetation from the site comprising four (4) non-native shrubs and trees and one (1) native tree and construction of a two-storey Dual Occupancy consisting of two (2) detached units.

Due to the site's Major Centre Zone location, initially the applicant sought to establish a Multiple Dwelling on the site, consistent with the Planning Scheme's desire for increased residential densities in close proximity to employment and services. However, due to the site's constrained frontages, the density was reduced to respond to the inability to accommodate safe or suitable access and parking for additional dwellings.

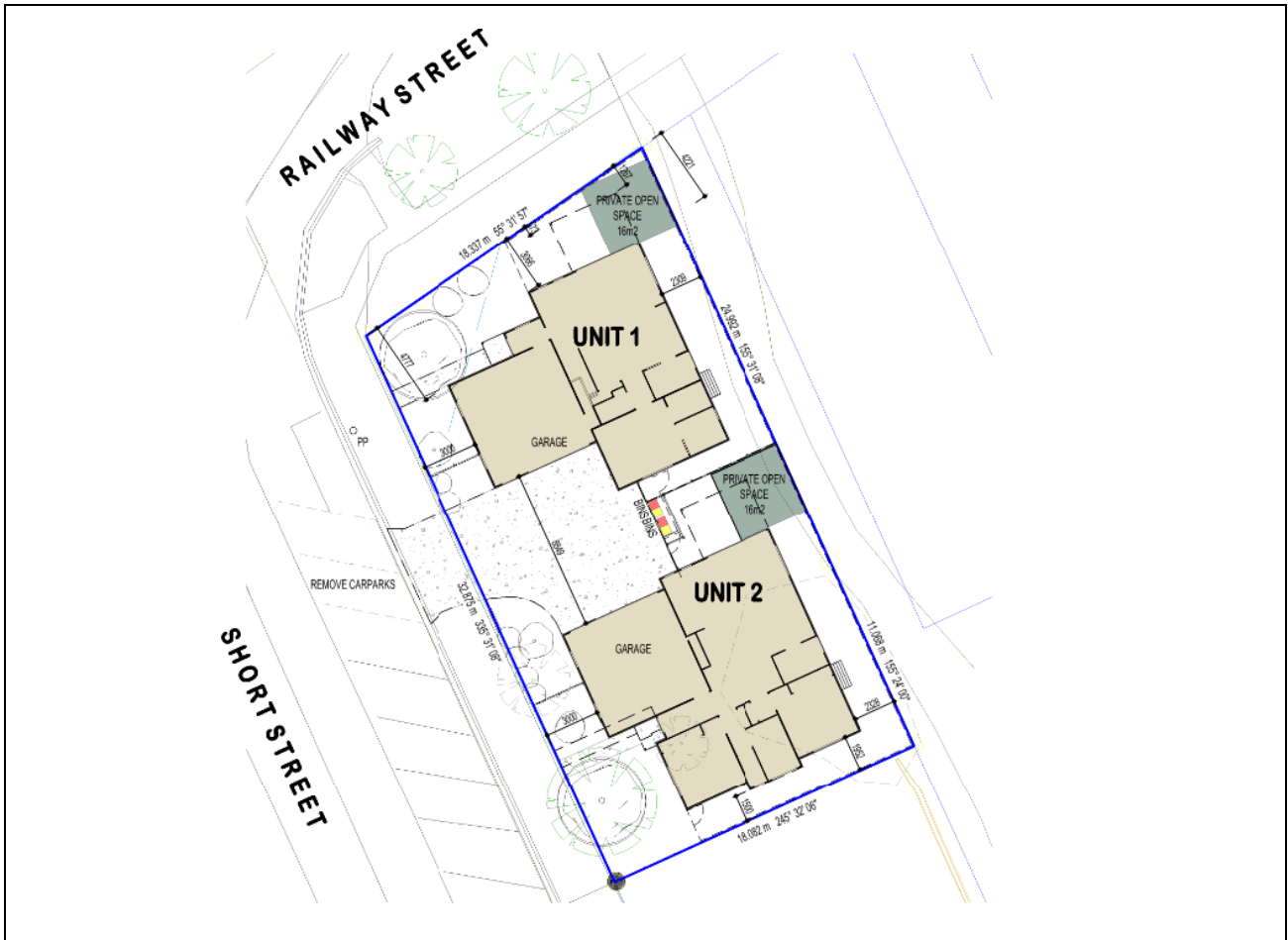
As such, based on advice received during the preliminary design stage, the resulting Dual Occupancy will have a building footprint of 287.6m<sup>2</sup> which translates to a site cover of 46.2% with an impervious area of 371.1m<sup>2</sup> which translates to 59.66% of the site. Both units will take access via a new 5m wide crossover to Short Street. Access to Short Street (in preference to Railway Street) was confirmed with Council via email correspondence in the preliminary design phase. Refer to additional comments in section 3.3.

Building setback to site boundaries will be in accordance with the following:

- North (primary frontage): 3.0m;
- East (side): 2.32m;
- South (rear): 1.5m; and
- West (secondary frontage): 3.0m.

The proposed site layout will be generally in accordance with the Site Plan, Sheet No. A.001, Revision B, prepared by Building Design Professionals, dated 17 June 2026, attached at **Appendix D**, with an excerpt of the plan reproduced in **Figure 5**.

FIGURE 5 - PROPOSED SITE PLAN



### 3.2 BUILT FORM AND CHARACTER

Each of the proposed units will consist of four (4) bedrooms, three (3) bathrooms (including ensuite for the master bedroom), amenities, laundry and open plan kitchen, living and dining area. Each unit will also be provided with a private open space area which is accessed directly from the living area.

Unit 1 will have a Gross Floor Area (GFA) of 167.2m<sup>2</sup> with Unit 2 having a GFA of 170.0m<sup>2</sup>. This equates to a total GFA of 337.2m<sup>2</sup>.

The internal layout of the proposed development will be generally in accordance with Proposed Floor Plan and Proposed Upper Floor Plan, Sheet Nos. A.100 and A.101 respectively, Revision B, prepared by Building Design Professionals, dated 17 June 2026, attached at **Appendix D**, and an excerpt of the plan is reproduced in **Figures 6-7**.

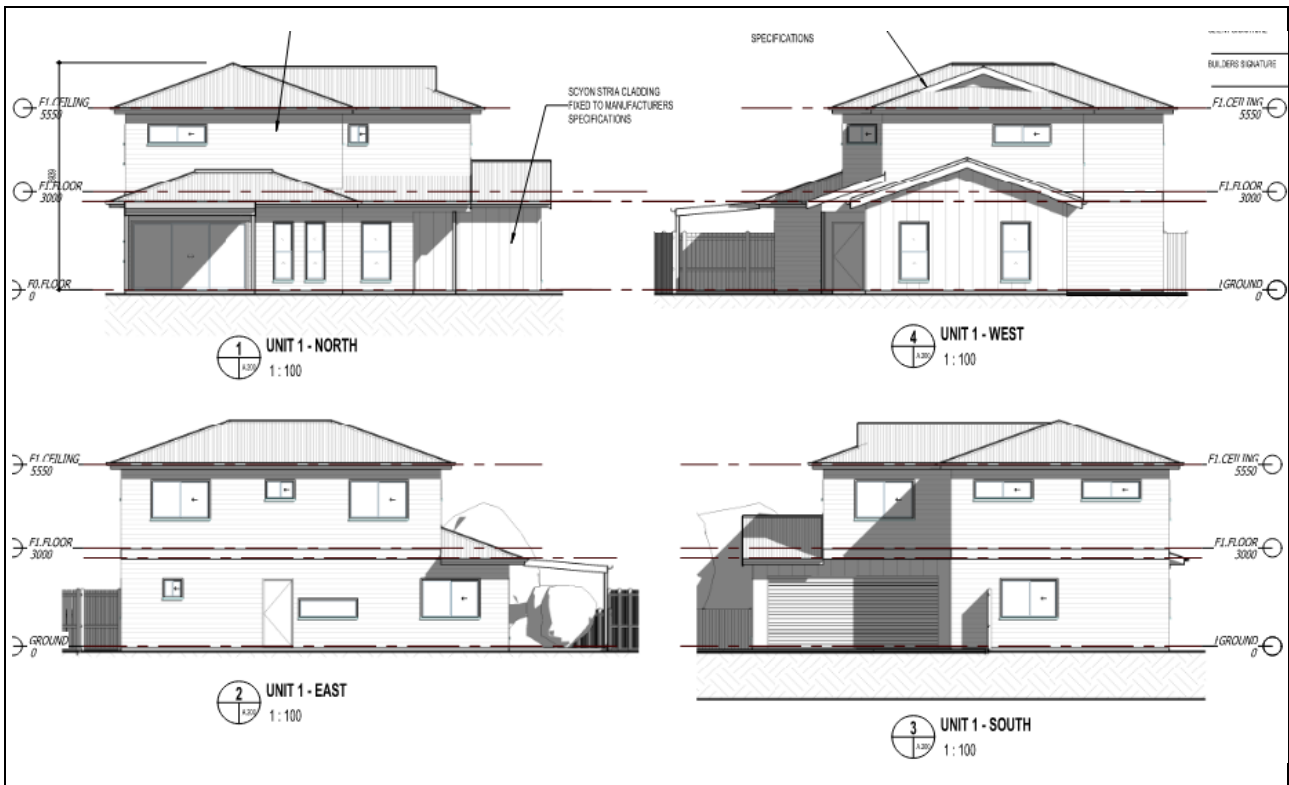


The building design also includes porches to both units and gabling to create visual interest at the street level and provide legibility. Unit 2 will take access off Short Street, with a north-facing courtyard at the rear. Unit 1 will have a north-facing courtyard to Railway Street and will take pedestrian access off Short Street.

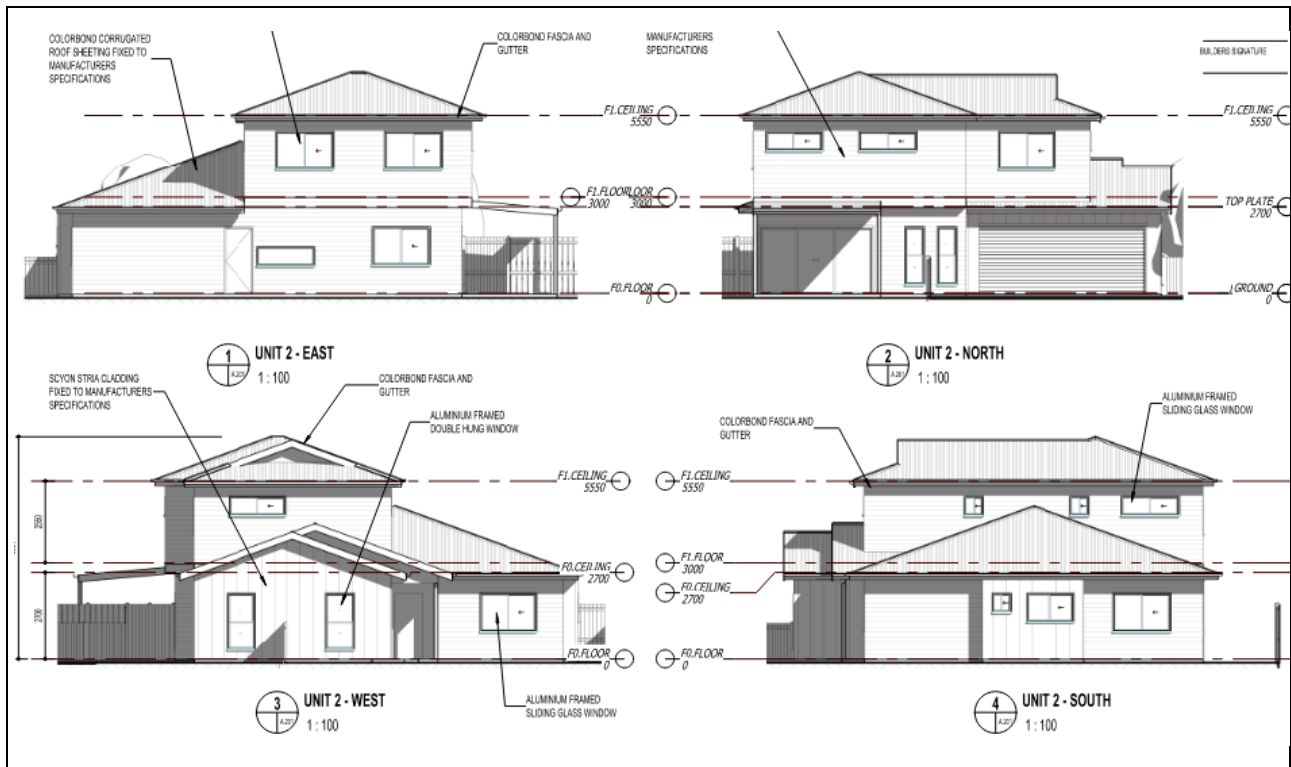
The proposed buildings will be two (2) storeys and will have a maximum height of 6.93m from natural ground level.

The external appearance of the proposed development will be generally in accordance with Proposed Elevations, Sheet No. A.200 and A.201, Revision B, prepared by Building Design Professionals, dated 17 June 2026, attached at **Appendix D**, and an excerpt of the plan is reproduced in **Figure 8-9**.

**FIGURE 8 - PROPOSED ELEVATION PLANS – UNIT 1**



**FIGURE 9 - PROPOSED ELEVATION PLANS – UNIT 2**



### 3.3 ACCESS & CARPARKING

Primary vehicular access to the site will be provided via a new 5m wide shared driveway crossover to Short Street. The proposed crossover location has been designed to respond to the site’s constrained frontage and the established street trees on Railway Street. Consultation with Council’s Engineers during the Prelodgement phase confirmed that there was insufficient separation from the Railway and Short Streets intersection for a driveway crossover to be placed within the site’s Railway Street frontage. There is an access easement on the adjoining property to the east, however despite seeking access from the adjoining owner, the current owner of the land has declined in writing to give permission for the applicant to use this easement to access the subject site.

In addition to the inadequate separation to the intersection there are significant established street trees located within the Railway Street frontage that would require removal or may be detrimentally impacted to facilitate a driveway crossover on the Railway Street frontage.

The proposed driveway crossover has been located on Short Street consistent with advice from Council’s Road Operations team. The crossover location has been chosen to provide adequate separation to the intersection and to limit removal of the marked on-street parking bays. The driveway crossover has been located as centrally within the frontage as possible, whilst avoiding the existing power pole (refer to Photograph 2 and the Site Plan at **Appendix D**) and only removing the carparks necessary to achieve safe site distances for egress movements.

The swept path diagrams and sight line assessments attached at **Appendix F** confirm vehicles utilising the driveway can compliantly enter and exit the site and enclosed garages.

Unit 1 and Unit 2 are provided with a double garage which will provide enclosed parking for two vehicles. Accordingly, the proposed development makes provision for a total of four (4) carparking spaces.

### **3.4 LANDSCAPING**

Landscaping will be provided throughout the site to provide visual screening and enhance the microclimate within the site.

No landscaping plan has been provided as part of this application. Documentation prepared by a relevant professional will be provided following approval of the proposed development.

### **3.5 INFRASTRUCTURE AND SERVICING**

The proposed development will be connected to all available urban infrastructure networks including Council's reticulated water supply and sewerage network, electricity and telecommunications services infrastructure. The land drains to the existing table drain in Railway Street and a stormwater management plan and commentary has been provided at **Appendix E**.

## 4.0 PLANNING FRAMEWORK

### 4.1 THE PLANNING ACT 2016

The purpose of the *Planning Act 2016* (PA) is to establish an efficient, effective, transparent, integrated, coordinated, and accountable system of land use planning (planning), development assessment and related matters that facilitates the achievement of ecological sustainability.

The proposed development involves the start of a new use of the premises and accordingly comprises a Material Change of Use under Schedule 2 of the *Planning Act 2016* (**the Act**). A Material Change of Use must be obtained prior to the commencement of the use of premises. Assessment against the *Toowoomba Regional Planning Scheme 2012* indicates that this development application is Impact Assessable in accordance with Section 45(5) of the Act.

### 4.2 STATE PLANNING POLICY

Pursuant to the provisions of section 45(5) of the *Planning Act 2016*, the Assessment Manager, in considering an impact assessable development application, must assess the application in respect of the State Planning Policy.

The *State Planning Policy July 2017 (SPP)* commenced on 3 July 2017 and replaces *State Planning Policy April 2016*. The SPP expresses the state's interests in land use planning and development and promotes these interests through plan making and development decisions of state and local government. The SPP applies, to the extent relevant, when:

- making or amending a local planning instrument;
- designating premises for infrastructure;
- making or amending a regional plan;
- development assessment by local government, if its planning scheme has not yet appropriately integrated the relevant SPP state interests policies; and
- an assessment manager or referral agency other than local government is assessing a development application.

The SPP addresses seventeen (17) state interests categorised under the following themes:

- (i) Liveable communities and housing.
- (ii) Economic growth.
- (iii) Environment and heritage.
- (iv) Safety and Resilience to Hazards.
- (v) Infrastructure.

A state interest is defined under Schedule 2 of the *Planning Act 2016* (the Act):

- (a) an interest that the Minister considers affects an economic or environmental interest of the State or a part of the State; or
- (b) an interest that the Minister considers affects the interest of ensuring this Act's purpose is achieved.

The proposed development has been assessed against the state interests included under *Part E: State Interest Policies and Assessment Benchmarks* of the SPP as follows:

#### 4.2.1 STATE INTEREST POLICIES AND ASSESSMENT BENCHMARKS

Part E of the SPP contains state interest policies and where relevant, the assessment benchmarks for each state interest. The assessment benchmarks for each relevant state interest are listed below. For each of these state interests, Part E of the SPP advises when the assessment benchmarks apply and if so, what matters the application must be assessed against, to the extent the SPP has not been identified in a local planning instrument as being appropriately integrated.

Part 2 of the *Toowoomba Regional Planning Scheme 2012* identifies the State interests under the State Planning Policy that have been integrated into the Planning Scheme. In this instance, the *Toowoomba Regional Planning Scheme 2012* confirms that some of the State interests relevant to the Toowoomba Regional Council region have been appropriately incorporated into the Planning Scheme.

The Planning Scheme incorporates the following State interests:

- Safety and Resilience to Hazards (as it relates to flood hazards)

Accordingly, where the proposed development achieves compliance with the assessment benchmarks of the Planning Scheme, the development is considered to comply with the provisions of the State Planning Policy as it relates to the above State interests. The development is therefore also required to be assessed against the assessment benchmarks under the State Planning Policy for the relevant State interests not integrated into the Planning Scheme. An assessment of each of the relevant State interests is provided below.

##### 1. Liveable Communities

These provisions apply to a development application in an urban area involving premises that is, or will be, accessed by common private title, for a material change of use or reconfiguring a lot where the application involves buildings (attached or detached) that are not covered by other legislation or planning provisions mandating fire hydrants.

Where these circumstances apply, the development:

- (i) ensures fire hydrants are installed and located to enable fire services to access water safely, effectively and efficiently;
- (ii) road widths, and construction within the development, are adequate for fire emergency vehicles to gain access to a safe working area close to buildings and near water supplies whether or not on-street parking spaces are occupied; and
- (iii) fire hydrants are suitably identified so that fire services can locate them at all hours.

In this instance, the proposed development does not involve any of the above circumstances, and accordingly, these provisions are not relevant to the assessment of this application.

##### 2. Mining and Extractive Resources

These provisions relate to the following developments:

- (1) Reconfiguring a Lot within a Key Resource Area (**KRA**);
- (2) A Material Change of Use within the resource/processing area of a KRA or the separation area for the resource/processing area of a KRA; or
- (3) A Material Change of Use within the transport route separation area of a KRA that will result in an increase in the number of people working or residing in the transport route separation area.

In this instance, the proposed development does not involve any of the above circumstances, and accordingly, these provisions are not relevant to the assessment of this application.

### 3. Water Quality

These provisions relate to the following matters of state interest:

- (1) Receiving waters - including development applications for:
  - (a) a material change of use for urban purposes that involves a land area of 2,500m<sup>2</sup> or greater that will result in:
    - (i) an impervious area greater than 25% of the net developable area; or
    - (ii) six or more dwellings; or
  - (b) reconfiguring a lot for urban purposes that involves a land area 2,500m<sup>2</sup> or greater and will result in six or more lots; or
  - (c) operational works for urban purposes that disturb 2,500m<sup>2</sup> or greater of land area.
- (2) For water supply buffer areas – a development application located wholly outside an urban area and relating to premises that is within, or partly within, a water supply buffer area, that involves: (i) material change of use activities listed in Part E of the SPP, or (ii) reconfiguring a lot to create five or more additional lots where one or more lots are less than 16ha and any of the created lots rely on on-site wastewater treatment.

In this instance the proposed development involves a Material Change of Use on land with an area less than 2,500m<sup>2</sup> and does not involve the creation of six (6) or more dwellings intended for urban purposes. Accordingly, the assessment provisions for this State interest are not relevant to the assessment of this application.

### 4. Natural Hazards, Risk and Resilience

These provisions relate to development applications for a material change of use, reconfiguring a lot or operational work on land within:

- (i) a flood hazard area;
- (ii) a bushfire hazard area;
- (iii) a landslide hazard area,
- (iv) storm tide inundation areas; and
- (v) erosion prone area.

In this instance, the site is identified as being impacted by natural hazards. In particular, the site is impacted by Flood Hazard Area – Local Government Flood Mapping Area under the State Planning Policy mapping.

Section 2 of the *Toowoomba Regional Planning Scheme 2012* confirms that matters associated with this State interest, as it relates to flood hazards are appropriately integrated into the Planning Scheme. In this instance, the site is not impacted by the locally refined Flood Hazard Overlay and accordingly, is not considered to be impacted by Flood Risk.

Accordingly, the assessment provisions for this State interest are not relevant to the assessment of this application.

### 5. Strategic Airports and Aviation Facilities

These provisions relate to development applications that involve land located within a local government area that contains/impacted by a strategic airport identified in the SPP Part E, Table 2, or an aviation facility identified in Appendix 2 of the *strategic airports and aviation facilities guidance material* and involve:

- (i) a material change of use of premises that will result in work encroaching into the operational airspace of a strategic airport, or on land within the light restriction zone, lighting area buffer

- or the wildlife hazard buffer zone of a strategic airport, or results in work encroaching into the building restricted area of an aviation facility; or
- (ii) a material change of use or reconfiguring a lot where any part of the land is within the 20ANEF contour or greater, or the public safety area of a strategic airport; or
- (iii) building work not associated with a material change of use that will result in work intruding into the operational airspace of a strategic airport, or where any part of the premises is within the light restriction zone of lighting area buffer of a strategic airport, or results in work encroaching into the building restricted area of an aviation facility; or
- (iv) operational work not associated with a material change of use where any work or associated activity will intrude into the operational airspace of a strategic airport, or where any part of the premises is within the light restriction zone of lighting area buffer of a strategic airport, or results in work encroaching into the building restricted area of an aviation facility.

Where these circumstances apply, the development application is assessed against the strategic airports and aviation facilities assessment benchmarks listed under the SPP.

In this instance, the site is not impacted by a Strategic Airport. Accordingly, the assessment provisions for this State interest are not relevant to the assessment of this application

The proposal has been assessed to comply with all applicable matters of state interest included in the State Planning Policy.

### 4.3 STATE REFERRAL AGENCIES

Section 54 of the *Planning Act 2016* and Section 22 and Schedule 10 of the *Planning Regulation 2017 (the Regulation)* are of relevance for the purposes of determining applicable referral agencies and their jurisdiction in terms of being advice or concurrence agencies as well as the relevant assessment benchmarks that are to be addressed.

The State Assessment and Referral Agency (**SARA**), established under the auspices of the Department of State Development, Infrastructure and Planning (**DSDIP**) is the assessment manager or referral agency for development applications where the State has a jurisdiction pursuant to Schedule 8 or 10 of the Regulation. Schedule 10 regulates when applications are referable. In accordance with Schedule 8, DSDIP will not be the assessment manager for this development application. The SDAP components of the overall development will be addressed by DSDIP as part of the application referral process.

**TABLE 1 - REFERRAL TRIGGER ASSESSMENT**

Part	Application Involving	Applicable	Comment
Part 1	Airport land	No	The proposed development is not located on airport land.
Part 2A	Caboolture West Interim Structure Plan	No	The site is not located within the Caboolture West investigation or growth areas.
Part 3	Clearing native vegetation	No	The proposed development does not involve clearing native vegetation or give rise to accepted clearing.
Part 4	Contaminated land	No	The site is not identified as contaminated land.
Part 5	Environmentally relevant activities	No	An approval for an ERA is not sought.
Part 6	Fisheries	No	The proposed development is not associated with:

Part	Application Involving	Applicable	Comment
			<ul style="list-style-type: none"> <li>• aquaculture; or</li> <li>• the removal, destruction or damage of marine plants; or</li> <li>• the constructing or raising of waterway barrier works; and</li> <li>• is not within a declared fish habitat area.</li> </ul>
Part 7	Hazardous chemical facilities	No	The proposed development is not for a hazardous chemical facility.
Part 8	Heritage places	No	The proposed development is not associated with a locally listed place. In addition, the site is not associated with, nor does it adjoin a property on the Queensland heritage register.
<b>Part 9</b>	<b>Infrastructure-related</b>	<b>Yes</b>	<p>The following relates to infrastructure-related referrals:</p> <ul style="list-style-type: none"> <li>• The proposed development does not exceed the thresholds identified at Schedule 20.</li> <li>• The site is not associated with a designated premises.</li> <li>• The site is not located within 25m of a State transport corridor.</li> <li>• <b>The site is located adjacent to a local road that intersects with a State controlled road and is within 100m of a State-controlled intersection.</b></li> <li>• The site is not identified as a future State-controlled road.</li> <li>• The site is not located within 100m of an electricity substation.</li> <li>• The site is not subject to an easement for the benefit of an electricity distribution entity, or transmission entity and for a transmission grid or supply network.</li> <li>• The site is not associated with an easement for oil, gas or electricity infrastructure.</li> <li>• The site is not in proximity to a State controlled transport tunnel (both existing and/or future).</li> </ul>
Part 10	Koala habitat area	No	The site is not mapped as containing koala habitat areas.
Part 11	Noise attenuation land	No	The site is not identified as being noise attenuation land and the site is not within proximity of an off-road motorcycle facility. The proposed development is not associated with prohibited development.
Part 12	Operational works for reconfiguring a lot	No	The proposed development does not involve Operational Works.
Part 13	Ports	No	The site is not associated with port land.
Part 14	Reconfiguring a lot under Land Title Act	No	The proposed development does not involve Reconfiguring a Lot under the Land Title Act.
Part 15	SEQ Development area	No	The site is not located within an SEQ development area.
Part 16	SEQ regional landscape and rural production area	No	The site is not located within the SEQ regional landscape and rural production area or the SEQ rural living area.

Part	Application Involving	Applicable	Comment
	and SEQ rural living area		
Part 16AA	Solar Farms	No	The proposed development does not involve a solar farm.
Part 16A	Southport Spit	No	The site is not located in the Southport Spit.
Part 16B	SEQ Northern Inter-Urban Break	No	The site is not located within the identified SEQ Northern Inter-Urban Break area.
Part 17	Tidal works or work in a coastal management district	No	The proposed development does not involve tidal works or works within a coastal management district.
Part 18	Urban design	No	The proposed development does not involve an increase in GFA that is greater than 25,000m <sup>2</sup> .
Part 19	Water-related development	No	The proposed development does not involve: <ul style="list-style-type: none"> <li>the taking or interfering of water; or</li> <li>removing quarry material from a watercourse or lake; or</li> <li>relates to a dam; or</li> <li>the construction of a levee.</li> </ul>
Part 20	Wetland protection area	No	The site is not identified as being within a wetland protection area.
Part 21	Wind farms	No	The proposed development does not involve a wind farm

The above assessment of the referral triggers under the *Planning Regulation 2017* confirms that the proposed development will trigger referral in this instance. Accordingly, the following referral triggers apply to this application.

Referral Trigger	Planning Regulation 2017	Agency Type	Technical Agency
Development application for material change of use where the site is located adjacent to a local road that intersects with a State controlled road and is within 100m of a State-controlled intersection.	Schedule 10, Part 9, Division 4, Subdivision 2, Table 4	Concurrence Agency	Department of Transport and Main Roads

#### 4.4 STATE DEVELOPMENT ASSESSMENT PROVISIONS

The State Development Assessment Provisions (**SDAP**) provide assessment benchmarks for the assessment of development applications where the chief executive administering the *Planning Act 2016 (the Act)* is the assessment manager or a referral agency. The chief executive through the State Assessment and Referral Agency (**SARA**) uses the SDAP to deliver a coordinated, whole-of-government approach to the state's assessment of development applications. The SDAP applies throughout the State and contains the matters of State interest the chief executive may have regard to when assessing/deciding a development application as either the assessment manager or referral agency.

The SDAP contains criteria for assessment within State Codes to clarify when the State is to be involved in the assessment of an application and the specific matters of State interest that apply. The SDAP is a specific assessment benchmark that a development must be assessed against as prescribed under the Regulation. In this instance, the proposed development triggers assessment against the following State Codes:

- **State Code 1** – Development is a State-Controlled Road Environment

An assessment of the proposed development against the applicable State Code is provided at **Appendix C**.

## **4.5 REGIONAL PLAN**

The site is located within the Toowoomba Regional Council Local Government Area and therefore the provisions of the Darling Downs Regional Plan apply in accordance with the provisions of the *Planning Regulation 2017*. An assessment of the development against the regional plan provisions is provided below.

### **4.5.2 DARLING DOWNS REGIONAL PLAN**

The Darling Downs Regional Plan was adopted on 14 October 2013 and covers the local authority areas of Balonne Shire Council and the Regional Councils of Goondiwindi, Maranoa, Southern Downs, Toowoomba and Western Downs.

The policies contained in the regional plan contribute towards the protection of strategic areas of priority agricultural land use from potentially incompatible resource activities and seek to maximise opportunities for co-existence of resources and agricultural land use.

The regional plan also safeguards areas required for the growth of towns in the regions through the establishment of Priority Living Areas while providing for resource activities to locate within these areas where it meets communities' expectations as determined by the relevant local government.

In this instance, the site is located within the **Priority Living Area (PLA)** associated with the town of Pittsworth. The Priority Living Area identifies land for the development and expansion of towns within the region. The proposal involves an urban use in an area suitable for such purposes. Accordingly, the application complies with the provisions of Darling Downs Regional Plan.

## **4.6 TOOWOOMBA REGIONAL PLANNING SCHEME 2012**

### **4.6.1 INTRODUCTION**

Pursuant to the provisions of section 45(5) of the *Planning Act 2016*, a Development Application must be assessed against the applicable assessment benchmarks of the applicable categorising instrument. The applicable categorising instrument in this instance is the *Toowoomba Regional Planning Scheme 2012*. A summary of the assessment of the proposal against the provisions of this instrument is outlined below.

### **4.6.2 DEFINITIONS**

Under the provisions of the *Toowoomba Regional Planning Scheme 2012*, the proposed use is defined as Dual Occupancy. The relevant use definition is as follows:

**dual occupancy—**

- means a residential use of premises for 2 households involving*

- a. 2 dwellings (whether attached or detached) on a single lot or 2 dwellings (whether attached or detached) on separate lots that share a common property; and
  - b. any domestic outbuilding associated with the dwellings; but
- ii. does not include a residential use of premises that involves a secondary dwelling.

#### 4.6.3 ZONING

Under the *Toowoomba Regional Planning Scheme 2012*, the region is divided into twenty (20) land use zones. Certain zones are further divided into precincts for the purposes of conveying preferred land use intent or in order to assign assessment status to individual uses.

The site is predominately located within the **Major Centre Zone**. The zoning of the site and surrounding locality is illustrated in **Figure 10**.

**FIGURE 10 - ZONING MAPPING**



Under the assessment tables applicable to the Major Centre Zone at section 5.5 of the Planning Scheme, a Material Change of Use for a Dual Occupancy is identified as **Impact Assessable** development.

#### 4.7 ASSESSMENT BENCHMARKS

As identified in section 4.6, this Development Application is subject to Impact Assessment and therefore requires assessment against the following provisions of the *Toowoomba Regional Planning Scheme 2012*.

- Strategic Framework; refer to section 4.7.1
- Overall Outcomes – Major Centre Zone; refer to section 4.7.2

- Applicable Codes; refer to section 4.7.4

#### 4.7.1 STRATEGIC FRAMEWORK

Part 3 of the *Toowoomba Regional Planning Scheme 2012* provides the Strategic Framework that sets the policy direction for the planning scheme and forms the basis for ensuring development occurs at appropriate locations within the planning scheme area. The structure of the Strategic Framework comprises:

- seven (7) themes supported by strategic outcomes;
- elements that further refine the strategic outcomes sought; and
- specific outcomes and land use strategies for each of the elements.

The development has been assessed against the applicable provisions of the Strategic Framework and a summary of this assessment is provided in the table attached at **Appendix A**. An assessment of the proposal against the provisions of the Strategic Framework indicates the proposal complies with the integrated policy direction sought for the planning scheme area and will not compromise the relevant strategic outcomes/elements sought for the individual themes.

#### 4.7.2 OVERALL OUTCOMES – MAJOR CENTRE ZONE

Section 6.3.2.2 of the *Toowoomba Regional Planning Scheme 2012* outlines the general intent for development within the Major Centre Zone through the collective identification of a series of overall outcomes for the zone. The proposed development generally complies with the intent of the Major Centre Zone as detailed in **Table 2**.

**TABLE 2 - ASSESSMENT OF OVERALL OUTCOMES**

Overall Outcome	Response
(3) The overall outcomes for the major rural centres of Oakey, Pittsworth, Crows Nest, Millmerran and Clifton are:	
(a) uses and activities service the needs of the residents and visitors of the major rural centres and the broader rural locality;	<b>Complies:</b> The proposed development provides additional housing to service the needs of residents within the Pittsworth locality.
(b) residential development within the centre does not occur at a scale that compromises the ability of the centre to operate as a concentrated and vibrant commercial and community activity node;	<b>Complies:</b> The proposed development is of a scale that does not compromise the ability of the centre to operate as a concentrated and vibrant commercial and community activity node. It is noted that the provision of additional unit/s was investigated during preliminary phases, however due to the site's access and parking constraints, the economic and contextually-appropriate provision of a higher density was not feasible.
(c) development contributes to the traditional character and heritage values of the streetscape; and	<b>Complies:</b> The proposed development has been designed to contribute to the traditional character of the streetscape.
(d) large format shops and showrooms and car parking areas are located behind smaller active front buildings that front the main street and	<b>Not Applicable</b>

Overall Outcome	Response
maintain the pedestrian scale and connectivity of the main street.	

The above assessment demonstrates that the proposed development generally satisfies the intent and outcomes sought for land included in the Major Centre Zone.

### 4.7.3 APPLICABLE CODES

The codes applicable to the assessment of the proposed development are identified in **Table 3** followed by a summary of the assessment outcomes.

**TABLE 3 - APPLICABLE CODES**

Zone Code
<ul style="list-style-type: none"> <li>Major Centre Zone Code</li> </ul>
Development Codes
<ul style="list-style-type: none"> <li>Medium Density Residential Code</li> <li>Environmental Standards Code</li> <li>Integrated Water Cycle Management Code</li> <li>Landscaping Code</li> <li>Transport, Access and Parking Code</li> <li>Works and Services Code</li> </ul>

An assessment of the proposed development against the above codes is provided at **Appendix B** and confirms that the development complies with the outcomes sought within these codes.

## **5.0 CONSULTATION**

### **5.1 STATUTORY NOTIFICATION**

This Development Application is Impact Assessable and accordingly is required to be publicly notified under the provisions of the *Planning Act 2016* and its associated regulations. Public notification will be undertaken at the appropriate stage of the assessment process, as set out in the Development Assessment Rules prescribed under the *Planning Regulation 2017*.

### **5.2 PRE-LODGEMENT MEETINGS**

The proposed development was not the subject of pre-lodgement discussions, however written advice was received from Council regarding access, street tree and parking matters. This advice has informed the site design, dwelling density and response to the above constraints.

## 6.0 CONCLUSION

This Development Application seeks approval for a Material Change of Use for Dual Occupancy on land at 50 Railway Street, Pittsworth. The assessment that has been undertaken has demonstrated the following:

- The development is residential in nature within an existing commercial area and accordingly is compliant with the intent of the Major Centre Zone. The proposed development provides additional housing at a density appropriate for the regional locality, within the commercial centre of Pittsworth.
- The proposed development has been designed to ensure it does not adversely impact on the surrounding environment or the safety of people and/or property as a result of natural hazards.
- The proposed development achieves compliance with the benchmarks within the relevant zone, overlay and development codes (as outlined in section 4.7).

Having regard to the matters and issues raised in this report it is recommended that Council support this Development Application for a Material Change of Use.

The proposal warrants approval subject to the imposition of reasonable and relevant conditions.

## **APPENDIX A - STRATEGIC FRAMEWORK**

*Toowoomba Regional Planning Scheme 2012*

## **APPENDIX B - ASSESSMENT BENCHMARKS**

*Toowoomba Regional Planning Scheme 2012*

## **APPENDIX C - STATE CODE**

## **APPENDIX D - PROPOSAL PLANS**

*Building Design Professionals*

**APPENDIX E - CONCEPT STORMWATER MANAGEMENT PLAN**  
*Baker Rossow*

## **APPENDIX F - ENGINEERING PLANS**

*Baker Rossow*

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

Theme/Element	Relevance Yes/No	Assessment
<b>3.3 SETTLEMENT PATTERN</b>		
3.3.1 Strategic Outcomes	<b>Yes</b>	The proposed development facilitates an orderly planned infill development on the subject site. The development reflects a low intensity nature and scale appropriate for and reflective of surrounding development within the immediate regional locality. The development provides an opportunity to respond to the planning need and demand for medium density infill development in locations which are proximate to employment, services and transport. The development will maintain a high level of residential amenity within the locality and is consistent with the established pattern of development within the locality.
3.3.2 Element – network of towns	<b>Yes</b>	The development supports the Pittsworth major centre.
3.3.3 Element – compact urban form	<b>Yes</b>	The development contributes to a compact urban form through the provision of a dual occupancy development at a density that can be accommodated on the site. The development has been sensitively designed to mitigate potential amenity and safety issues and ensures adequate separation and buffering from nearby commercial uses.
3.3.4 Element – suburban neighbourhoods	No	
3.3.5 Element – urban neighbourhoods	No	
3.3.6 Element – centres of activity	<b>Yes</b>	The proposed dual occupancy will contribute to the provision of a variety of housing forms which will provide residents with ready access to the range of uses and employment. The proposed development provides a built form that provides a varied character to the Pittsworth Major Centre. The development positively contributes to an improved choice and range of residential accommodation in a commercial area that has convenient access to services and facilities. The development contributes to a compact urban form through the provision of a dual occupancy development at a density that can be accommodated on the site. The development has been sensitively designed to mitigate potential amenity issues the proposed dwellings and ensures adequate separation and buffering from nearby commercial uses.
3.3.7 Element – central business district (CBD)	No	
3.3.8 Element – sustainable urban development	<b>Yes</b>	The development has been designed to comply with sustainable urban design principles. The architectural design will contribute to a distinctive character of the location and respond to the local climatic conditions and site constraints. Private open space and living areas have been located to take advantage of the site's north-eastern aspect and landscaping will provide shading and softening of the built form, in addition to the mature street trees on Railway Street.

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

<b>Theme/Element</b>	<b>Relevance Yes/No</b>	<b>Assessment</b>
3.3.9 Element – rural landscape	No	
3.3.10 Element – natural places	No	
3.3.11 Element – development constraints	No	
3.3.12 Element – incompatible land uses	<b>Yes</b>	The development has been designed to ensure potential land use conflicts are minimised or avoided with respect to visual amenity, overlooking, overshadowing and downstream drainage. The proposed residential activity will not generate adverse amenity impacts on nearby commercial premises.
<b>3.4 NATURAL ENVIRONMENT</b>		
3.4.1 Strategic Outcomes	No	
3.4.2 Element - protect ecosystems with biodiversity values	No	
3.4.3 Element – waterways, wetlands and aquifers	No	
3.4.4 Element - air quality	No	
3.4.5 Element – climate change	No	
3.4.6 Element – environmental offsets	No	
<b>3.5 COMMUNITY IDENTITY AND DIVERSITY</b>		
3.5.1 Strategic Outcomes	<b>Yes</b>	The development meets the changing needs of the community through provision of high-quality, low-maintenance residential accommodation. The development contributes to a compact urban form through provision of a dual occupancy development at a residential density that can be accommodated on the site. The development has been sensitively designed to mitigate potential residential amenity issues and ensures adequate separation and buffering from nearby commercial uses.
3.5.2 Element – rural community identity	<b>Yes</b>	The development supports Pittsworth as a key regional centre through its offering of a high-quality, low maintenance residential accommodation in close proximity to the CBD.
3.5.3 Element – Toowoomba City identity	No	
3.5.4 Element – urban design	<b>Yes</b>	The development has been architecturally and purposefully designed to ensure the building will be aesthetically pleasing and highly functional. The development purposefully designed in a way that will complement the design and look of the surrounding neighbourhood and is consistent with setbacks and material used in other buildings in the surrounding locality.
3.5.5 Element – community facilities and services	No	

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

Theme/Element	Relevance Yes/No	Assessment
3.5.6 Element – sport and recreation	No	
3.5.7 Element – urban parks and public places	No	
3.5.8 Element – housing diversity and affordability	<b>Yes</b>	The development contributes to housing diversity and meets the changing needs of the community through provision of high-quality, low-maintenance residential accommodation in close proximity to the Pittsworth CBD.
3.5.9 Element – cultural diversity and heritage	No	
<b>3.6 NATURAL RESOURCES AND LANDSCAPE</b>		
3.6.1 Strategic Outcomes	No	
3.6.2 Element – scenic amenity	No	
3.6.3 Element – water resources	No	
3.6.4 Element – sustainable production	No	
3.6.5 Element – natural economic resources	No	
3.6.6 Element – stock routes	No	
<b>3.7 ACCESS AND MOBILITY</b>		
3.7.1 Strategic Outcomes	<b>Yes</b>	The development is located on a Local Road that is of a suitable standard to accommodate traffic generated by the infill development. The site is located in an urban area which is well-serviced by pedestrian paths, higher-order roads and centres. The development makes provision for sufficient on-site car parking.
3.7.2 Element – integrated transport system	<b>Yes</b>	Refer to the response to 3.7.1.
3.7.3 Element – active transport	<b>Yes</b>	Refer to the response to 3.7.1.
3.7.4 Element – public transport	<b>Yes</b>	There are no public transport options in Pittsworth.
3.7.5 Element – road network	<b>Yes</b>	Refer to the response to 3.7.1.
<b>3.8 INFRASTRUCTURE AND SERVICES</b>		
3.8.1 Strategic Outcomes	<b>Yes</b>	The site is capable of being connected to all urban services and makes ample provision for on-site carparking.
3.8.2 Element – coordinated infrastructure planning and delivery	<b>Yes</b>	Refer to the response to 3.8.1.

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

Theme/Element	Relevance Yes/No	Assessment
3.8.3 Element – integrating water management and infrastructure	<b>Yes</b>	Refer to the response to 3.8.1.
3.8.4 Element – waste water management infrastructure and services	<b>Yes</b>	Refer to the response to 3.8.1.
3.8.5 Element – utility infrastructure and services	<b>Yes</b>	Refer to the response to 3.8.1.
3.8.6 Element – waste management and recycling	<b>Yes</b>	The development makes adequate provision for domestic and recyclable waste.
<b>3.9 ECONOMIC DEVELOPMENT</b>		
3.9.1 Strategic Outcomes	<b>Yes</b>	The development supports Pittsworth as a Regional Centre through the provision of a high-quality, low-maintenance residential accommodation near the CBD. The development positively contributes to the range and choice of residential accommodation through the provision of a dual occupancy development at a density that can be accommodated on the site.
3.9.2 Element - economic growth	No	
3.9.3 Element – activity centres and employment	No	
3.9.4 Element – creative and knowledge-based industries	No	
3.9.5 Element – tourism	No	

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Caretaker's accommodation</b>		
<p>PO<sub>1</sub> Development provides for the accommodation of a caretaker and their family members involved in the running of a non-residential use, in a manner that:</p> <ul style="list-style-type: none"> <li>(a) does not compromise the productivity of the use;</li> <li>(b) is safe for the residents;</li> <li>(c) has regard to the landscape and private recreation needs of the residents; and</li> <li>(d) does not impact on the function or amenity of the centre at street level.</li> </ul>	<p>AO<sub>1.1</sub> A caretaker's accommodation is:</p> <ul style="list-style-type: none"> <li>(a) separated from significant levels of emissions (adverse to human health or amenity) generated by the use/s of the site by at least 6m;</li> <li>(b) provided with a private landscape and recreation area which:               <ul style="list-style-type: none"> <li>(i) is directly accessible from a habitable room; and</li> <li>(ii) if a balcony, a veranda or a deck, has a minimum area of 8m<sup>2</sup> with minimum dimensions of 2.4m.</li> </ul> </li> </ul> <p>AO<sub>1.2</sub> No more than one (1) caretaker's accommodation is established per non-residential use.</p>	<p><b>N/A:</b> The proposed development is associated with a Dual Occupancy.</p>
<b>Building Work (not associated with a Material Change of Use)</b>		
<p>PO<sub>2</sub> Provision is made for on-site vehicle parking to meet the demand likely to be generated by the development and to avoid on-street parking where that would adversely impact on the safety or capacity of the road network or unduly impact on local amenity.</p>	<p>PO<sub>2.1</sub> Car parking is provided in accordance with the Transport, Access and Parking Code.</p>	<p><b>N/A:</b> The proposed development does not involve Building Work not associated with a Material Change of Use.</p>
<p>PO<sub>3</sub> Landscaping makes a positive contribution to the site and the amenity of the surrounding area and existing landscaping is not diminished.</p>	<p>PO<sub>3.1</sub> No reduction of previously approved landscaping areas is to occur.</p>	<p><b>N/A:</b> The proposed development does not involve Building Work not associated with a Material Change of Use.</p>
<p>PO<sub>4</sub> Stormwater resulting from roofed areas is collected and discharged in a manner that</p>	<p>PO<sub>4.1</sub> Roof water is collected and discharged in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.</p>	<p><b>N/A:</b> The proposed development does not involve Building Work not associated with a Material Change of Use.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>does not adversely affect the stability of buildings or the use of adjacent land.</p>		
<p>PO<sub>5</sub> Wastewater treatment and disposal is provided that is appropriate for the level of demand generated, protects public health and avoids environmental harm.</p>	<p>PO<sub>5.1</sub> Where within a wastewater area, the development is connected to the Council's reticulated wastewater system in accordance with SC6.3 PSP No.3 Engineering Standards – Water and Wastewater Infrastructure.</p> <p>OR</p> <p>PO<sub>5.2</sub> Waste water systems and connections are designed and constructed in accordance with SC6.3 PSP No.3 Engineering Standards - Water and Wastewater Infrastructure.</p>	<p><b>N/A:</b> The proposed development does not involve Building Work not associated with a Material Change of Use.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Uses, Role and Function</b>		
<p>PO<sub>1</sub> The zone accommodates a wide range of centre related activities including discount department stores that service the retail needs of the major centre catchment, but not including Department Stores.</p>	<p>AO<sub>1.1</sub> Uses which are consistent with the intent of the zone include:</p> <ul style="list-style-type: none"> <li>(a) accommodation activities (other than a dwelling house, dual occupancy or tourist park);</li> <li>(b) adult store;</li> <li>(c) agricultural supplies store;</li> <li>(d) bar;</li> <li>(e) business activities (other than bulk landscape supplies);</li> <li>(f) child care centre;</li> <li>(g) club;</li> <li>(h) community care centre;</li> <li>(i) community residence;</li> <li>(j) community use;</li> <li>(k) educational establishment;</li> <li>(l) emergency services;</li> <li>(m) entertainment activities;</li> <li>(n) function facility;</li> <li>(o) funeral parlour;</li> <li>(p) health care services;</li> <li>(q) home based business;</li> <li>(r) indoor sport and recreation;</li> <li>(s) major electricity infrastructure;</li> <li>(t) market;</li> <li>(u) place of worship;</li> <li>(v) outdoor sport and recreation;</li> <li>(w) service industry use;</li> <li>(x) Shop</li> <li>(y) Shopping centre</li> </ul>	<p><b>Performance Solution:</b> The proposed development is associated with a Dual Occupancy which is not listed as a consistent or inconsistent use under AO<sub>1.1</sub> or AO<sub>1.2</sub>. In this regard, it is noted that while a Dual Occupancy is not a use that is consistent with the intent of the zone, accommodation activities are considered consistent. A multiple dwelling development was considered for the site, however given the site constraints with regards to allotment size, height limit and vehicle access, an increased dwelling density was not feasible. The proposed development seeks to provide residential accommodation within close proximity to the existing town centre and services at a density that is suitable for the site.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	(z) substation; and (aa) veterinary services AO <sub>1.2</sub> Uses which are inconsistent with the intent of the zone include: (a) Rural activities (other than those listed in AO <sub>1.1</sub> ). (b) Medium impact, high impact or special industry.	
PO <sub>2</sub> Development does not undermine the viability, role or function of the principal activity centre or of other major centres and occurs commensurate with demand in the relevant catchment.	No acceptable outcome is nominated.	<b>Complies:</b> The proposed development is associated with a Dual Occupancy and accordingly, is not of a nature that would undermine the viability or role of the Pittsworth Major Centre. Additionally, the proposed development will provide housing that will increase patronage of the existing commercial centre.
PO <sub>3</sub> Residential development: (a) occurs in a form and location that is consistent with the scale and efficient functioning of the centre; (b) does not interrupt ground level activity and circulation; and (c) does not compromise the intent for the centre to provide retail and service provision consistent with the intent of the major centre.	AO <sub>3.1</sub> Residential development achieves a minimum net density of: (a) 40 dwelling units per hectare; or (b) 60-80 dwelling units per hectare where located in the Kearneys Spring or Clifford Gardens Centre where fronting public transport routes, or public open space.	<b>Performance Solution:</b> The site has an area of 623m <sup>2</sup> and accommodates two dwellings, resulting in an approximate net density of 32 dwellings per hectare. While marginally below the numerical target, the proposal remains consistent with the intent of the density provisions by facilitating an efficient use of serviced land without resulting in overdevelopment of the site. A multiple dwelling development was initially considered for the site, however given the site constraints with regards to allotment size, height limit and vehicle access, an increased dwelling density was not feasible. The development maintains appropriate setbacks, private open space, landscaping and on-site parking, ensuring an acceptable residential outcome compatible with the surrounding locality. The proposed development does not interrupt ground level activity

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
		and circulation and does not compromise the existing retail and commercial services within the locality.
<b>Bulk and scale</b>		
PO <sub>4</sub> Building size and scale is consistent with the role, function and character of the centre, and provides an appropriate interface with the scale and character of proximate residential areas.	AO <sub>4.1</sub> Development does not exceed a maximum building height of: (a) six (6) storeys in the Kearneys Spring and Clifford Gardens centres; or (b) three (3) storeys in the Wilsonton centre; or (c) two (2) storeys in all other major centres.  AO <sub>4.2</sub> Development does not exceed a plot ratio of 2.0.	<b>Complies:</b> The proposed development has a maximum height of 6.93m over two (2) storeys.
<b>Major Centres – Centre Design</b>		
PO <sub>5</sub> Development in the Oakey, Pittsworth, Crows Nest, Millmerran and Clifton Centres maintain the main street style of the centre where development: (a) is oriented to the street with active uses provided such as shops, restaurants and cafes located at ground level; and (b) incorporates footpaths, tree planting, lighting, seating and other streetscape elements that enhance the quality of the pedestrian environment.	AO <sub>5.1</sub> Buildings in the Oakey, Pittsworth, Crows Nest, Millmerran and Clifton Centres: (a) are built to the street frontage; (b) have active facades that face the street; and (c) have continuous awnings along the main street and at corners of the main street and intersecting secondary streets.  AO <sub>5.2</sub> Large scale building, such as supermarkets and showrooms, are sleeved behind smaller scale buildings fronting the main street and provide pedestrian access to the main street through integrated building entrances.  AO <sub>5.3</sub> Car parking and loading/unloading areas are located behind buildings fronting the main street.	<b>Performance Solution:</b> The development site does not have direct frontage to the main street, with Short Street and Railway Street both acting as a back-of-house access to the commercial premises fronting Yandilla Street. Buildings within this locality are not built to the boundary and are provided with a more residential style built form, with setbacks to boundaries and a more centralised built form. The proposed Dual Occupancy provides an approximately 3m setback to both Railway Street and Short Street to facilitate appropriate on-site vehicle parking and manoeuvring and private open space. Having regard to the existing streetscape context and the proposed use the proposed setbacks are considered suitable.

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>6</sub> Development facilitates the elements shown in Figures 6.3.2:1 - 3 – Kearneys Spring, Wilsonton and Clifford Gardens major centre concept diagrams, and generally enhances and enlivens the centre by:</p> <ul style="list-style-type: none"> <li>(a) providing vibrant and attractive spaces to facilitate social interaction;</li> <li>(b) ensuring frontages to the public road network or internal pedestrian movement network are well articulated and where appropriate, incorporate uses that activate the ground floor;</li> <li>(c) integrating all elements of the centre so that users can efficiently move throughout the centre;</li> <li>(d) providing a retail and commercial environment that is vibrant and competitive; and</li> <li>(e) directing community and entertainment uses towards the town square, major movement networks and pedestrian paths and places.</li> </ul>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The site is not located within the Kearneys Spring, Wilsonton or Clifford Gardens Major Centres.</p>
<p>PO<sub>7</sub> Development includes a town square or public plaza in the major centres of Kearneys Spring and Wilsonton in a position generally consistent with that indicated on Figures 6.3.2:1 - 2 – Kearneys Spring and Wilsonton major centre concept diagrams. The town squares in Kearneys Spring and Wilsonton are memorable public spaces that provide community and centre focus, and:</p>	<p>AO<sub>7.1</sub> The town square in the Major Centre Zone in Wilsonton is a minimum of 400m<sup>2</sup> in area.</p>	<p><b>N/A:</b> The site is not located within the Wilsonton Major Centre.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(a) are appropriately furnished with street furniture including seats, bins, bicycle racks, drinking fountains and other embellishments;</li> <li>(b) are landscaped to include ground covers, shade trees and other vegetation;</li> <li>(c) provide a balance of urban outdoor spaces and green space areas;</li> <li>(d) are abutted by buildings with active uses such as shops, restaurants and cafes; and</li> <li>(e) are afforded convenient pedestrian access connection to enable movement of people to and from other parts of the centre and beyond.</li> </ul>		
PO <sub>8</sub> Buildings are oriented to and aligned parallel with the external street network and internal pedestrian network, as indicated in Figures 6.3.2:1 - 3 – Kearneys Spring, Wilsonton and Clifford Gardens major centre concept diagrams.	No acceptable outcome is nominated.	<b>N/A:</b> The site is not located within the Kearneys Spring, Wilsonton or Clifford Gardens Major Centres.
PO <sub>9</sub> Development provides for active frontages in the form of continuous retail uses such as shops and food and drink outlets or offices (where providing a service with direct interaction with the public, such as banks and real estate agents) at ground level in the locations shown as active frontages on Figures 6.3.2:1 - 3 – Kearneys Spring, Wilsonton and Clifford Gardens major centre concept diagrams.	No acceptable outcome is nominated.	<b>N/A:</b> The site is not located within the Kearneys Spring, Wilsonton or Clifford Gardens Major Centres.
PO <sub>10</sub> New buildings that provide active frontages incorporate footpath awnings and display	No acceptable outcome is nominated.	<b>Performance Solution:</b> The proposed development is for a residential use and appropriate setbacks are

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
windows to provide weather protection and shade to pedestrians.		provided to ensure the amenity of the residents. Given the proposal is for residential accommodation footpath awnings and display windows are not proposed.
PO <sub>11</sub> In the Kearneys Spring Centre development: <ul style="list-style-type: none"> <li>(a) establishes an active building frontage and a pedestrian environment incorporating footpaths, tree planting, lighting, seating and other streetscape elements along Stenner and Hume Streets;</li> <li>(b) large enclosed retail anchor tenancies are sleeved by a mix of retail, commercial and dining uses in smaller tenancies which orientate towards pedestrian pathways and nodes;</li> <li>(c) community and entertainment uses are directed toward the major external movement networks and the internal pedestrian pathways and places;</li> <li>(d) repositions the majority of car parking behind or under buildings;</li> <li>(e) integrates car parking areas of new and existing development to support multi-destination trip ends and avoid short vehicle movements on the local road network, including by use of multi-level structures that deal with the change in grade from east to west across the centre;</li> <li>(f) provides safe, direct, attractive and comfortable pedestrian connections between new and existing development,</li> </ul>	No acceptable outcome is nominated.	<b>N/A:</b> The site is not within the Kearneys Spring Major Centre.

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>including creative use of stairs, ramps, platforms and elevated small squares or parks to deal with the change in grade;</p> <p>(g) a network of boulevards is established through street tree planting along Ruthven Street, Stenner Street, Hume Street and Spring Street to contain the centre, define main external movement networks and to enhance the amenity of adjoining residential areas; and</p> <p>(h) street tree planting and centre planting promotes a consistent 'garden city' image.</p>		
<p>PO12 In the Clifford Gardens Centre development:</p> <p>(a) establish a new internal street:</p> <p style="margin-left: 20px;">(i) based on the alignment of the existing main driveway situated to the west of Westray Street and east of the existing shopping centre to provide a connection to the shopping centre component on the north-east of the centre at Vacy and Princess Streets;</p> <p style="margin-left: 20px;">(ii) to act as a 'main street' internal to the centre with new buildings providing active frontages;</p> <p style="margin-left: 20px;">(iii) providing convenient access to an active building frontage and a pedestrian environment incorporating footpaths, tree planting, lighting, seating and other streetscape elements;</p>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The site is not within the Clifford Gardens Major Centre.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – assessment benchmarks for assessable development

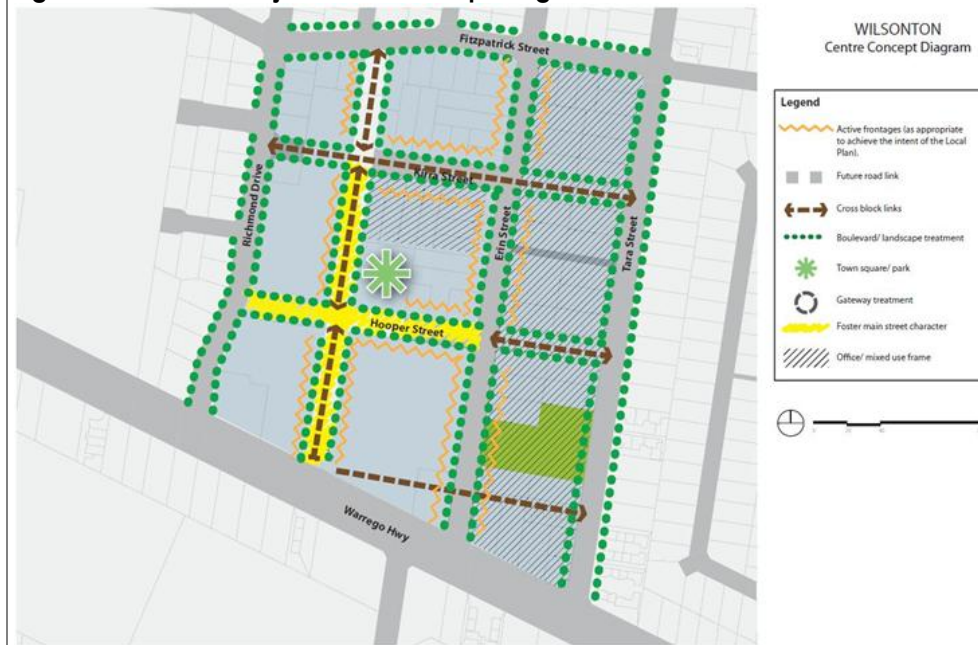
Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(iv) incorporating angled parking to foster the 'main street' character and provide convenient access to commercial activities fronting the street and other car parking being located under or behind buildings to provide active frontages to ground level on Westray, Princess and the internal street; and</li> <li>(v) street trees planting and centre planting promotes a consistent 'garden city' image.</li> <li>(b) a network of boulevards is established along James Street, Vacy Street and Anzac Avenue through street trees to define the main external movement networks and to enhance the amenity of adjoining residential areas.</li> </ul>		
<p>PO13 In the Wilsonton Centre development:</p> <ul style="list-style-type: none"> <li>(a) incorporating large, enclosed retail anchor tenancies is sleeved by a mix of retail, commercial and dining uses in smaller tenancies which orientate towards a town square, streets and other pedestrian pathways and nodes; and</li> <li>(b) a mixed-use precinct is created with residential uses generally occurring above ground and first floor levels oriented to Erin and Tara Streets.</li> </ul>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The site is not located within the Wilsonton Major Centre.</p>

Figure 1: Kearneys Spring major centre concept diagram



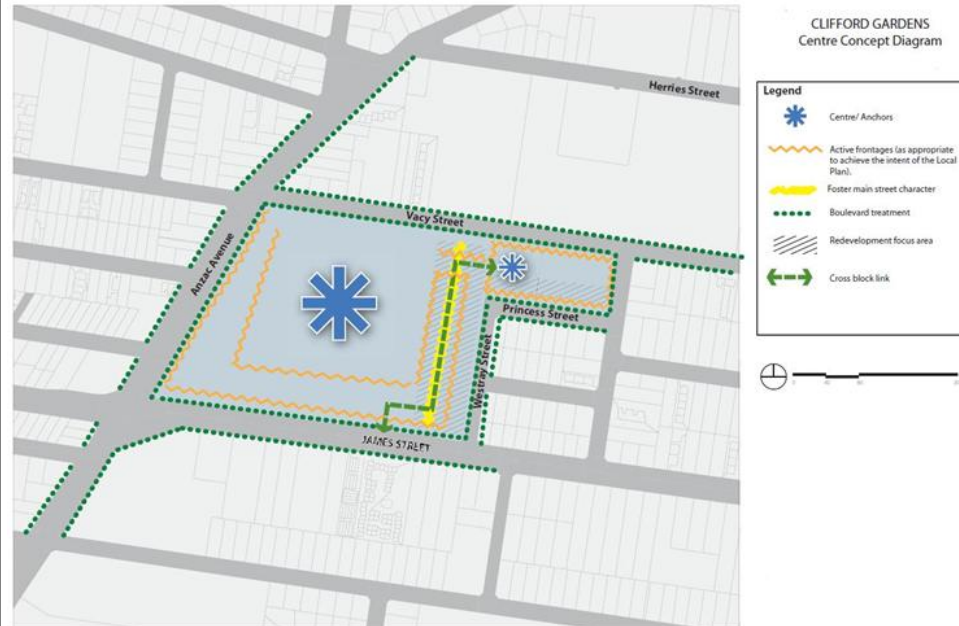
Note: Sight Distance – Minimum sight distance is achieved in accordance with the Department of Transport and Main Roads Planning and Design Manual and the Road Landscape Manual.

Figure 2: Wilsonton major centre concept diagram



Note: Sight Distance – Minimum sight distance is achieved in accordance with the Department of Transport and Main Roads Planning and Design Manual and the Road Landscape Manual.

Figure 3: Clifford Gardens centre concept diagram



Note: Sight Distance – Minimum sight distance is achieved in accordance with the Department of Transport and Main Roads Planning and Design Manual and the Road Landscape Manual.

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>14</sub> Building setbacks facilitate:</p> <ul style="list-style-type: none"> <li>(a) an attractive pedestrian environment at ground level which accommodates space for pedestrian circulation, social interaction and streetscaping;</li> <li>(b) continuity of building lines that enhance the local character; and</li> <li>(c) buffering to adjoining sensitive land uses where appropriate.</li> </ul>	<p>AO<sub>14.1</sub> On streets identified as having a boulevard treatment on Figures 6.3.2:1 - 3 Kearneys Spring, Wilsonton and Clifford Gardens major centre concept diagrams, buildings are set back a minimum of 6m.</p> <p>AO<sub>14.2</sub> In Kearneys Spring and Clifford Gardens on road frontages, other than one having a boulevard treatment or where fronting the internal pedestrian network (refer Figure 6.3.2:1 and 3) development of:</p> <ul style="list-style-type: none"> <li>(a) the first two (2) storeys of a building are built to the front boundary alignment or to the edge of the pedestrian pathway; and</li> <li>(b) parts of a building above two (2) storeys in height are set back a minimum of 6m from the boundary alignment or the edge of the pedestrian pathway.</li> </ul> <p>AO<sub>14.3</sub> Elsewhere, buildings are setback within 20% of the setbacks of adjoining buildings.</p> <p>AO<sub>14.4</sub> Buildings are set back 3m or half the height of the building whichever is the greater to any side or rear boundary which adjoins a residential use or zone.</p>	<p><b>N/A:</b> The site is not located within the Kearneys Spring, Wilsonton or Clifford Gardens Major Centres.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>15</sub> New development or redevelopment involving large format stores or large shopping centres ensure that these components:</p> <ul style="list-style-type: none"> <li>(a) are not visually prominent in terms of building bulk and do not present as buildings on stand alone sites surrounded by extensive areas of surface car parking;</li> <li>(b) are sleeved by active frontages to pedestrian routes and streets; and</li> <li>(c) establish a competitive tension between anchor stores on a site or on separate sites through adequate separation of the anchor stores and activation of significant pedestrian routes between them.</li> </ul>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The proposed development is associated with a Dual Occupancy.</p>
<b>Movement Networks</b>		
<p>PO<sub>16</sub> Development adjacent to a road identified as having a boulevard treatment contributes to the establishment of a boulevard character for the roads through:</p> <ul style="list-style-type: none"> <li>(a) deep planting of large trees in generous building setbacks; and</li> <li>(b) maintaining a visually open area on private property adjoining the road and avoiding the construction of high solid front fences.</li> </ul>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The adjoining road is not identified as having boulevard treatment.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Major Centre Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>17</sub> Vehicular access does not impede the operational capacity, safety or efficiency of major roads.</p>	<p>AO<sub>17.1</sub> In Kearneys Spring, development does not provide new additional vehicular access points to Ruthven Street and Hume Street.</p> <p>AO<sub>17.2</sub> In Clifford Gardens, new additional vehicular access points are not provided to Anzac Avenue or James Street.</p> <p>AO<sub>17.3</sub> In the Wilsonton major centre new additional vehicular access points are not provided to Bridge Street and existing access point are not intensified.</p>	<p><b>N/A:</b> The site is not located within the Kearneys Spring, Wilsonton or Clifford Gardens Major Centres.</p>
<p>PO<sub>18</sub> Development facilitates an integrated and connected pedestrian and bicycle movement network in and around the area in accordance with Figures 6.3.2:1 - 3 – (Kearneys Spring, Wilsonton and Clifford Gardens major centre concept diagrams) which provides:</p> <ul style="list-style-type: none"> <li>(a) access between all major elements of the centre;</li> <li>(b) gathering places for centre users positioned at regular intervals; and</li> <li>(c) a safe, convenient and legible circulation network.</li> </ul>	<p>AO<sub>18.1</sub> Pedestrian routes are a minimum of 5m wide.</p> <p>AO<sub>18.2</sub> In Wilsonton, connections are provided in the form of either new public roads or private but permanently publicly accessible roadways in private developments, and include:</p> <ul style="list-style-type: none"> <li>(a) the augmentation of Kirra Street between Tara Street and Richmond Drive;</li> <li>(b) the augmentation and realignment of Hooper Street between Tara Street and Richmond Drive;</li> <li>(c) a new north-south link through the centre from Sprott Street;</li> <li>(d) a new east-west link through the centre from the Sprott Street extension to Streten Park; and</li> <li>(e) opportunities for angle parking along Hooper Street to foster a 'main street' character.</li> </ul>	<p><b>N/A:</b> The proposed development is not of a nature that warrants facilitation of movement networks.</p> <p><b>N/A:</b> The site is not located within the Wilsonton Major Centre.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Major Centre Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>19</sub> Majority of car parking is provided at the rear of or under buildings.	No acceptable outcome is nominated.	<b>Complies:</b> The proposed development provides a double garage for each unit. The garages have been oriented so that the garage opening does not front the street. Appropriate building articulation, including window openings and a gable roof form, are provided along the garage walls to provide a visually pleasing façade that positively contributes to the streetscape.
PO <sub>20</sub> Development of individual sites occurs in a way that facilitates integration of access and parking across adjoining sites as they are progressively redeveloped.	No acceptable outcome is nominated.	<b>Complies:</b> The proposed development is not reliant upon integrated access or shared parking arrangements with adjoining sites. The surrounding allotments are separately developed with established and independent vehicle access arrangements, and there is no demonstrated need or practical opportunity for integrated access or parking outcomes. The proposal provides compliant on-site access and parking wholly within the subject site and will not prejudice the future redevelopment potential of adjoining land.

Toowoomba Regional Planning Scheme 2012 (v28.0)

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Where a Dual Occupancy</b>		
PO <sub>1</sub> Dual Occupancy development is dispersed within residential neighbourhoods, not clustering in groups of more than three adjoining or opposite sites, and does not become the dominant form of housing within a street by occupying more than 20 percent of properties within a street block.	AO <sub>1.1</sub> In Greenfield Areas Dual Occupancy development occurs on designated Dual Occupancy lots.	<b>N/A:</b> the site is not located within a Greenfield Area.
PO <sub>2</sub> The development is designed in a manner that responds to the sites opportunities and constraints, including any existing building on the lot, and to the broader urban and streetscape contexts.  Note: Development should be designed and documented by a suitably qualified professional (i.e. architect or building designer). Details of professional qualifications should be noted on drawings submitted with any development application	AO <sub>2.1</sub> The design of the development considers and responds to: (a) Site specific qualities including: (i) site topography and slope (contours at a minimum of 1m intervals); (ii) Views (iii) solar aspect and access; (iv) prevailing breezes (v) existing buildings on the site; (vi) site access (vii) services (viii) identification of buildings to be demolished; (ix) existing vegetation (locations to be surveyed and the diameter of the trunk at 1m above ground level shown. On highly buttressed trees the extent of buttressed roots is to be shown); (x) proposed position of new buildings overlaid (b) Nearby features including:	<b>Complies:</b> The proposed development has been designed where possible to consider the physical characteristics of the site. In particular, the proposed development ensures that sufficient solar access is provided for each unit. Furthermore, site access has been located to ensure that pedestrian and vehicle access to the site can occur in a safe manner.

Toowoomba Regional Planning Scheme 2012 (v28.0)

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(i) opportunities for on street visitor car parking;</li> <li>(ii) buildings on adjoining land</li> <li>(iii) important views from neighbouring properties</li> <li>(iv) features and character of adjoining land and buildings including doors &amp; windows</li> <li>(v) street fixtures;</li> <li>(c) Local qualities and amenities including:               <ul style="list-style-type: none"> <li>(i) available public transport;</li> <li>(ii) nearby amenities such as parks, shops</li> </ul> </li> </ul> <p>AO<sub>2.2</sub> Site design retains all existing street trees.</p> <p>Note: See Figure 1 below for examples of site analysis and nearby feature plans.</p>	<p><b>Complies:</b> The proposed development will not require the removal of street trees.</p>

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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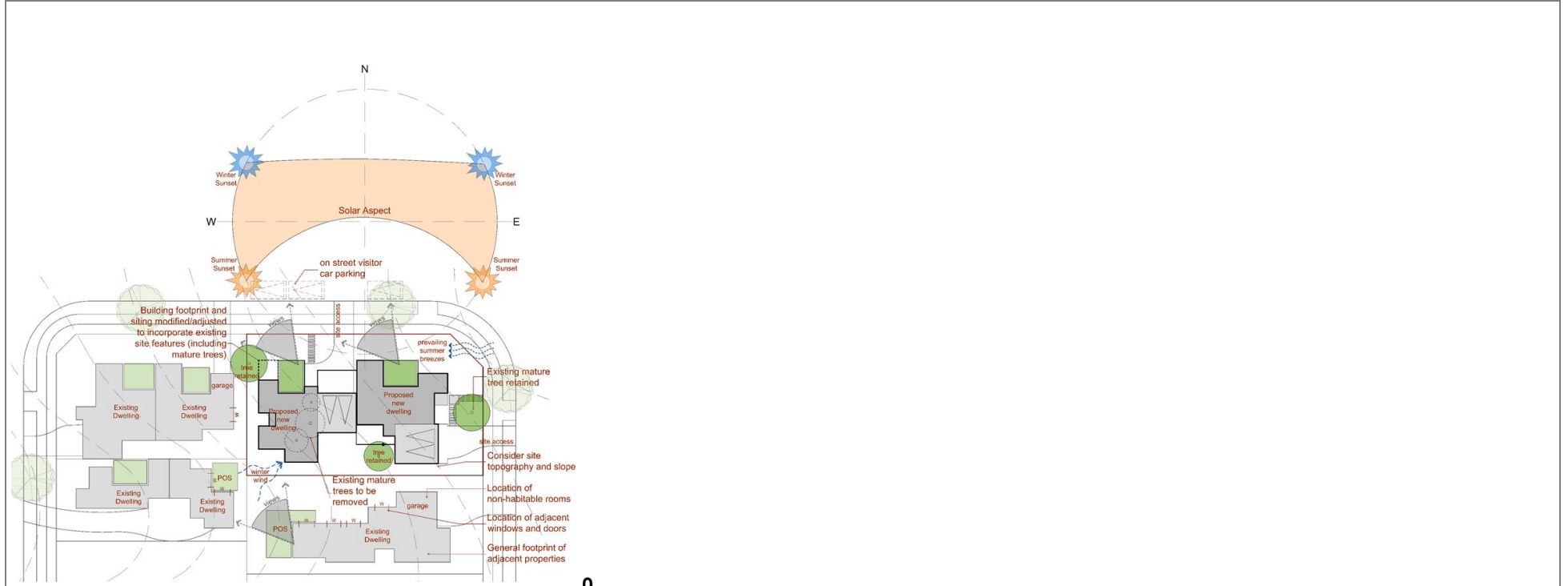


Figure 1

<p>PO<sub>3</sub> Development responds to the slope of the land in the siting, design and form of buildings and structures to minimise requirements for cut and fill.</p>	<p>AO<sub>3.1</sub> Cut and fill beyond natural ground level does not exceed a vertical distance of 1m, unless contained within building walls and in accordance with the requirements outlined in</p>	<p><b>Complies:</b> Cut and fill will be designed in accordance with the relevant standards.</p>
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.</p> <p>Note: See Figure 2.</p> <p>AO3.2 Batters and earth-retaining structures are constructed in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.</p>	<p><b>Complies:</b> If used, batters and retaining walls will be designed in accordance with the relevant standards.</p>

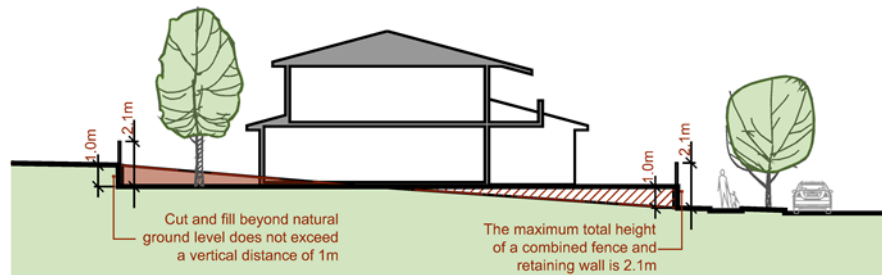


Figure 2

PO <sub>4</sub>	Development is designed to facilitate the retention of any existing street tree adjacent to the site.	AO <sub>4.1</sub> Development does not compromise or require the removal of any street tree.	<b>Complies:</b> The proposed development ensures that street trees are retained.
PO <sub>5</sub>	Development: <ul style="list-style-type: none"> <li>(a) provides for the on-site infiltration of stormwater and minimises the additional burden on drainage infrastructure;</li> <li>(b) minimises the visual impact of hard surface areas; and</li> </ul>	AO <sub>5.1</sub> The impervious site cover does not exceed Table 1:	<b>Complies:</b> The proposed development has an impervious site cover of 59%.

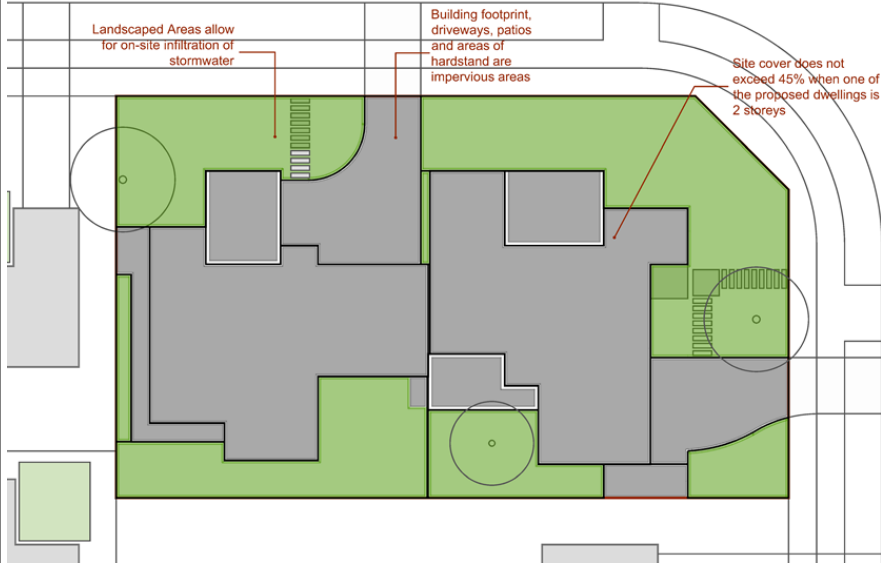
Toowoomba Regional Planning Scheme 2012 (v28.0)

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary																
<p>(c) respects the existing or preferred neighbourhood character and responds to the features of the site.</p>	<table border="1" style="margin-bottom: 10px;"> <thead> <tr> <th colspan="2">Table 1</th> </tr> <tr> <th>Average building height</th> <th>Maximum impervious site cover</th> </tr> </thead> <tbody> <tr> <td>If both of the proposed dwellings are 1 storey</td> <td>70%</td> </tr> <tr> <td>If one or both of the proposed dwellings is 2 storeys</td> <td>65%</td> </tr> </tbody> </table> <p>AO<sub>5.2</sub> No more than 50% of the front setbacks area is impervious.</p> <p>AO<sub>5.3</sub> Site cover does not exceed rates specified in Table 2.</p> <table border="1" style="margin-bottom: 10px;"> <thead> <tr> <th colspan="2">Table 2</th> </tr> <tr> <th>Dwelling Height</th> <th>Maximum site cover</th> </tr> </thead> <tbody> <tr> <td>If both of proposed dwellings are 1 storey</td> <td>50%</td> </tr> <tr> <td>If one of proposed dwellings is 2 storeys</td> <td>45%</td> </tr> </tbody> </table> <p>Note: See Figure 3.</p>	Table 1		Average building height	Maximum impervious site cover	If both of the proposed dwellings are 1 storey	70%	If one or both of the proposed dwellings is 2 storeys	65%	Table 2		Dwelling Height	Maximum site cover	If both of proposed dwellings are 1 storey	50%	If one of proposed dwellings is 2 storeys	45%	<p><b>Complies:</b> Less than 50% of the front setback is imperviously sealed.</p> <p><b>Complies:</b> The proposed development has a site cover of 42% which is less than the 45% threshold.</p>
Table 1																		
Average building height	Maximum impervious site cover																	
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
 <p>The diagram shows a site plan with various areas highlighted. Green areas represent landscaped zones, while grey areas represent building footprints, driveways, and hardstands. Red lines and text callouts provide specific details: 'Landscaped Areas allow for on-site infiltration of stormwater' points to a green area with a grid pattern; 'Building footprint, driveways, patios and areas of hardstand are impervious areas' points to a grey building footprint; and 'Site cover does not exceed 45% when one of the proposed dwellings is 2 storeys' points to a green area within a building footprint.</p>		
<p><b>Figure 3</b></p> <p>PO<sub>6</sub> Areas of hardstand required for parking and pathways:            (a) are minimised,            (b) incorporate generous landscaping:                (i) to promote a garden streetscape character, and                (ii) to minimise visual impacts of views to hardstand areas for occupants and the public, particularly in the front garden.</p>	<p>AO<sub>6.1</sub> Landscaped areas are provided in accordance with Table 3:</p>	<p><b>Complies:</b> The proposed development provides sufficient landscaping which complies with Table 3.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary																		
	<table border="1" data-bbox="857 523 1375 1257"> <thead> <tr> <th colspan="3" data-bbox="857 523 1375 571">Table 3</th> </tr> <tr> <th data-bbox="857 571 1025 1018">Building height</th> <th colspan="2" data-bbox="1025 571 1375 627">Minimum landscaped area</th> </tr> <tr> <td data-bbox="857 627 1025 1018"></td> <td data-bbox="1025 627 1198 1018">Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:</td> <td data-bbox="1198 627 1375 1018">Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:</td> </tr> <tr> <td data-bbox="857 1018 1025 1098">1-2 storeys</td> <td data-bbox="1025 1018 1198 1098">10% site area</td> <td data-bbox="1198 1018 1375 1098">15% site area</td> </tr> <tr> <td data-bbox="857 1098 1025 1177">3 storeys</td> <td data-bbox="1025 1098 1198 1177">12.5% site area</td> <td data-bbox="1198 1098 1375 1177">17.5% site area</td> </tr> <tr> <td data-bbox="857 1177 1025 1257">4+ storeys</td> <td data-bbox="1025 1177 1198 1257">15% site area</td> <td data-bbox="1198 1177 1375 1257">20% site area</td> </tr> </thead> </table> <p data-bbox="801 1268 1435 1380">Note: Landscaped areas included in the calculations consist of garden beds and lawn areas throughout the site with a width of no less than 1.5m;</p>	Table 3			Building height	Minimum landscaped area			Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:	Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:	1-2 storeys	10% site area	15% site area	3 storeys	12.5% site area	17.5% site area	4+ storeys	15% site area	20% site area	
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4+ storeys	15% site area	20% site area																		

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>Excludes all paved and other impervious areas such as driveways, patios, terraces, car parking spaces and bin and other utility areas. See Figure 4.</p>	

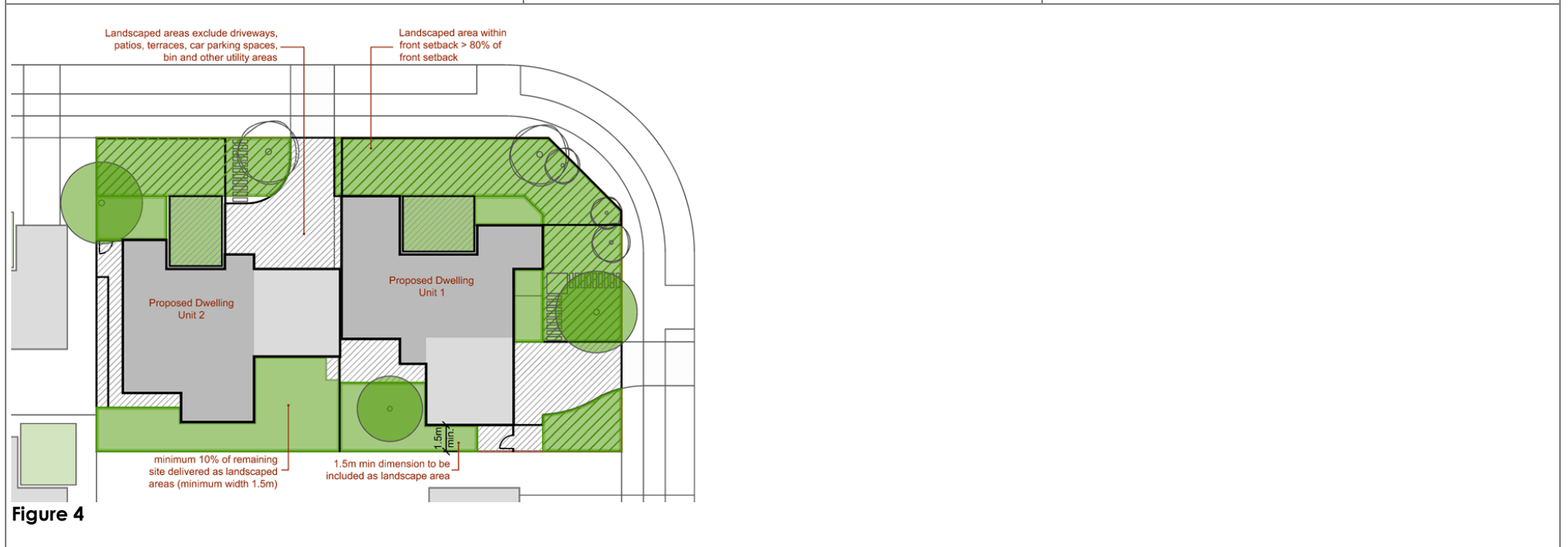


Figure 4

<p>PO<sub>7</sub> The front building setback is consistent with the prevailing front setbacks of other residential buildings in the street.</p>	<p>AO<sub>7.1</sub> Buildings are setback from the road frontage:</p>	<p><b>Complies:</b> The proposed development complies with the setback requirements of the Major Centre Zone Code which prevails over this code.</p>
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**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary										
<p>Note: The prevailing setback of the street is determined by the setbacks of the residential buildings on any lot along the road within 100m of the site.</p>	<p>(a) within 10% of the average front setback of existing residential buildings within 100m of the site; or</p> <p>(b) where there is no adjoining buildings the front setback is provided in accordance with Table 4.</p> <table border="1" data-bbox="846 692 1384 1110"> <thead> <tr> <th colspan="2" data-bbox="846 692 1384 743">Table 4</th> </tr> <tr> <th data-bbox="846 743 1155 801">Building element</th> <th data-bbox="1155 743 1384 801">Minimum setback</th> </tr> </thead> <tbody> <tr> <td data-bbox="846 801 1155 880">Solid building wall (other than a garage)</td> <td data-bbox="1155 801 1384 880">4.0m</td> </tr> <tr> <td data-bbox="846 880 1155 970">Solid building wall (where for a garage)</td> <td data-bbox="1155 880 1384 970">5.5m</td> </tr> <tr> <td data-bbox="846 970 1155 1110">Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)</td> <td data-bbox="1155 970 1384 1110">3.5m (or 3m where secondary road frontage)</td> </tr> </tbody> </table> <p>Note: See Figure 5.</p>	Table 4		Building element	Minimum setback	Solid building wall (other than a garage)	4.0m	Solid building wall (where for a garage)	5.5m	Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)	3.5m (or 3m where secondary road frontage)	
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Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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Toowoomba Regional Planning Scheme 2012 (v28.0)

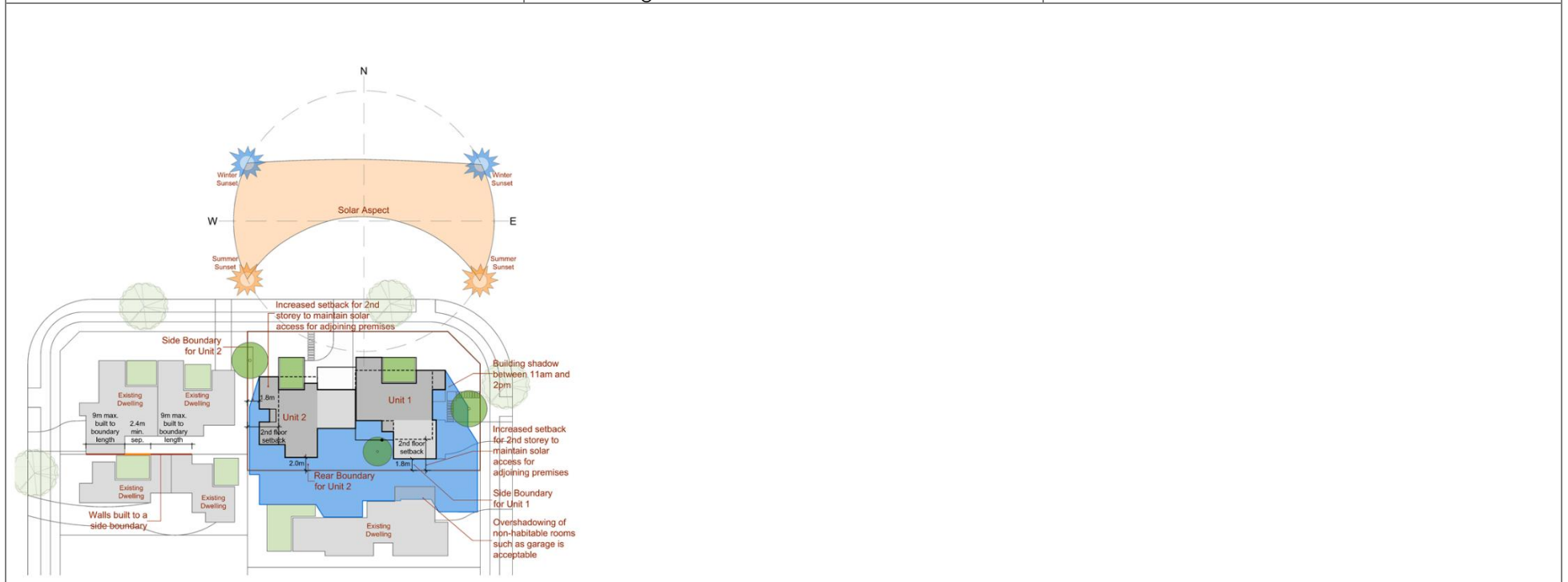
**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Figure 5</b>		
<p>PO<sub>8</sub> Side and rear building setbacks:</p> <ul style="list-style-type: none"> <li>(a) are appropriate to the scale of the development and the intended character of the zone and/or precinct and local plan in which the site is located;</li> <li>(b) provide for adequate daylight for habitable rooms and open space areas on and adjoining the site; and</li> <li>(c) are sufficient to minimise overshadowing and overlooking of adjoining premises.</li> </ul>	<p>AO<sub>8.1</sub> Any part of a building built to a side boundary:</p> <ul style="list-style-type: none"> <li>(a) must not exceed a maximum height of 3.5m;</li> <li>(b) must not exceed a maximum length of 9m;</li> <li>(c) must be a minimum distance of 2.4m from any other wall built to the side boundary (whether on the site or an adjoining site) or be joined to another built to boundary wall if on a adjoining site; and</li> <li>(d) must not exceed a maximum total length of 50% of the side boundary.</li> </ul> <p>OR</p> <p>AO<sub>8.2</sub> Buildings are set back from side boundaries in accordance with the following:</p> <ul style="list-style-type: none"> <li>(a) For lots with frontages less than 20m:               <ul style="list-style-type: none"> <li>(i) 1.2m for any part of the building up to 3.5m high; and</li> <li>(ii) 2m for any part of the building above 3.5m high.</li> </ul> </li> <li>(b) For lots with frontages greater than 20m:               <ul style="list-style-type: none"> <li>(i) 1.8m for any part of the building up to 3.5m high; and</li> <li>(ii) 2.4m for any part of the building above 3.5m high.</li> </ul> </li> </ul> <p>AO<sub>8.3</sub> Buildings are set back from the rear boundary a minimum of 3m.</p> <p>AO<sub>8.4</sub> Development ensures that solar access to habitable rooms and private open space of adjoining premises: -</p>	<p><b>N/A:</b> The proposed development does not involve built to boundary walls.</p> <p><b>Complies:</b> The proposed development complies with the setback requirements of the Major Centre Zone Code which prevails over this code.</p> <p><b>N/A:</b> The site is a corner lot and accordingly, does not have a rear boundary.</p> <p><b>Complies:</b> The proposed development has been designed to ensure that habitable rooms and private open space receives sufficient sunlight during the required hours.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(a) achieves a minimum of 3 hours of sunlight between 9am and 3pm on June 21; or</p> <p>(b) is not further reduced where existing solar access is less than that specified in (a).</p> <p>Note: See Figure 6.</p>	



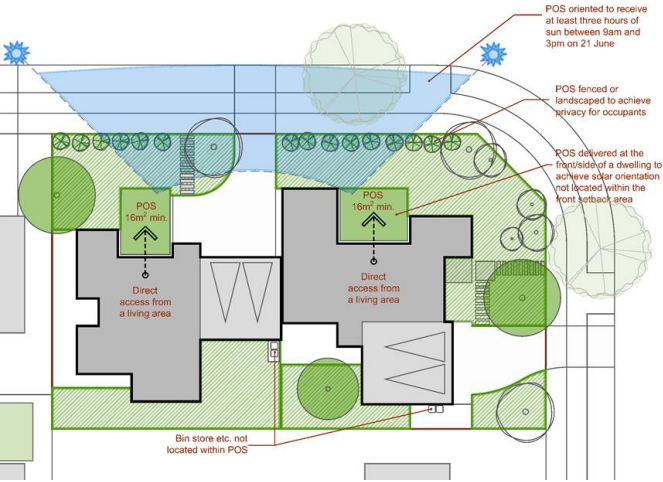
Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Figure 6</b>		
<p>PO<sub>9</sub> Development provides adequate and suitable private open space to meet the needs of occupants for privacy, relaxation and entertainment and to accommodate visitors.</p>	<p>AO<sub>9.1</sub> Private open space has a minimum area of 16m<sup>2</sup> and minimum dimensions of 4 metres by 4 metres.</p> <p>AO<sub>9.2</sub> Private open space is:</p> <ul style="list-style-type: none"> <li>(a) is oriented to receive at least three hours of sun between 9am and 5pm on 21 June.</li> <li>(b) directly accessible from a living area or kitchen;</li> <li>(c) fenced or landscaped to achieve privacy for occupants;</li> <li>(d) clear of bins, clothes lines, hot water systems, air conditioning units, above ground water tanks and associated infrastructure and other forms of ancillary equipment and</li> <li>(e) not located within the front setback area.</li> </ul> <p>Note: See Figure 7.</p>	<p><b>Complies:</b> The proposed private open space of each unit has an area of 16m<sup>2</sup> with dimensions of 4x4.</p> <p><b>Complies:</b> Private open space has been located to ensure that sufficient sunlight is received in the required hours and is directly connected to the living area of each unit. Furthermore, private open space will be fenced to ensure privacy and will not be within proximity of other features.</p>

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Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
 <p><b>Figure 7</b></p>		
<p>PO<sub>10</sub> All car parking is designed to accommodate a standard car (B85 vehicle) and incorporates adequate manoeuvring area to parking spaces (including carports and garages).</p>	<p>AO<sub>10.1</sub> Design of the site layout must ensure a B85 vehicle (as defined in AS 2890.1:2004 Parking facilities - Off-street car parking) is able to enter an enclosed garage or visitor car park in a single forward manoeuvre and exit in a single reverse manoeuvre.</p>	<p><b>Complies:</b> The proposed development ensures that B85 vehicles can enter and exit the site in a single manoeuvre; refer to the Vehicle Swept Path Diagrams attached at <b>Appendix F</b>.</p>
<p>PO<sub>11</sub> Development provides resident and visitor car parking which is integrated into the site and building design and:</p> <ul style="list-style-type: none"> <li>(a) does not negatively impact the amenity of the site, adjoining sites or streetscape;</li> <li>(b) is not a prominent visual feature from the street or other public spaces; and</li> </ul>	<p>AO<sub>11.1</sub> Parking is only provided within the front setback area where it is:</p> <ul style="list-style-type: none"> <li>(a) located on the driveway directly in front of a carport or garage; and</li> <li>(b) not covered.</li> </ul> <p>Note: Where car parking is provided in accordance with AO<sub>11.1</sub>(a) the driveway should have a</p>	<p><b>Complies:</b> Parking for both units will not be provided within the front setback.</p>

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**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(c) contributes to the establishment of a garden setting by avoiding large areas of visible hardstand.</p>	<p>minimum length of 5 metres to avoid parked vehicles extending beyond the site boundary.</p> <p>AO<sub>11.2</sub> Where car parking is provided below ground:</p> <p>(a) Access ramps are not located within 6m of the street boundary and do not exceed an opening width of 6m; and</p> <p>(b) Entry to the site for pedestrians is provided separate from vehicle entries by way of a safe, well lit and clearly identifiable pathway or gatehouse;</p> <p>AO<sub>11.3</sub> All car parking is freely available for use by to the applicable occupants and visitors of the development.</p> <p>Note: See Figure 8.</p>	<p><b>N/A:</b> The proposed development does not involve below ground parking.</p> <p><b>Complies:</b> Carparking will be freely available for use by residents.</p>

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

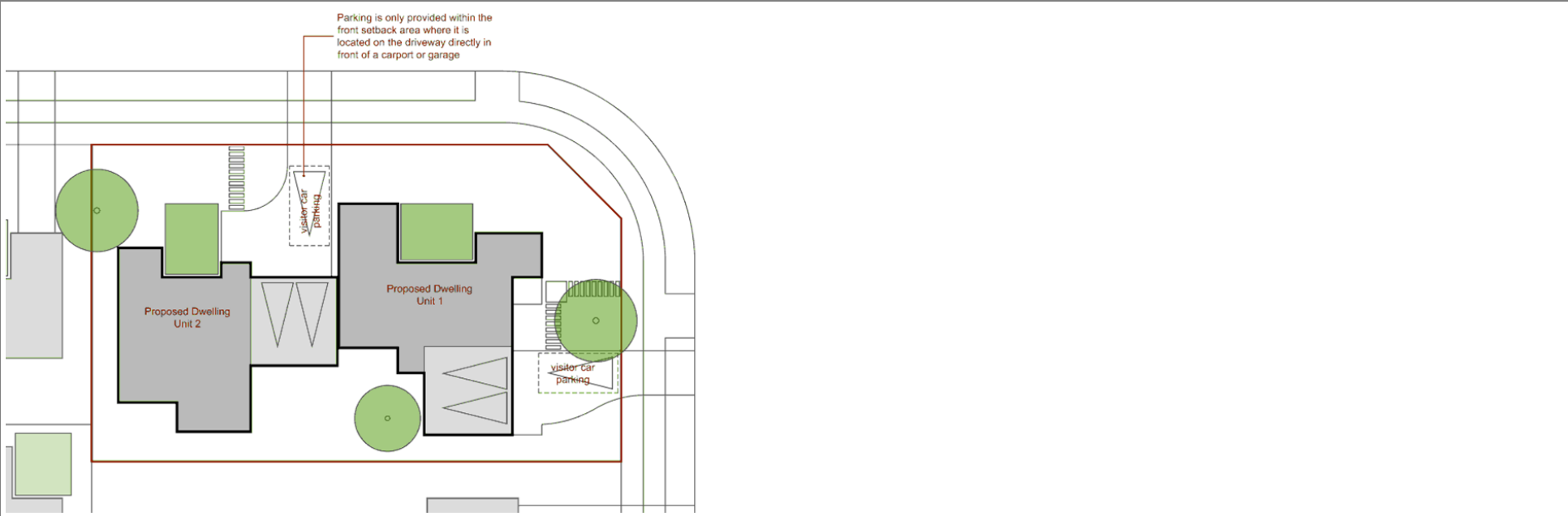
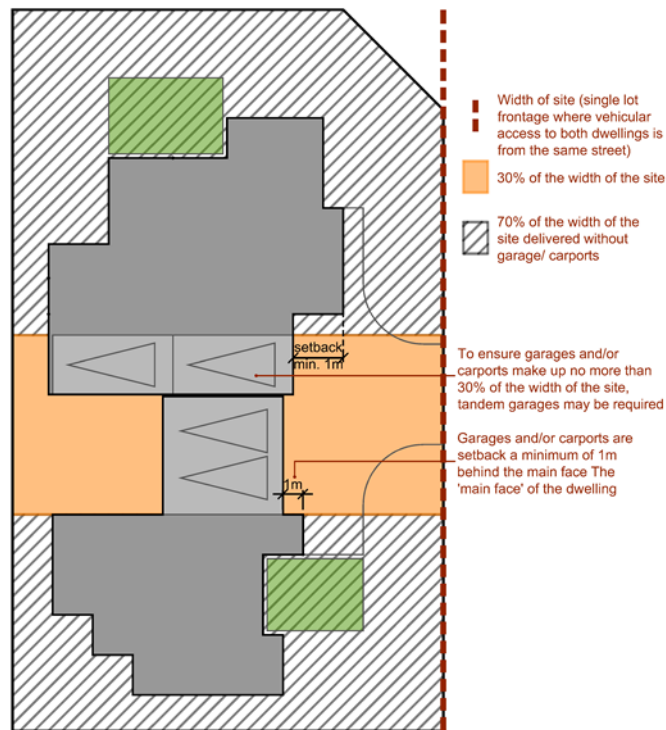
Performance outcomes	Acceptable outcomes	Compliance summary
		
<p>PO<sub>12</sub> Garages and carports (except those with frontage to a rear lane) do not dominate the street frontage.</p>	<p>AO<sub>12.1</sub> Garages and/or carports facing a street make up no more than 30% of the width of the site.            AO<sub>12.2</sub> Garages and/or carports facing a street are setback a minimum of 1 metre behind the main face of the dwelling.            Note: See Figures 9 and 10.</p>	<p><b>Complies:</b> Garages as part of the proposed development will not take up greater than 30% of the frontage width. Both garages are oriented to ensure that the garage opening does not front the street and the garage wall has been articulated with residential features such as windows and a gable roof to ensure it presents to the street as part of the unit built form. To</p>

Figure 8

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
		<p>this end it is noted that landscaping is provided which will soften the visual appearance of the garage. Furthermore, gabling in the roofing for the first storey creates visual interest which diverts attention from the garage.</p>



Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

Figure 9

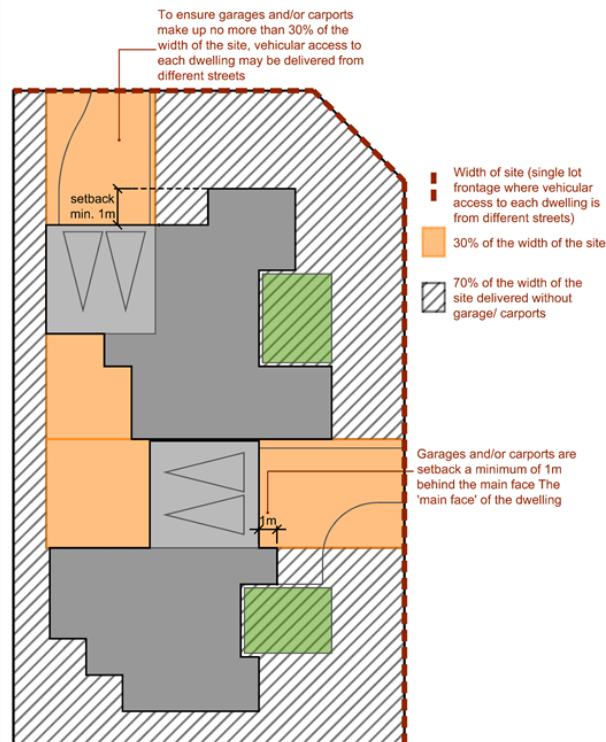


Figure 10

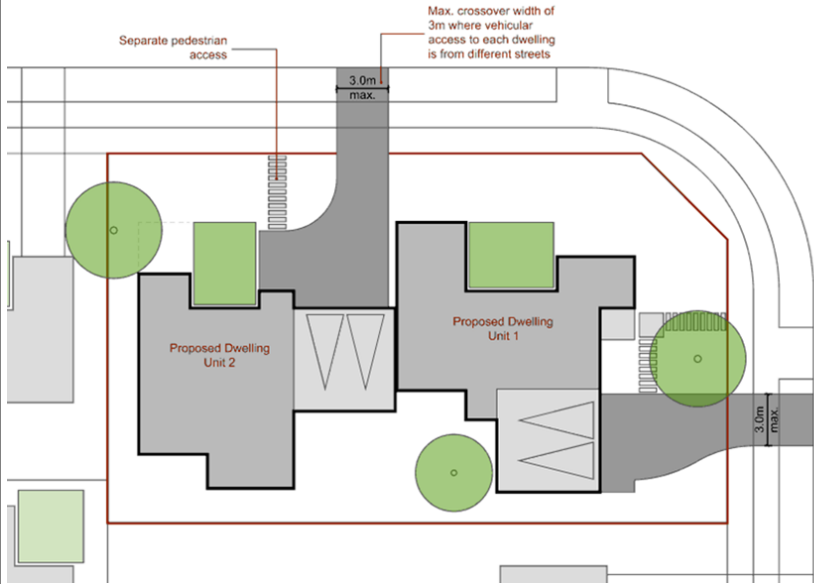
Toowoomba Regional Planning Scheme 2012 (v28.0)

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>13</sub> Safe and equitable access for both pedestrians and vehicles is provided, to and from buildings and the street whilst minimising the extent and number of driveways.</p>	<p>AO<sub>13.1</sub> Where vehicular access to both dwellings is from the same street the driveway is shared and has a maximum crossover width of 5m.</p> <p>AO<sub>13.2</sub> Where vehicular access to each dwelling is from different streets the driveway has a maximum crossover width of 3m.</p> <p>AO<sub>13.3</sub> Where the site abuts a lane vehicle access is provided from the lane.</p> <p>AO<sub>13.4</sub> Separate pedestrian access is provided to each dwelling, clearly distinguishable from the vehicle zone.</p> <p>Note: See Figure 11.</p>	<p><b>Complies:</b> Both units are provided with vehicular access from Short Street. The shared driveway has a maximum crossover width of 5m. A separate pedestrian access is provided to each unit from Short Street.</p>

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Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
 <p>The diagram illustrates a site plan for two proposed dwelling units, Unit 1 and Unit 2. Unit 1 is on the right and Unit 2 is on the left. They are connected by a central access area. A separate pedestrian access is shown on the left. A crossover area is shown at the top with a maximum width of 3.0m. A note states: 'Max. crossover width of 3m where vehicular access to each dwelling is from different streets'. Another note indicates a '3.0m max.' setback for the crossover area. The units are shown with various setbacks and access points.</p>		
<p><b>Figure 11</b></p> <p>PO<sub>14</sub> The privacy of residents of the development or adjoining premises is protected when they are inside a dwelling or in a private open space area.</p>	<p>AO<sub>14.1</sub> Habitable room windows of a dwelling are separated a minimum of 9m from a habitable room window or private open space of another dwelling on the same or an adjoining site.</p> <p>OR</p> <p>AO<sub>14.2</sub> Where there is a direct view within 9m into a habitable room or private open space of</p>	<p><b>Complies:</b> It is noted that habitable rooms on the first storey of each unit will not have direct view into habitable rooms and private open space of adjoining premises.</p> <p><b>N/A:</b> Refer to response to AO<sub>14.1</sub>.</p>

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Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>another dwelling on the same or an adjoining site</p> <ul style="list-style-type: none"> <li>(a) the elements with the view such as windows, balconies, or terraces of a dwelling must be screened in accordance with AO14.3; or</li> <li>(b) the Windows have translucent glazing up to or sill heights of at least 1.5m from the internal floor level; or</li> <li>(c) a solid fence or masonry wall with a minimum height of 1.5m is positioned between the elements with the view.</li> </ul> <p>AO14.3 Where screening is used, it:</p> <ul style="list-style-type: none"> <li>(a) is a solid translucent screen or a louvre perforated panel, trellis or the like that has a maximum of 50% open to solid ratio; and</li> <li>(b) is a fixed, permanent element of a design that complements the style, detailing and materials of the development.</li> </ul> <p>Note: See Figure 12.</p>	<p><b>Complies:</b> Where screening is used, it will be designed in accordance with AO14.3.</p>

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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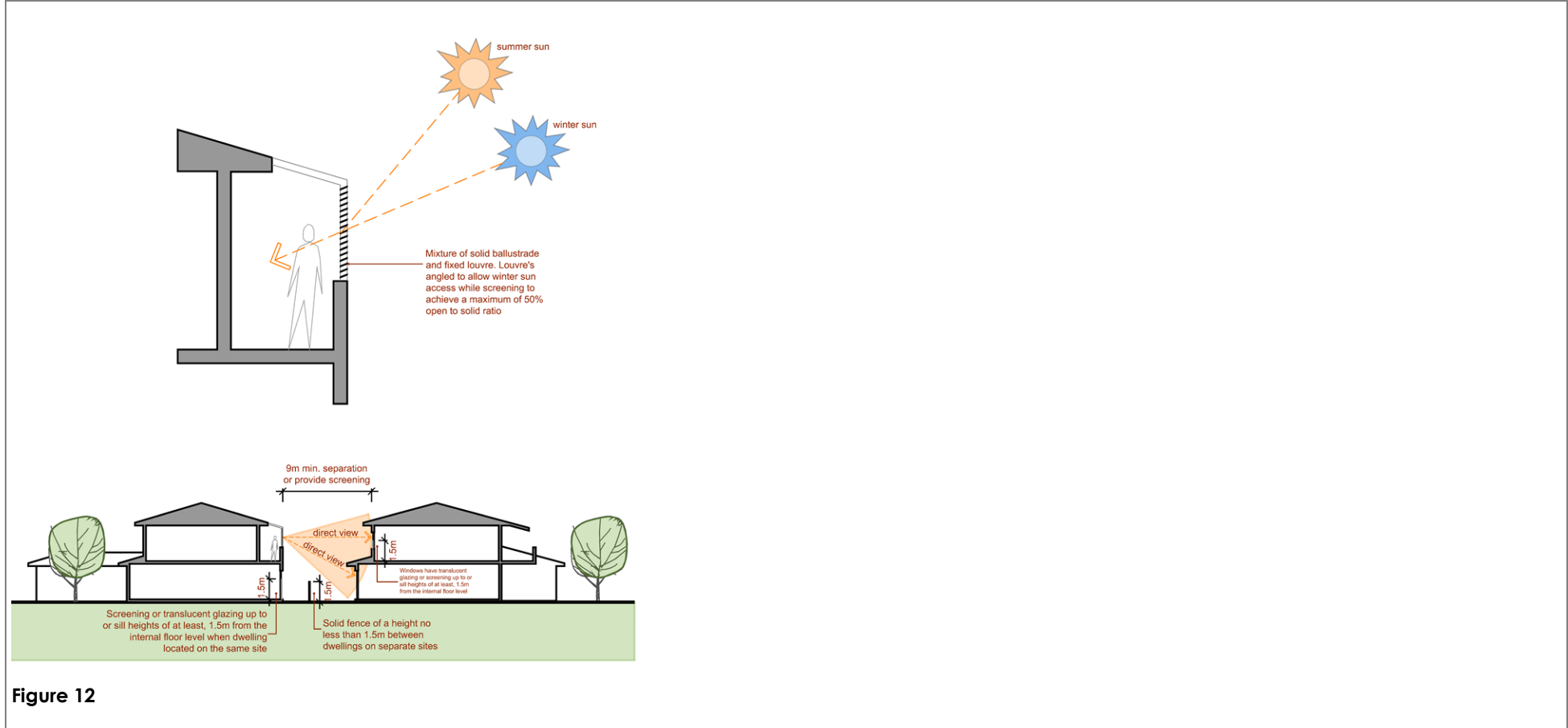


Figure 12

Toowoomba Regional Planning Scheme 2012 (v28.0)

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>15</sub> The development is designed so that occupants are not adversely affected by adjoining non-residential uses.</p>	<p>AO<sub>15.1</sub> A 1.8m high solid screen fence and 2m wide vegetated buffer is provided along any section of the site boundary that is common with vehicle movement and/or parking areas associated with a non-residential use on an adjoining site.</p>	<p><b>N/A:</b> The site does not adjoin a non-residential use.</p>
<p>PO<sub>16</sub> Buildings are designed to a high aesthetic standard providing:</p> <ul style="list-style-type: none"> <li>(a) prominent features towards the street frontage;</li> <li>(b) detailing and finishes appropriate to the scale, quality and character of the street and to help differentiate between dwellings</li> <li>(c) opportunities for informal surveillance of the street and other public spaces;</li> <li>(d) clear and visible entries; and elements that complement or enhance the character of the local neighbourhood.</li> </ul>	<p>AO<sub>16.1</sub> Buildings:</p> <ul style="list-style-type: none"> <li>(a) are oriented to and address the street frontage;</li> <li>(b) have front entries that provide full weather protection and where within 10m of any street, have clearly defined front entries that are visible from the street;</li> <li>(c) where within 10m of any street; have one or more habitable rooms facing the street boundary that have windows or doors with a minimum area of clear glazing of 0.25m<sup>2</sup> for each linear metre of street frontage; and</li> <li>(d) are designed to deliver depth in the façade facing the street or public space with a variation not less than 2m deep for a minimum 50% of the building width at ground level.</li> </ul> <p>Note: Variation in building façade may be achieved through delivery of balconies, patios or building footprint articulation. The roofline over a balcony or patio may remain consistent with the dominant roof form.</p> <p>Note: See Figure 13.</p>	<p><b>Performance Solution:</b> The proposed development has been designed to exhibit a high aesthetic standard and will feature defined entries which address the respective street frontage. Furthermore, the proposed development has been designed to utilise elements which ensure it integrates with the established character of the street.</p>

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Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO17 Development incorporates typical domestic detailing to achieve a residential scale living environment with aesthetic standards and amenity for occupants similar to those of single dwellings in the neighbourhood.</p>	<p>AO17.1 The maximum single wall length of a building is 32m with offsets of 600mm minimum provided every 7.5m;</p> <p>Note: Building walls separated by a distance of less than 10% of their combined length are considered as one wall.</p>	<p><b>N/A:</b> The proposed development does not involve a wall greater than 32m in length.</p>

Figure 13

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>17.2</sub> Building bulk is minimised through articulation and other measures, including the provision of at least one (1) element from at least three (3) of the following element groups:</p> <ul style="list-style-type: none"> <li>(a) verandahs, porches, pergolas or balconies;</li> <li>(b) roof overhangs (minimum 600mm deep);</li> <li>(c) window hoods/screens;</li> <li>(d) awning and shade structures; or</li> <li>(e) 600mm vertical gutter offset to create roof articulation.</li> </ul> <p>Note: See Figure 14.</p>	<p><b>Complies:</b> The proposed development incorporates elements such as balconies, porches, roof overhangs and gutter offsets to create articulation and soften the overall impact of building bulk.</p>

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary



Figure 14

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**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>18</sub> Development provides landscaping that contributes to the landscape character, shade and the micro-climate of the neighbourhood and site through the retention or provision of at least one canopy tree.</p>	<p>AO<sub>18.1</sub> One (1) tree capable of achieving a height of at least six (6) metres and canopy spread of at least five (5) metres at maturity is provided.</p>	<p><b>Complies:</b> The proposed development will incorporate two new trees within the front setback of the units as per the conceptual details on the Site Plan at <b>Appendix D</b>. It is also noted that the proposed development has been designed to retain existing streets along Railway Street.</p>
<p>PO<sub>19</sub> The treatment of fencing and retaining walls in all situations reinforces the high quality, amenity and safety of the development.</p>	<p>AO<sub>19.1</sub> The maximum total height of a combined fence and retaining wall is 2.1m, as measured from the base of the retaining wall to the top of the fence.</p> <p>AO<sub>19.2</sub> Where a combined fence and retaining wall is located on the front boundary, the fence has a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>AO<sub>19.3</sub> Retaining walls and fences are constructed of high quality materials and contribute to residential amenity.</p> <p>AO<sub>19.4</sub> Any fence on the front boundary has a minimum 30% transparency when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>Note: Where retaining walls are set off the property boundary by a minimum of 600mm, they are not considered to be combined with the fencing. (See Figure 15.)</p>	<p><b>N/A:</b> The proposed development does not involve a combined fence and retaining wall.</p> <p><b>N/A:</b> The proposed development does not involve a combined fence and retaining wall.</p> <p><b>N/A:</b> The proposed development does not involve a combined fence and retaining wall.</p> <p><b>Complies:</b> Fencing provided on the front boundary will have a minimum transparency of 30%.</p>

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Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

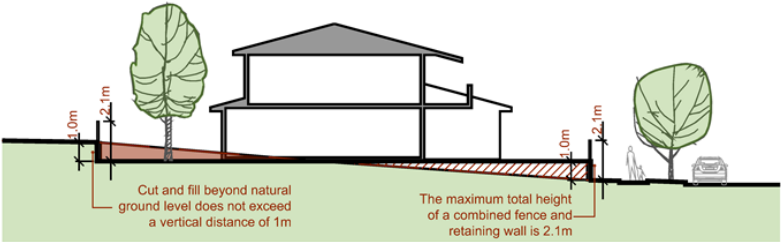
Performance outcomes	Acceptable outcomes	Compliance summary
 <p data-bbox="412 746 568 794">Cut and fill beyond natural ground level does not exceed a vertical distance of 1m</p> <p data-bbox="696 756 853 804">The maximum total height of a combined fence and retaining wall is 2.1m</p>		
<p>PO<sub>20</sub> Front fencing contributes to privacy, screening, security, and street character while maintaining a visual relationship between dwellings and public spaces.</p>	<p>AO<sub>20.1</sub> Fences located on a front property boundary or a common boundary with an adjacent park or within the front setback area:</p> <ul style="list-style-type: none"> <li>(a) have a maximum height of 1.2 metres; or</li> <li>(b) have a maximum height of 1.5 metres and a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).</li> </ul> <p>Note: In instances where the fence is located in conjunction with a retaining wall refer PO<sub>19</sub>.</p>	<p><b>Complies:</b> Fencing provided along the front boundary will have a height of 1.2m, other than where it screens the POS of Unit 1.</p>
<p>PO<sub>21</sub> Stormwater discharge from the site is to a lawful point of discharge as defined in the Queensland Urban Drainage Manual (QUDM), without the use of pumped or charged pipe systems, and not to private land other than to an easement for stormwater purposes befitting the site and</p>	<p>AO<sub>21.1</sub> The site:</p> <ul style="list-style-type: none"> <li>(a) has a natural surface with an elevation that is higher than the abutting road and enables stormwater to drain gravitationally to the abutting road via subsurface pipes; or</li> </ul>	<p><b>Complies:</b> The proposed development will be provided with stormwater drainage which ensures that stormwater is able to be lawfully discharged in accordance with the relevant standards.</p>

Figure 15

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**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>allowing discharge to a lawful point of discharge in land over which Council has tenure or control.</p>	<p>(b) has a natural surface with an elevation that is higher than Council's stormwater drainage network in the abutting road and enables stormwater to drain gravitationally to the stormwater drainage network in the abutting road via subsurface pipes; or (c) has lawful access to an inter allotment drainage network with available capacity to meet the requirements of the development.</p>	
<p><b>Where a Dwelling House on a Hatchet Lot</b></p>		
<p>PO<sub>22</sub> The configuration and finish of the access strip/easement: (a) enhances the streetscape qualities of the area and any adjoining premises; (b) enhances the residential amenity of the area; and (c) contributes to a high quality residential outcome for the site, by the incorporation of landscaping, provision for adequate setbacks and use of high quality materials.</p>	<p>AO<sub>22.1</sub> Any area of hardstand, including the driveway, within the access strip / easement: (a) has a maximum width of 3m; and (b) is sealed with a rigid pavement. AO<sub>22.2</sub> The access strip/easement has a landscaped area/s with a combined minimum width of 2m for its full length. AO<sub>22.3</sub> The driveway landscaping must continue for the full length of the driveway for a minimum width of 0.5m. AO<sub>22.4</sub> Any fence on the inside boundary of the access strip / easement has a maximum height of one (1) metre for the first six (6) metres from the front boundary.</p>	<p><b>N/A:</b> The proposed development is for a Dual Occupancy.</p>
<p>PO<sub>23</sub> The dwelling does not result in a reduction of amenity, privacy, or loss of solar access for any adjoining property.</p>	<p>AO<sub>23.1</sub> The dwelling does not exceed a maximum height of 2 storeys or 8.5m above ground level. AO<sub>23.2</sub> The dwelling is setback a minimum of 2m from all boundaries.</p>	<p><b>N/A:</b> The proposed development is for a Dual Occupancy.</p>

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**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	AO <sub>23.3</sub> North-facing windows to habitable rooms and/or the private outdoor space of an adjoining premises does not have sunlight reduced to less than 3 hours of solar gain between 9 a.m. and 3 p.m. on 21 June.	
<p>PO<sub>24</sub> Stormwater discharge from the site is to a lawful point of discharge as defined in the Queensland Urban Drainage Manual (QUDM), without the use of pumped or charged pipe systems, and not to private land other than to an easement for stormwater purposes befitting the site and allowing discharge to a lawful point of discharge in land over which Council has tenure or control.</p> <p>Note: Land over which Council has tenure or control does not include Council's open space network.</p>	<p>AO<sub>24.1</sub> The site:</p> <ul style="list-style-type: none"> <li>(a) has a natural surface with an elevation that is higher than the abutting road and enables stormwater to drain gravitationally to the abutting road via subsurface pipes; or</li> <li>(b) has a natural surface with an elevation that is higher than Council's stormwater drainage network in the abutting road and enables stormwater to drain gravitationally to the stormwater drainage network in the abutting road via subsurface pipes; or</li> <li>(c) has lawful access to an inter allotment drainage network with available capacity to meet the requirements of the development.</li> </ul>	<p><b>N/A:</b> The proposed development is for a Dual Occupancy.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

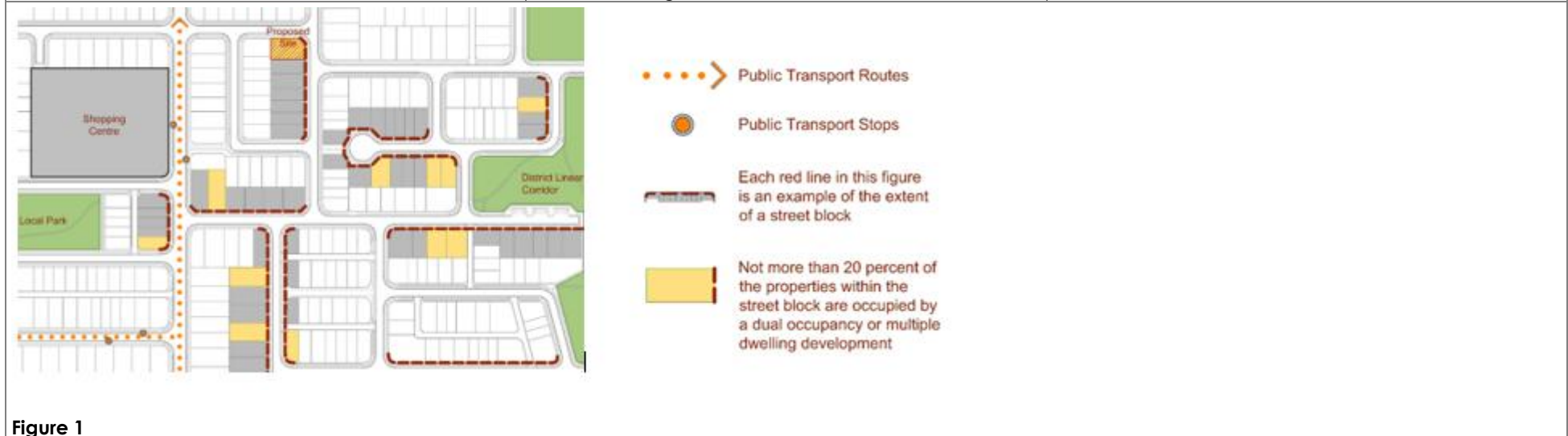
Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>1</sub> The development is designed in a manner that responds to the sites opportunities and constraints and to the broader urban and streetscape contexts.</p> <p>Note: Development should be designed and documented by a suitably qualified professional (i.e. architect or building designer). Details of professional qualifications should be noted on drawings submitted with any development application</p>	<p>AO<sub>1.1</sub> The design of the development considers and responds to:</p> <p>(a) Site specific qualities including:</p> <ul style="list-style-type: none"> <li>(i) site topography and slope (contours at a minimum of 1m intervals);</li> <li>(ii) Views</li> <li>(iii) solar aspect and access;</li> <li>(iv) prevailing breezes</li> <li>(v) existing buildings on the site;</li> <li>(vi) site access</li> <li>(vii) services</li> <li>(viii) identification of buildings to be demolished;</li> <li>(ix) existing vegetation (locations to be surveyed and the diameter of the trunk at 1m above ground level shown. On highly buttressed trees the extent of buttressed roots is to be shown);</li> <li>(x) proposed position of new buildings overlaid</li> </ul> <p>(b) Nearby features including:</p> <ul style="list-style-type: none"> <li>(i) opportunities for on street visitor car parking;</li> <li>(ii) buildings on adjoining land</li> <li>(iii) important views from neighbouring properties</li> <li>(iv) features and character of adjoining land and buildings including doors &amp; windows</li> <li>(v) street fixtures;</li> </ul> <p>(c) Local qualities and amenities including:</p> <ul style="list-style-type: none"> <li>(i) available public transport; and</li> </ul>	<p><b>Complies:</b> Refer to response to AO<sub>2.1</sub> of Table 9.3.7:1.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	(ii) nearby amenities such as parks, shops. AO <sub>1.2</sub> Site design retains all existing street trees. Note: See Figures 1 and 2.	<b>Complies:</b> Refer to response to AO <sub>2.2</sub> of Table 9.3.7:1.



Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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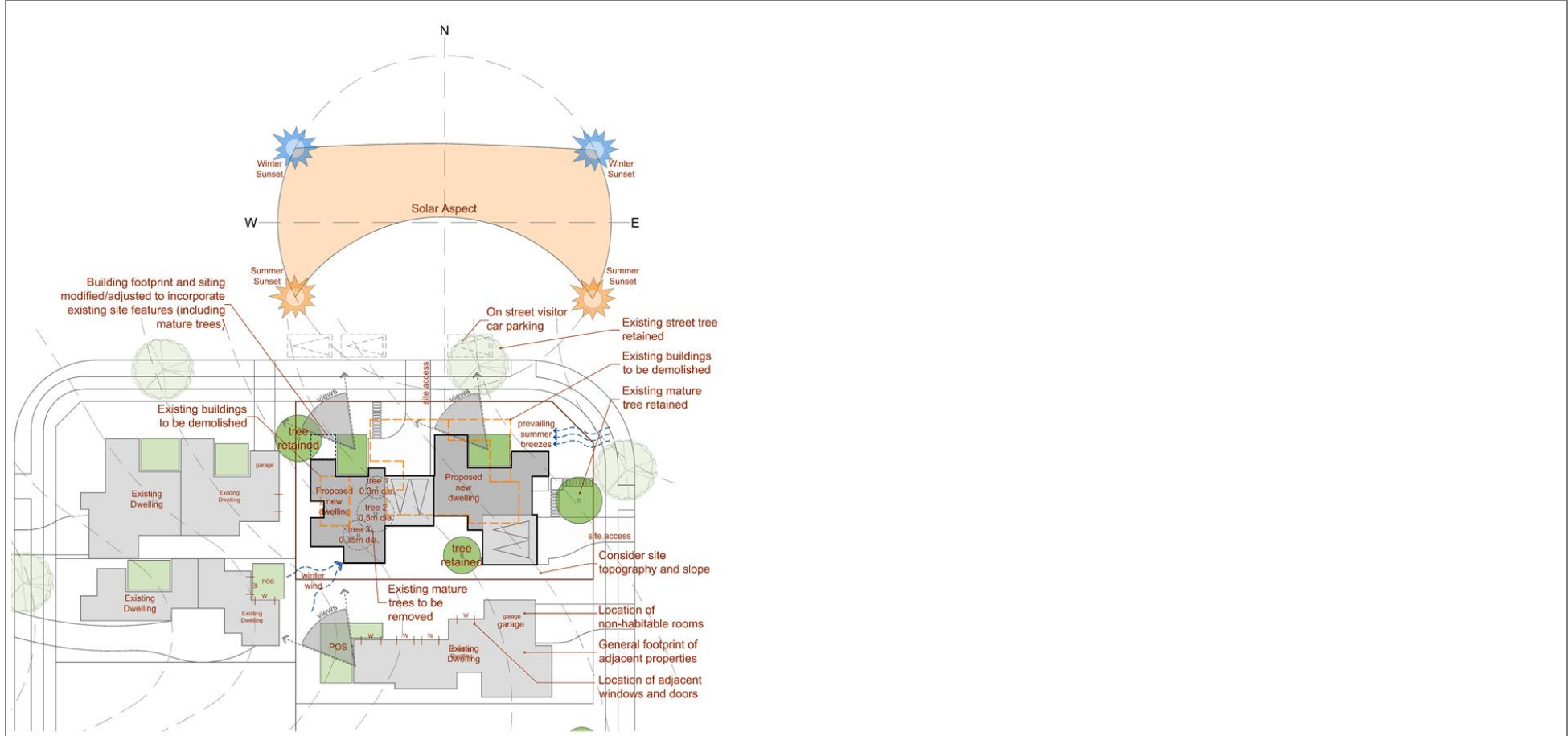


Figure 2

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>2</sub> Development responds to the slope of the land in the siting, design and form of buildings and structures to minimise requirements for cut and fill.</p>	<p>AO<sub>2.1</sub> Cut and fill beyond natural ground level does not exceed a vertical distance of 1m, unless contained within building walls and in accordance with the requirements outlined in SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure. The extent of cut and fill must be shown on the proposed site plan and elevations.</p> <p>AO<sub>2.2</sub> Batters and earth-retaining structures are constructed in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.</p> <p>Note: See Figure 3.</p>	<p><b>Complies:</b> Refer to response to AO<sub>3.1</sub> of Table 9.3.7:1.</p> <p><b>Complies:</b> Refer to response to AO<sub>3.2</sub> of Table 9.3.7:1.</p>

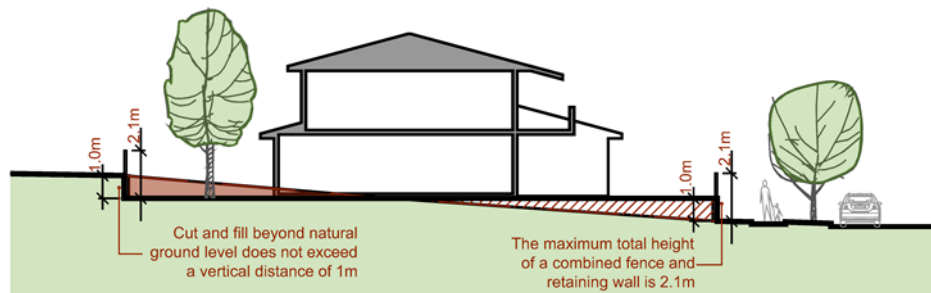


Figure 3

<p>PO<sub>3</sub> Development is designed to facilitate the retention of any existing street tree adjacent to the site.</p>	<p>AO<sub>3.1</sub> Development does not compromise or require the removal of any street tree.</p>	<p><b>Complies:</b> Refer to response to AO<sub>4.1</sub> of Table 9.3.7:1.</p>
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary																						
<p>PO<sub>4</sub> Development:</p> <ul style="list-style-type: none"> <li>(a) provides for the on-site infiltration of stormwater and minimises the additional burden on drainage infrastructure;</li> <li>(b) minimises the visual impact of hard surface areas; and</li> </ul> <p>respects the existing or preferred neighbourhood character and responds to the features of the site.</p>	<p>AO<sub>4.1</sub> The impervious site cover does not exceed Table 1:</p> <table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr> <th colspan="2">Table 1</th> </tr> <tr> <th>Average building height</th> <th>Maximum impervious site cover</th> </tr> </thead> <tbody> <tr> <td>If more than 50% of the proposed dwellings are 1 storey</td> <td>70%</td> </tr> <tr> <td>If more than 50% of the proposed dwellings are 2 storeys</td> <td>65%</td> </tr> <tr> <td>If more than 50% of the proposed dwellings are 3 or more storeys</td> <td>60%</td> </tr> </tbody> </table> <p>AO<sub>4.2</sub> No more than 50% of the front setbacks area is impervious.</p> <p>AO<sub>4.3</sub> Site cover does not exceed rates specified in Table 2.</p> <table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr> <th colspan="2">Table 2</th> </tr> <tr> <th>Dwelling Height</th> <th>Maximum site cover</th> </tr> </thead> <tbody> <tr> <td>If more than 50% of proposed dwellings are 1 storey</td> <td>50%</td> </tr> <tr> <td>If more than 50% of proposed dwellings are 2 storeys</td> <td>45%</td> </tr> <tr> <td>If more than 50% of proposed dwellings are 3 storeys</td> <td>40%</td> </tr> <tr> <td>If more than 50% of proposed dwellings are 4 storeys or more</td> <td>35%</td> </tr> </tbody> </table> <p>Note: See Figure 4.</p>	Table 1		Average building height	Maximum impervious site cover	If more than 50% of the proposed dwellings are 1 storey	70%	If more than 50% of the proposed dwellings are 2 storeys	65%	If more than 50% of the proposed dwellings are 3 or more storeys	60%	Table 2		Dwelling Height	Maximum site cover	If more than 50% of proposed dwellings are 1 storey	50%	If more than 50% of proposed dwellings are 2 storeys	45%	If more than 50% of proposed dwellings are 3 storeys	40%	If more than 50% of proposed dwellings are 4 storeys or more	35%	<p><b>Complies:</b> Refer to response to AO<sub>5.1</sub> of Table 9.3.7:1.</p> <p><b>Complies:</b> Refer to response to AO<sub>5.2</sub> of Table 9.3.7:1.</p> <p><b>Complies:</b> Refer to response to AO<sub>5.3</sub> of Table 9.3.7:1.</p>
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Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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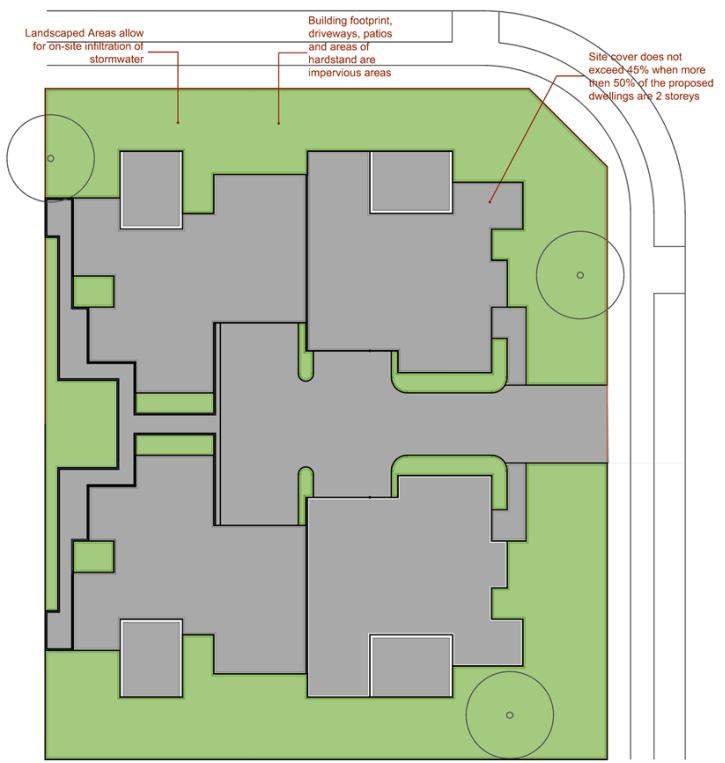


Figure 4

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary																	
<p>PO<sub>5</sub> Areas of hardstand required for parking and pathways:</p> <ul style="list-style-type: none"> <li>(a) are minimised,</li> <li>(b) Incorporate generous landscaping:               <ul style="list-style-type: none"> <li>(i) to promote a garden streetscape character, and</li> <li>(ii) (to minimise visual impacts of views to hardstand areas for occupants and the public, particularly in the front garden.</li> </ul> </li> </ul>	<p>AO<sub>5.1</sub> Landscaped areas are provided in accordance with Table 3:</p> <table border="1" data-bbox="891 539 1346 1177"> <thead> <tr> <th colspan="3">Table 3</th> </tr> <tr> <th rowspan="2">Building height</th> <th colspan="2">Minimum landscaped area</th> </tr> <tr> <th>Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:</th> <th>Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:</th> </tr> </thead> <tbody> <tr> <td>1-2 storeys</td> <td>10% site area</td> <td>15% site area</td> </tr> <tr> <td>3 storeys</td> <td>12.5% site area</td> <td>17.5% site area</td> </tr> <tr> <td>4+ storeys</td> <td>15% site area</td> <td>20% site area</td> </tr> </tbody> </table> <p>Note: Landscaped areas included in the calculations consist of garden beds and lawn areas throughout the site with a width of no less than 1.5m; Excludes all paved and other impervious areas such as driveways, patios, terraces, car parking spaces and bin and other utility areas. See Figure 5.</p>	Table 3			Building height	Minimum landscaped area		Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:	Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:	1-2 storeys	10% site area	15% site area	3 storeys	12.5% site area	17.5% site area	4+ storeys	15% site area	20% site area	<p><b>Complies:</b> Refer to response to AO<sub>6.1</sub> of Table 9.3.7:1.</p>
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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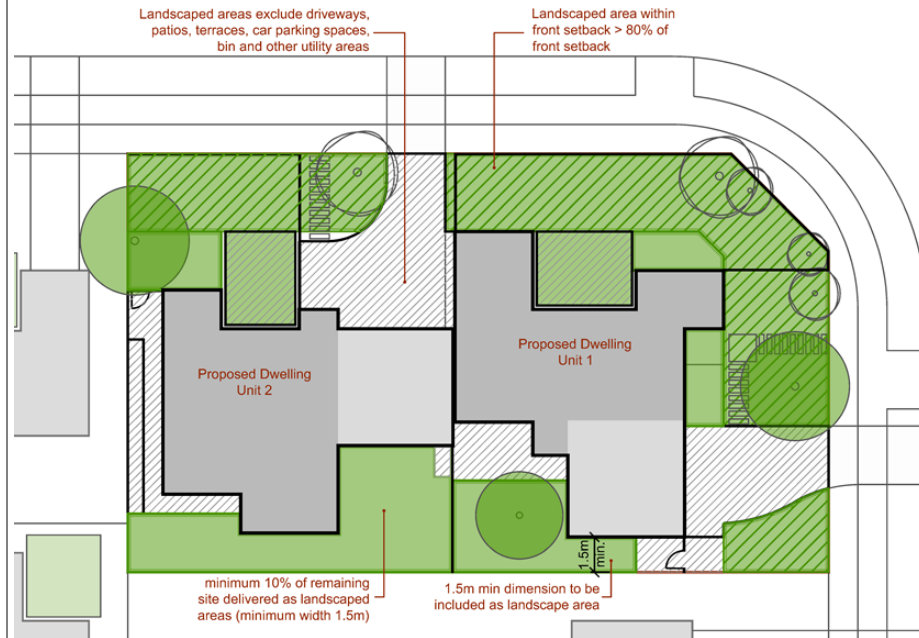


Figure 5

<p>PO<sub>6</sub> The front building setback is consistent with the prevailing front setbacks of other residential buildings in the street.</p> <p>Note: The prevailing setback of the street is determined by the setbacks of the residential buildings on any lot along the road within 100m of the site.</p>	<p>AO<sub>6.1</sub> Buildings are setback from the road frontage:</p> <ul style="list-style-type: none"> <li>(a) within 10% of the average front setback of existing residential buildings within 100m of the site; or</li> <li>(b) where there is no adjoining buildings the front setback is provided in accordance with Table 4.</li> </ul>	<p><b>Complies:</b> Refer to response to AO<sub>7.1</sub> of Table 9.3.7:1.</p>
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary										
	<table border="1"> <thead> <tr> <th colspan="2" data-bbox="853 480 1379 528">Table 4</th> </tr> <tr> <th data-bbox="853 528 1137 584">Building element</th> <th data-bbox="1137 528 1379 584">Minimum setback</th> </tr> </thead> <tbody> <tr> <td data-bbox="853 584 1137 663">Solid building wall (other than a garage)</td> <td data-bbox="1137 584 1379 663">4.0m</td> </tr> <tr> <td data-bbox="853 663 1137 759">Solid building wall (where for a garage)</td> <td data-bbox="1137 663 1379 759">5.5m</td> </tr> <tr> <td data-bbox="853 759 1137 895">Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)</td> <td data-bbox="1137 759 1379 895">3.5m (or 3m where secondary road frontage)</td> </tr> </tbody> </table> <p data-bbox="801 906 1048 932">Note: See Figure 6.</p>	Table 4		Building element	Minimum setback	Solid building wall (other than a garage)	4.0m	Solid building wall (where for a garage)	5.5m	Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)	3.5m (or 3m where secondary road frontage)	
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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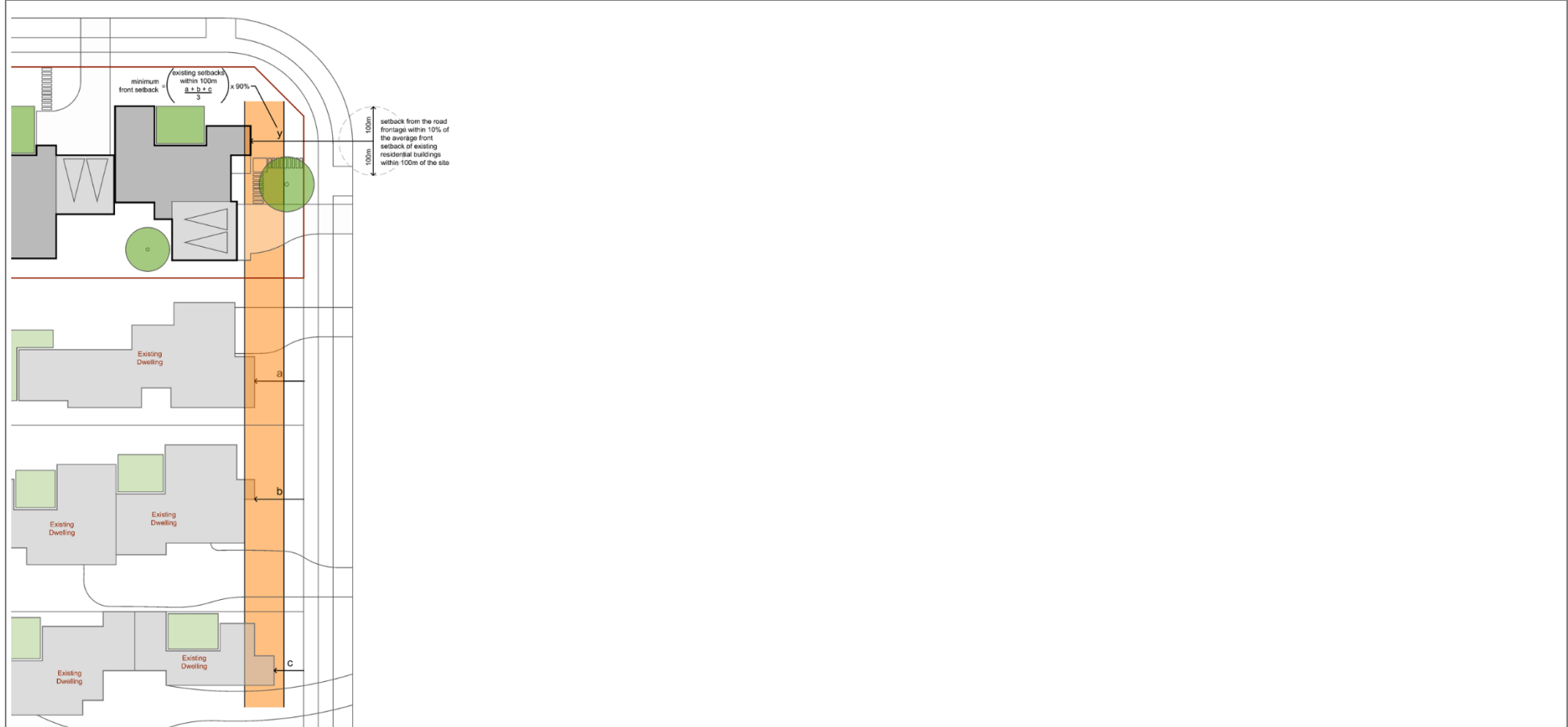


Figure 6

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>7</sub> Side and rear building setbacks:</p> <ul style="list-style-type: none"> <li>(a) are appropriate to the scale of the development and the intended character of the zone and/or precinct or local plan in which the site is located;</li> <li>(b) provide for adequate daylight for habitable rooms and open space areas for buildings on and adjoining the site; and</li> <li>(c) are sufficient to minimise overshadowing and overlooking of adjoining premises.</li> </ul>	<p>In partial fulfilment of the performance outcome</p> <p>AO<sub>7.1</sub> Any part of a building built to a side boundary:</p> <ul style="list-style-type: none"> <li>(a) must not exceed a maximum height of 3.5m</li> <li>(b) must not exceed a maximum length of 9m;</li> <li>(c) must be a minimum distance of 2.4m from any other wall built to the side boundary (whether on the site or an adjoining site) or be joined to another built to boundary wall if on an adjoining site; and</li> <li>(d) must not exceed a maximum total length of 50% of the side boundary.</li> </ul> <p>OR</p> <p>AO<sub>7.2</sub> Buildings are set back from side boundaries in accordance with the following:</p> <ul style="list-style-type: none"> <li>(a) For lots with frontages less than 20m: <ul style="list-style-type: none"> <li>(i) 1.2m for any part of the building up to 3.5m high; and</li> <li>(ii) 2m for any part of the building above 3.5m high.</li> </ul> </li> <li>(b) For lots with frontages greater than 20m: <ul style="list-style-type: none"> <li>(i) 1.8m for any part of the building up to 3.5m high; and</li> <li>(ii) 2.4m for any part of the building above 3.5m high.</li> </ul> </li> </ul> <p>AO<sub>7.3</sub> Buildings are set back from the rear boundary a minimum of 2m.</p>	<p><b>N/A:</b> Refer to response to AO<sub>8.1</sub> of Table 9.3.7:1.</p> <p><b>Complies:</b> Refer to response to AO<sub>8.2</sub> of Table 9.3.7:1.</p> <p><b>N/A:</b> Refer to response to AO<sub>8.3</sub> of Table 9.3.7:1.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>7.4</sub> Development ensures that solar access to habitable rooms and private open space of adjoining premises: -</p> <ul style="list-style-type: none"> <li>(a) achieves a minimum of 3 hours of sunlight between 9am and 3pm on June 21; or</li> <li>(b) is not further reduced where existing solar access is less than that specified in (a).</li> </ul> <p>Note: See figure 7.</p>	<p><b>Complies:</b> Refer to response to AO<sub>8.4</sub> of Table 9.3.7:1.</p>

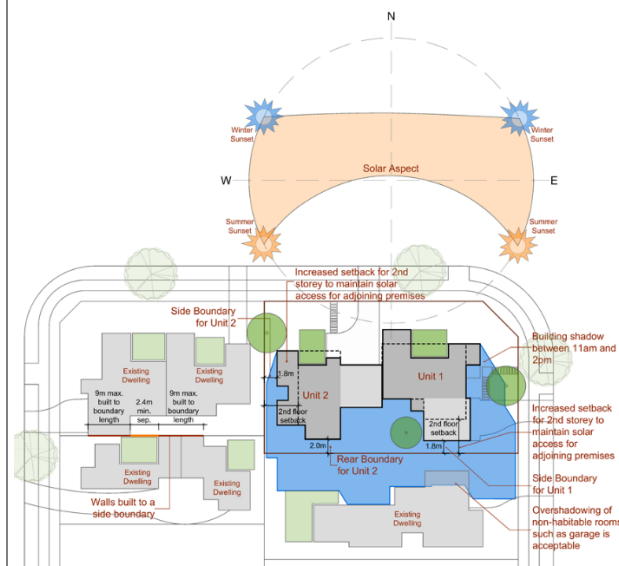


Figure 7



**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(a) is oriented to receive at least three hours of sun between 9am and 3pm on 21 June.</li> <li>(b) directly accessible from a living area or kitchen;</li> <li>(c) fenced or landscaped to achieve privacy for occupants;</li> <li>(d) clear of bins, clothes lines, hot water systems, air conditioning units, above ground water tanks and associated infrastructure and other forms of ancillary equipment; and</li> <li>(e) not located within the front setback area.</li> </ul> <p>Note: See Figure 8.</p>	

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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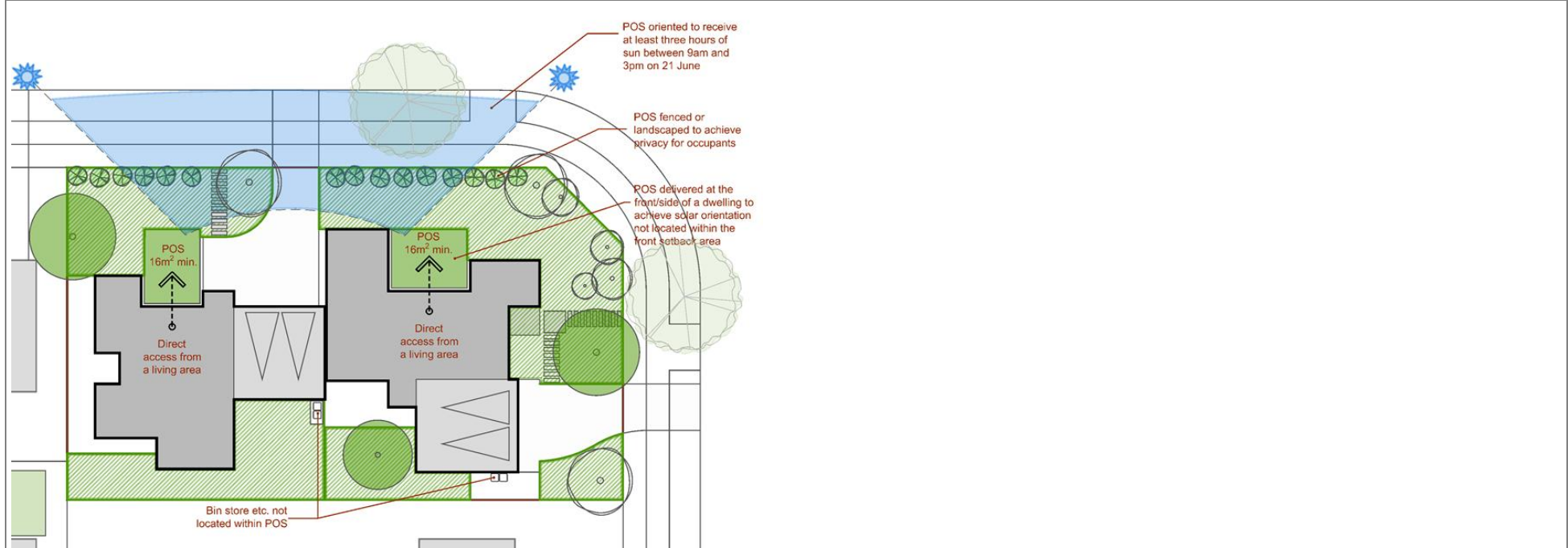
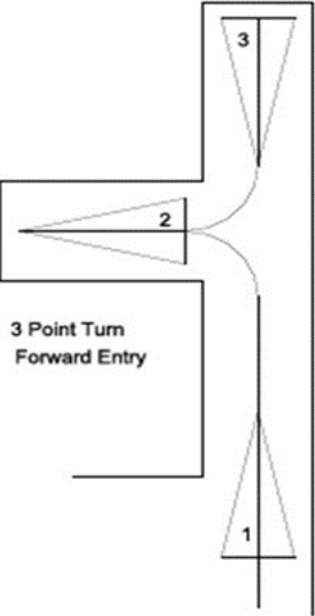


Figure 8

<p>PO<sub>9</sub> All car parking is designed to accommodate a standard car (B85 vehicle for occupant car parking and B99 vehicle for visitor car parking) and: (a) is clearly identifiable and of adequate dimensions;</p>	<p>AO<sub>9.1</sub> Design of the site layout must ensure a B85 vehicle (as defined in AS 2890.1:2004 Parking facilities - Off-street car parking) is able to enter an enclosed garage or visitor car park in a single forward manoeuvre and exit in a single reverse manoeuvre prior to departing the site in a forward direction.</p>	<p><b>Complies:</b> Refer to response to AO<sub>10.1</sub> of Table 9.3.7:1.</p>
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary														
<p>(b) incorporates adequate manoeuvring area to parking spaces (including carports and garages); and</p> <p>(c) incorporates a clear, safe and effective circulation system.</p>  <p>Figure 9</p>	<p>AO<sub>9.2</sub> Design of the site layout must ensure a B99 vehicle (as defined in AS 2890.1:2004 Parking facilities - Off-street car parking) is able to enter a visitor car park in a single forward manoeuvre and exit in a single reverse manoeuvre prior to departing the site in a forward direction.</p> <p>Note: See Figure 9.</p> <p>AO<sub>9.3</sub> Each parking space has minimum dimensions as per Table 7:</p> <table border="1" data-bbox="949 740 1285 1326"> <caption>Table 7</caption> <thead> <tr> <th>Parking Space</th> <th>Minimum Dimensions</th> </tr> </thead> <tbody> <tr> <td>Single uncovered parking space (with at least 400mm clearance between either side of the parking space and the nearest wall.)</td> <td>5.4 metres long by 2.6 metres wide</td> </tr> <tr> <td>Single uncovered parking space (with less than 400mm clearance between either side of the parking space and the nearest wall.)</td> <td>5.4 metres long by 3 metres wide</td> </tr> <tr> <td>Single carport</td> <td>5.4 metres long by 3 metres wide</td> </tr> <tr> <td>Double carport</td> <td>5.4 metres long by 5.8 metres wide</td> </tr> <tr> <td>Single garage</td> <td>6.4 metres long by 3 metres wide internally</td> </tr> <tr> <td>Double garage</td> <td>6.4 metres by 6.4 metres wide internally</td> </tr> </tbody> </table>	Parking Space	Minimum Dimensions	Single uncovered parking space (with at least 400mm clearance between either side of the parking space and the nearest wall.)	5.4 metres long by 2.6 metres wide	Single uncovered parking space (with less than 400mm clearance between either side of the parking space and the nearest wall.)	5.4 metres long by 3 metres wide	Single carport	5.4 metres long by 3 metres wide	Double carport	5.4 metres long by 5.8 metres wide	Single garage	6.4 metres long by 3 metres wide internally	Double garage	6.4 metres by 6.4 metres wide internally	<p><b>N/A:</b> The proposed development does not involve visitor carparks.</p> <p><b>Complies:</b> Each unit will be provided with parking that complies with Table 7. Refer also to the swept paths at <b>Appendix F.</b></p>
Parking Space	Minimum Dimensions															
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>10</sub> Development provides resident and visitor car parking which is integrated into the site and building design and:</p> <ul style="list-style-type: none"> <li>(a) Does not negatively impact the amenity of the site, adjoining sites or streetscape;</li> <li>(b) Is not a prominent visual feature from the street or other public spaces; and</li> <li>(c) Contributes to the establishment of a garden setting by avoiding large areas of visible hardstand.</li> </ul>	<p>AO<sub>10.1</sub> Parking is only provided within the front setback area where it is:</p> <ul style="list-style-type: none"> <li>(a) uncovered and located on a driveway directly in front of a carport or garage; or</li> <li>(b) the front setback is a least 6m, and</li> <li>(c) the car park surface is permeable;</li> <li>(d) a minimum 2m wide densely planted landscape buffer is provided between the car park and the front boundary for the full length of the car park;</li> <li>(e) the location of the car park does not compromise the ability of the dwelling to provide casual surveillance of the public realm; and</li> <li>(f) at least 60% of the front setback area is landscaped area.</li> </ul> <p>AO<sub>10.2</sub> Where car parking is provided below ground:</p> <ul style="list-style-type: none"> <li>(a) Access ramps are not located within 6m of the street boundary and do not exceed an opening width of 6m; and</li> <li>(b) Entry to the site for pedestrians is provided separate from vehicle entries by way of a safe, well lit and clearly identifiable pathway or gatehouse;</li> </ul> <p>AO<sub>10.3</sub> All car parking:</p> <ul style="list-style-type: none"> <li>(a) is clearly signposted; and</li> <li>(b) is freely available for use by to the applicable occupants and visitors of the development.</li> </ul> <p>Note: See Figures 10 and 11.</p>	<p><b>N/A:</b> Refer to response to AO<sub>11.1</sub> of Table 9.3.7:1.</p> <p><b>N/A:</b> Refer to response to AO<sub>11.2</sub> of Table 9.3.7:1.</p> <p><b>Complies:</b> Refer to response to AO<sub>11.2</sub> of Table 9.3.7:1.</p>

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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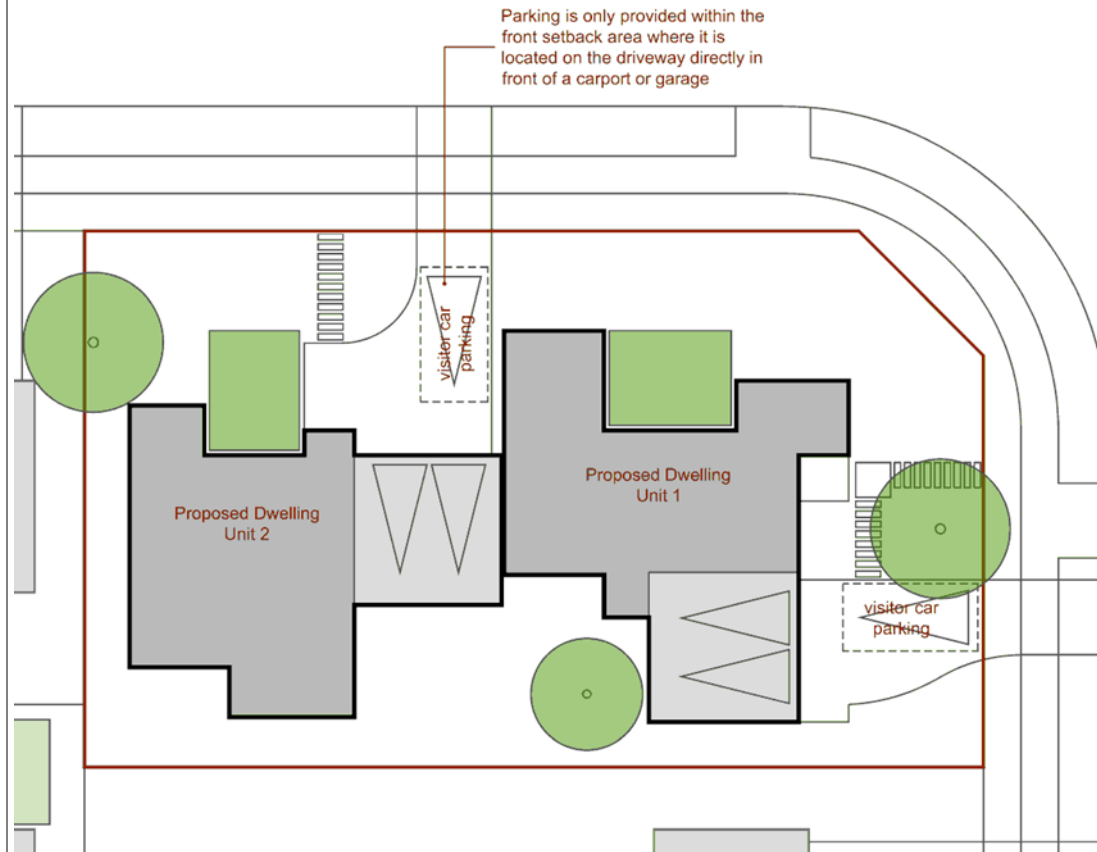


Figure 10

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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Figure 11

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>11</sub> Safe and equitable access for both pedestrians and vehicles is provided, to and from buildings and the street whilst minimising the extent and number of driveways.</p>	<p>In partial fulfilment of the performance outcome:            AO<sub>11.1</sub> Vehicular access from the street is provided via a single shared driveway with a maximum crossover width of 5m.            AO<sub>11.2</sub> Where the site abuts a lane vehicle access is provided from the lane.            Note: See Figure 12.</p>	<p><b>Complies:</b> Refer to response to PO<sub>13</sub> of Table 9.3.7:1.</p>

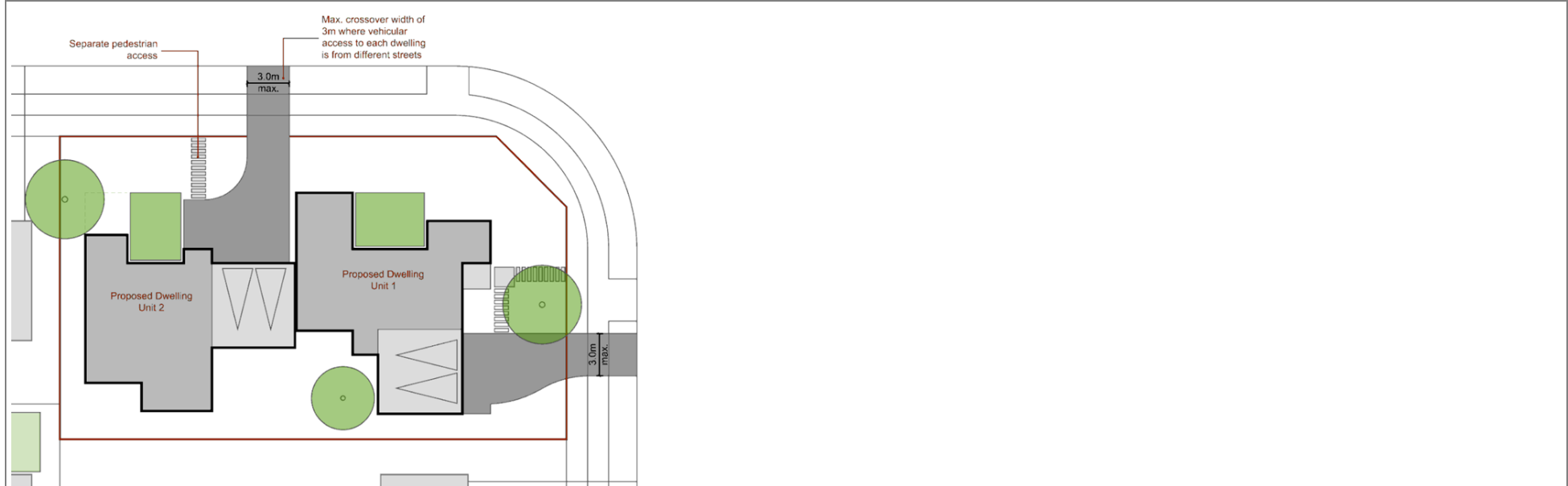


Figure 12

<p>PO<sub>12</sub> The privacy of residents of the development or adjoining premises is protected when they are</p>	<p>AO<sub>12.1</sub> Habitable room windows of a dwelling are separated a minimum of 9m from a habitable</p>	<p><b>Complies:</b> Refer to response to AO<sub>14.1</sub> of Table 9.3.7:1.</p>
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>inside a dwelling or in a private open space area.</p>	<p>room window or private open space of another dwelling on the same or an adjoining site.</p> <p>OR</p> <p>AO<sub>12.2</sub> Where there is a direct view within 9m into a habitable room or private open space of another dwelling on the same or an adjoining site:</p> <ul style="list-style-type: none"> <li>(a) The elements with the view such as windows, balconies, or terraces of a dwelling must be screened in accordance with AO<sub>13.3</sub>; or</li> <li>(b) The Windows have translucent glazing up to or sill heights of at least, 1.5m from the internal floor level; or</li> <li>(c) A solid fence or masonry wall with a minimum height of 1.5m is positioned between the elements with the view.</li> </ul> <p>AND</p> <p>AO<sub>12.3</sub> Where screening is used, it:</p> <ul style="list-style-type: none"> <li>(a) is a solid translucent screen or a louvre perforated panel, trellis or the like that has a maximum of 50% open to solid ratio; and</li> <li>(b) is a fixed, permanent element of a design that complements the style, detailing and materials of the development.</li> </ul> <p>Note: See Figure 13.</p>	<p><b>Complies:</b> Refer to response to AO<sub>14.2</sub> of Table 9.3.7:1.</p> <p><b>Complies:</b> Refer to response to AO<sub>14.3</sub> of Table 9.3.7:1.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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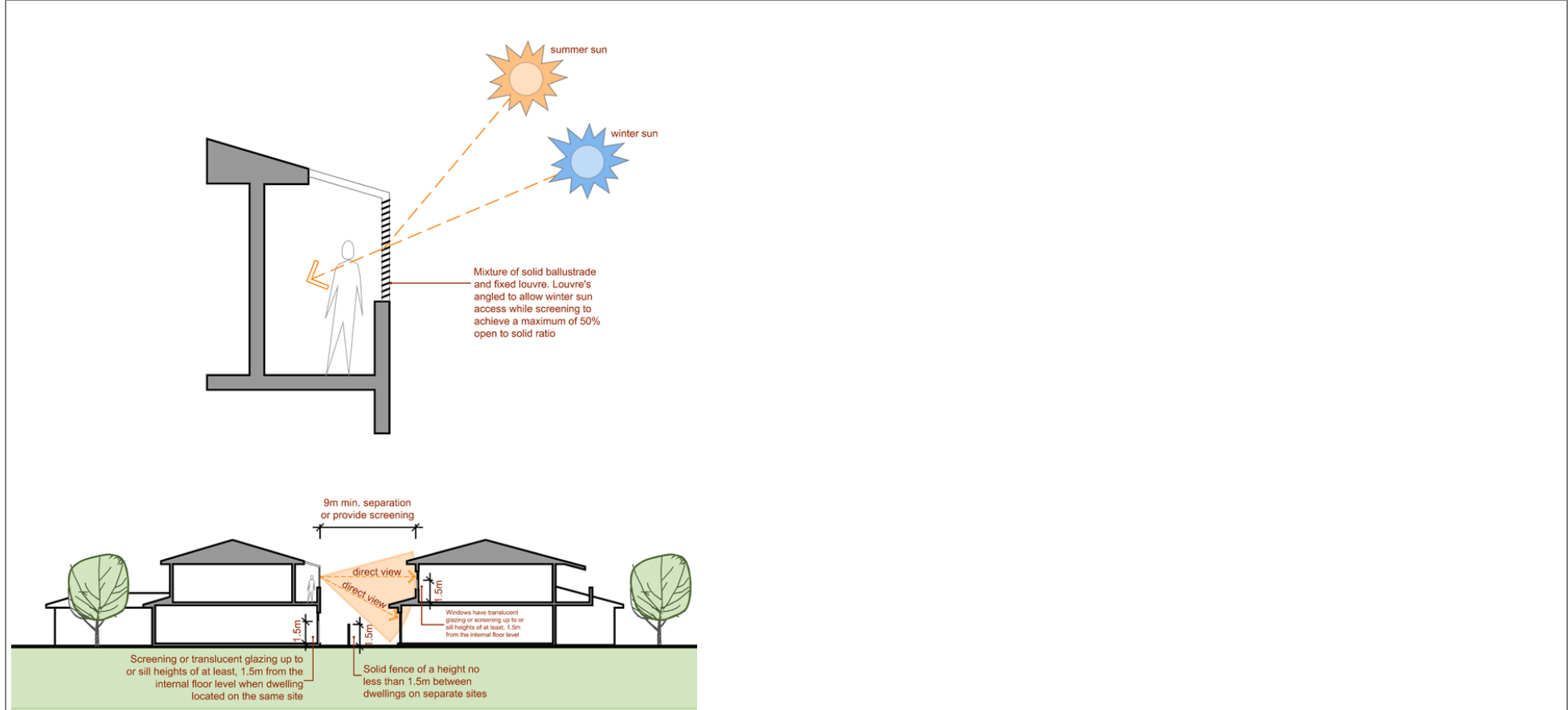


Figure 13

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>13</sub> The development is designed so that occupants are not adversely affected by adjoining non-residential uses.	In partial fulfilment of the performance outcome AO <sub>13.1</sub> A 1.8m high solid screen fence and 2m wide vegetated buffer is provided along any section of the site boundary that is common with vehicle movement and/or parking areas associated with a non-residential use on an adjoining site.	<b>N/A:</b> Refer to response to AO <sub>15.1</sub> of Table 9.3.7:1.
PO <sub>14</sub> Noise from the development does not unreasonably impact on existing or likely future dwellings on nearby land.	AO <sub>14.1</sub> For developments of six (6) dwellings or more, vehicle movement areas are located a minimum of 3m from any adjoining dwellings. AO <sub>14.2</sub> Swimming pools, BBQs, dining areas and other recreational facilities located within communal open areas are located a minimum of 3m from adjoining dwellings.  OR AO <sub>14.3</sub> Provide acoustic screening in accordance with an acoustic engineer's design where vehicle movement or recreational areas are located closer than 3m to any adjoining dwellings.  Note: A minimum 1.5m vegetation buffer will be required with any acoustic fence to provide amenity. (See Figures 14 and 15.)	<b>N/A:</b> The proposed development is for a Dual Occupancy.

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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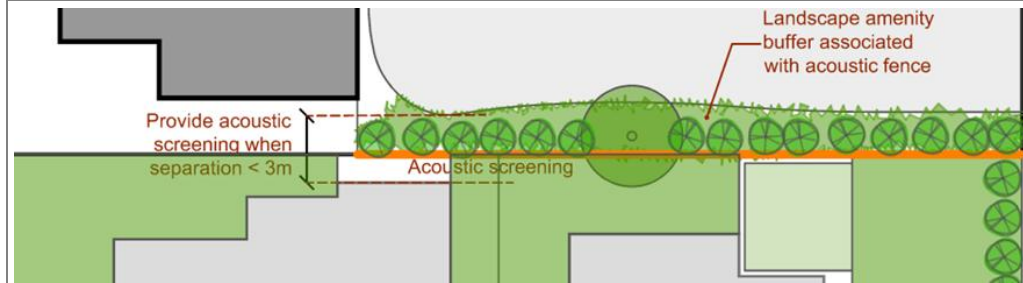
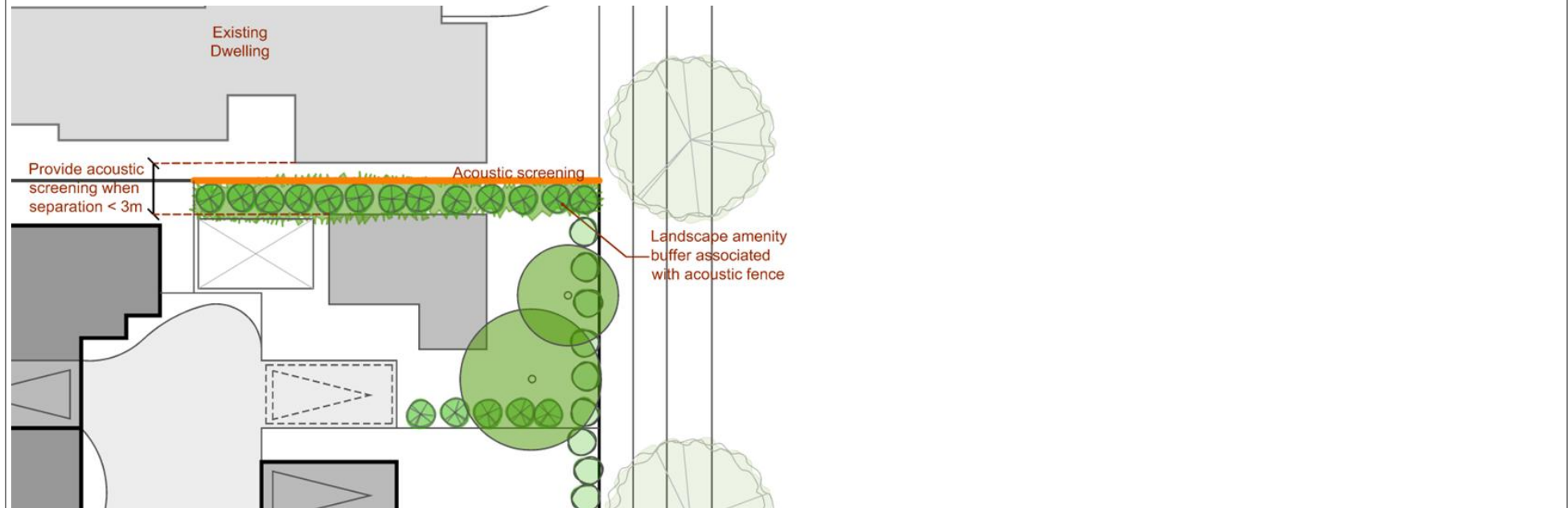


Figure 14



Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Figure 15</b>		
<p>PO<sub>15</sub> Where communal open space is provided, it:</p> <ul style="list-style-type: none"> <li>(a) is positioned on site to enjoy a good solar orientation;</li> <li>(b) includes landscaping and screening for privacy to provide amenity;</li> <li>(c) is easily accessible and conveniently located for the use of all occupants;</li> <li>(d) is useable and safe;</li> <li>(e) is designed and located to minimise internal and external impacts on the amenity of residents and neighbouring premises; and</li> <li>(f) contributes positively to the streetscape where located in a frontage area.</li> </ul>	<p>AO<sub>15.1</sub> Communal open space is oriented to receive at least three hours of sun between 9am and 5pm on 21 June.</p> <p>AO<sub>15.2</sub> Communal open space provided at the ground level, includes a balance of soft and hard landscape features with:</p> <ul style="list-style-type: none"> <li>(a) a minimum 50% open to the sky, and</li> <li>(b) a minimum 25% shaded by trees within 5 years.</li> </ul> <p>AO<sub>15.3</sub> The location and design of communal open space ensures that it is:</p> <ul style="list-style-type: none"> <li>(a) able to be accessed by all residents from a common area within the site;</li> <li>(b) subject to casual surveillance; and</li> <li>(c) separated from private areas of the site.</li> </ul> <p>AO<sub>15.4</sub> Communal open spaces may include indoor recreation facilities.</p> <p>Note: Communal open spaces do not contain:</p> <ul style="list-style-type: none"> <li>(a) vehicle driveways, parking, manoeuvring or other associated hardstand areas; or</li> <li>(b) structures such as rainwater tanks, bin storage areas, air conditioning plant, clothes hoists, etc. (See Figure 16.)</li> </ul>	<p><b>N/A:</b> The proposed development does not involve the provision of communal open space.</p>

Medium Density Residential Code – Assessment benchmarks for assessable development

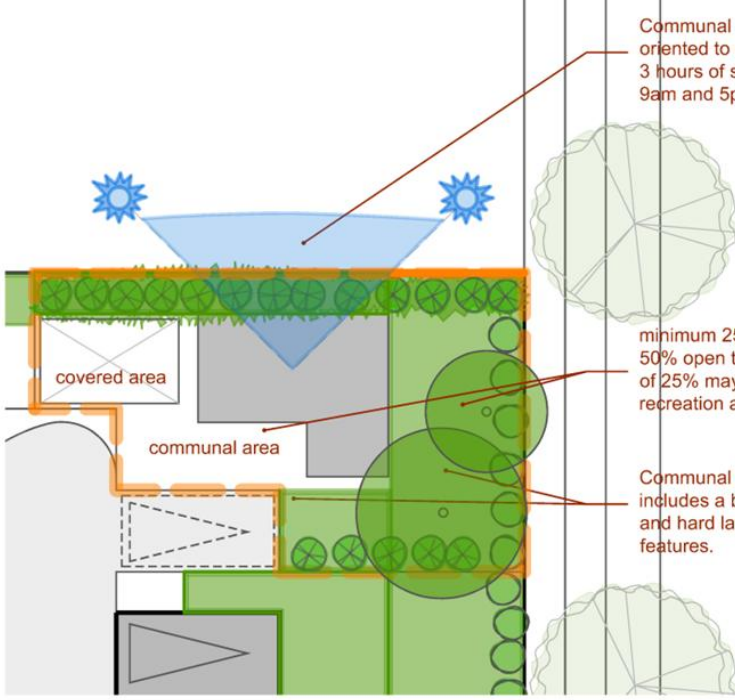
Performance outcomes	Acceptable outcomes	Compliance summary
	<p>Communal open space oriented to receive at least 3 hours of sun between 9am and 5pm on 21 June</p> <p>minimum 25% shade trees and 50% open to sky. Balance area of 25% may include covered recreation areas.</p> <p>Communal open space includes a balance of soft and hard landscape features.</p>	

Figure 16

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>16</sub> Bin storage and access to the collection point is provided in a manner which is convenient, hygienic and will not result in an amenity impact on any use onsite or adjoining the premises.</p>	<p>AO<sub>16.1</sub> Bin storage is:</p> <ul style="list-style-type: none"> <li>(a) not located within the minimum private open space area required for the development;</li> <li>(b) not located within 2m of any fresh air intake of any dwelling on the premises or adjoining premises; and</li> <li>(c) if in an area which is accessible by more than one dwelling, is clearly identified as the storage area for the relevant unit/dwelling.</li> </ul> <p>AO<sub>16.2</sub> A path of access is provided between any bin storage area and the collection point which:</p> <ul style="list-style-type: none"> <li>(a) is a minimum of 1.2m wide;</li> <li>(b) is clear of any obstruction (i.e. retaining wall, landscaping, hot water tank);</li> <li>(c) does not traverse through any habitable room (other than a garage);</li> <li>(d) provides an at grade path of access (i.e. no steps); and</li> <li>(e) is no longer than 30m.</li> </ul> <p>Note: See Figure 17.</p>	<p><b>Complies:</b> Each unit will be provided with screened bin storage areas that are located outside of private open space and more than 2m from any fresh air intake. The proposed development will not involve shared bin storage.</p> <p><b>Complies:</b> Each unit will be provided with a path of access between bin storage areas and the kerbside collection point which complies with AO<sub>16.2</sub>.</p>

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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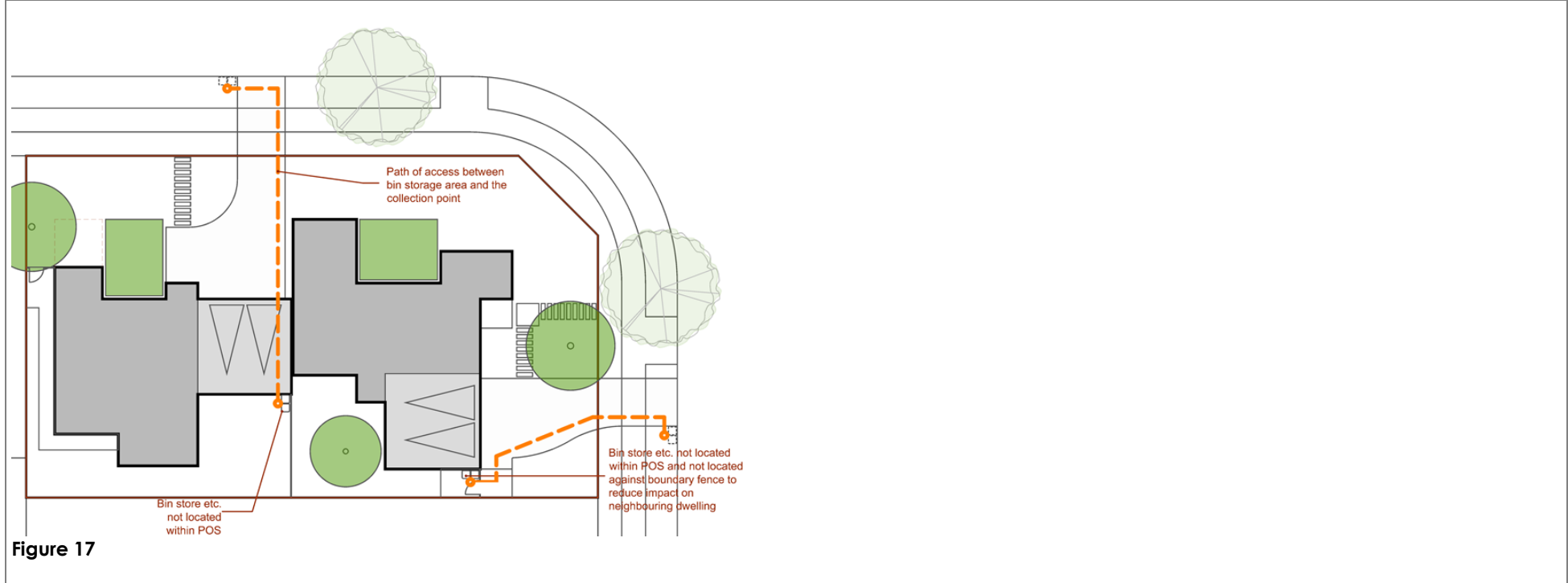
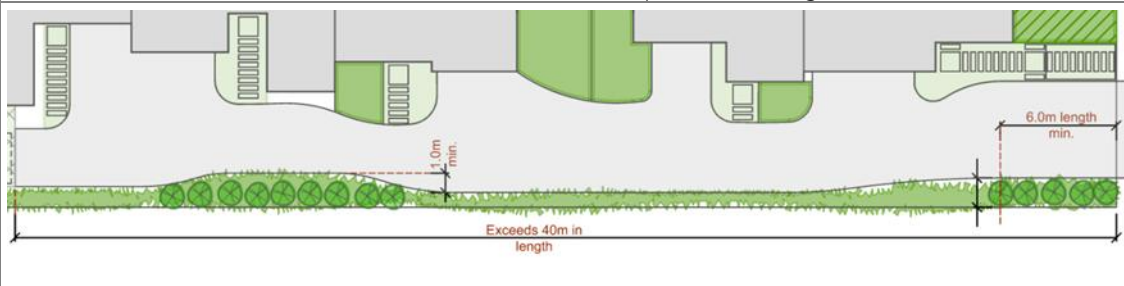


Figure 17

PO <sub>17</sub> Long, unvaried driveways to property boundaries are avoided and do not detract from the local streetscape.	AO <sub>17.1</sub> Driveways are setback from side boundaries a minimum of 1.5m for the first 6m and 750mm thereafter incorporating a continuous densely	<b>N/A:</b> The proposed development does not involve a long, unvaried driveway.
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>planted landscape strip between the edge of the driveway and side boundary.</p> <p>AO<sub>17.2</sub> Driveways that exceed 40 metres in length must incorporate a change of alignment of a minimum of 1m.</p> <p>Note: See Figure 18.</p>	
		
<p><b>Figure 18</b></p>		
<p>PO<sub>18</sub> Buildings are designed to a high aesthetic standard providing:</p> <ul style="list-style-type: none"> <li>(a) prominent features towards the street frontage;</li> <li>(b) detailing and finishes appropriate to the scale, quality and character of the street and to help differentiate between dwellings;</li> <li>(c) opportunities for informal surveillance of the street and other public spaces;</li> <li>(d) clear and visible entries; and</li> <li>(e) elements that complement or enhance the character of the local neighbourhood.</li> </ul>	<p>In partial fulfilment of the performance outcome:</p> <p>AO<sub>18.1</sub> Buildings:</p> <ul style="list-style-type: none"> <li>(a) where at the front of the site, are oriented to and address the street;</li> <li>(b) have front entries that provide full weather protection and where within 10m of any street, have clearly defined front entries that are visible from the street;</li> <li>(c) where within 10m of any street; have one or more habitable rooms facing the street boundary that have windows or doors with a minimum area of clear glazing of 0.25m<sup>2</sup> for each linear metre of street frontage; and</li> </ul>	<p><b>Performance Solution:</b> Refer to response to PO<sub>16</sub> of Table 9.3.7:1.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(d) are designed to deliver depth in the façade facing the street or public space with a variation not less than 2m deep for a minimum 50% of the building width at ground level.</p> <p>Note: See Figure 19.</p>	

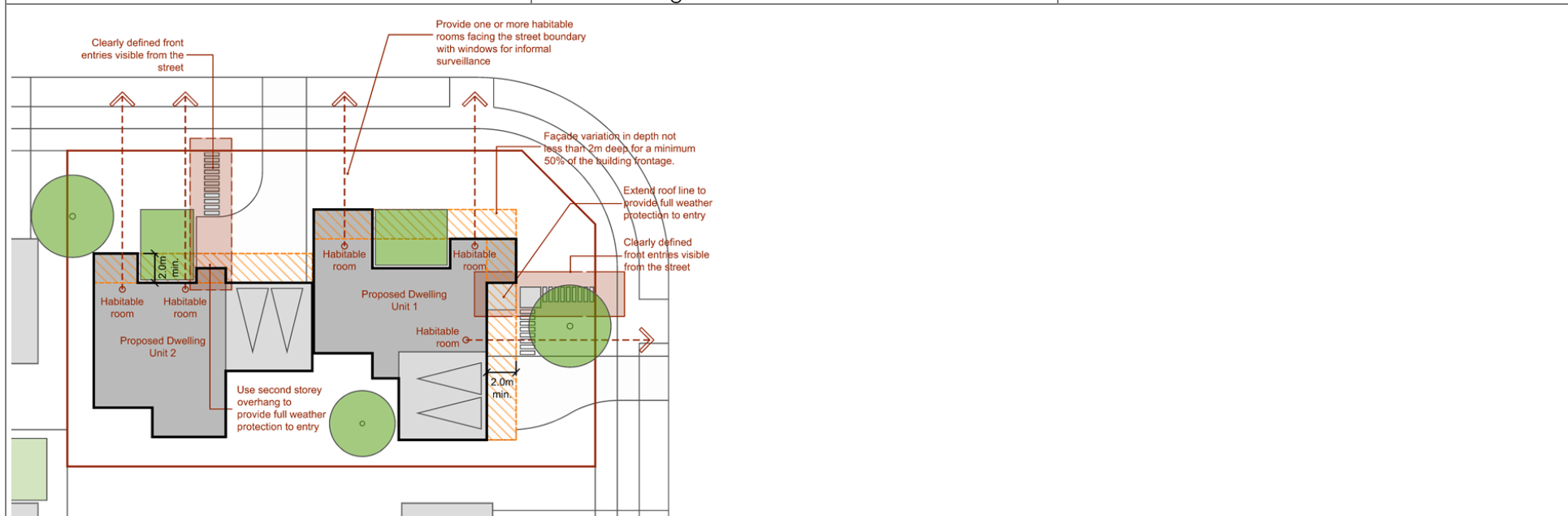


Figure 19

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>19</sub> Development incorporates typical domestic detailing to achieve a residential scale living environment with aesthetic standards and amenity for occupants similar to those of single dwellings in the neighbourhood.</p>	<p>In partial fulfilment of the performance outcome</p> <p>AO<sub>19.1</sub> The maximum single wall length of a building is 32m with offsets of 600mm minimum provided every 7.5m;</p> <p>Note: Building walls separated by a distance of less than 10% of their combined length are considered as one wall. (See Figure 20.)</p> <p>AO<sub>19.2</sub> Building bulk is minimised through articulation and other measures, including the provision of at least one (1) element from at least three (3) of the following element groups:</p> <ul style="list-style-type: none"> <li>(a) verandahs, porches, pergolas or balconies;</li> <li>(b) roof overhangs;</li> <li>(c) window hoods/screens;</li> <li>(d) awning and shade structures; or</li> <li>(e) 600mm vertical gutter offset to create roof articulation.</li> </ul> <p>AO<sub>19.3</sub> Townhouses or attached dwellings have:</p> <ul style="list-style-type: none"> <li>(a) no more than three (3) dwellings contained within a single building;</li> <li>(b) a minimum of 4.5m separation between buildings.</li> </ul>	<p><b>N/A:</b> Refer to response to AO<sub>19.1</sub> of Table 9.3.7:1.</p> <p><b>Complies:</b> Refer to response to AO<sub>17.2</sub> of Table 9.3.7:1.</p> <p><b>Complies:</b> The proposed development involves two separate buildings which each building containing only one (1) unit.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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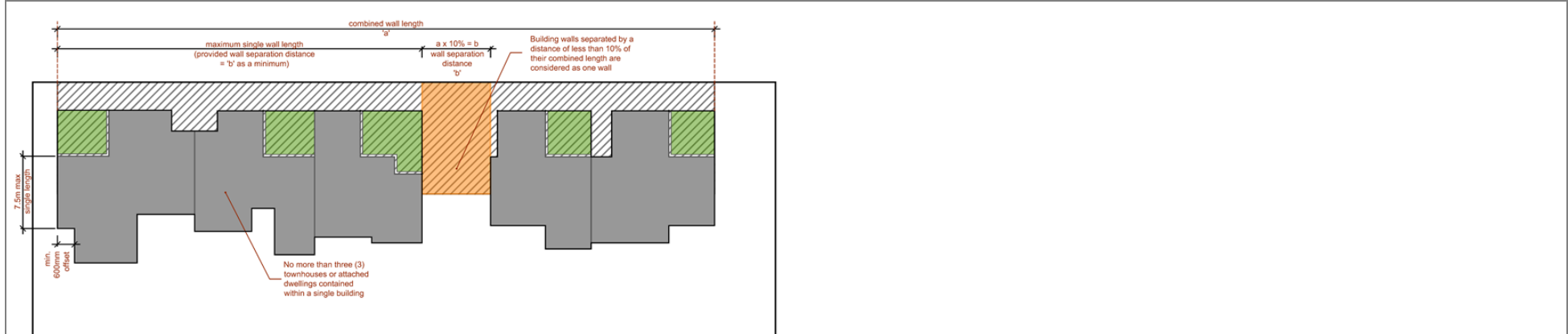


Figure 20

<p>PO<sub>20</sub> Facades include a variety of colours, finishes and textures based on those found in houses within the surrounding streetscape and appropriate to the desired character of the area.</p>	<p>In partial fulfilment of the performance outcome: AO<sub>20.1</sub> Building facades incorporate:</p> <ul style="list-style-type: none"> <li>(a) external elements and treatments that are of a high quality finish, durable and need minimal maintenance;</li> <li>(b) at least three (3) distinct textures to finishes or surfaces not including roof, rainwater goods, glazing, joinery, doors, garage doors or soffits; and</li> <li>(c) colours and textures that are similar with the colours and textures of adjoining buildings.</li> </ul>	<p><b>Complies:</b> The proposed development will be constructed of high quality, low maintenance materials and will feature a range of textures, colours and finishes which soften the visual impact of the development and ensure that it integrates with the surrounding locality.</p>
<p>PO<sub>21</sub> Development provides landscaping that contributes to the landscape character, shade and the micro-climate of the neighbourhood and site through the retention of existing significant trees on the site.</p>	<p>In partial fulfilment of the performance outcome: AO<sub>21.1</sub> Landscape design plans for the subject site are prepared by a Registered Landscape Architect and lodged with the application for development approval.</p>	<p><b>Complies:</b> No landscaping plan has been provided as part of this application. Documentation prepared by a relevant professional will be provided following approval of the proposed development.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

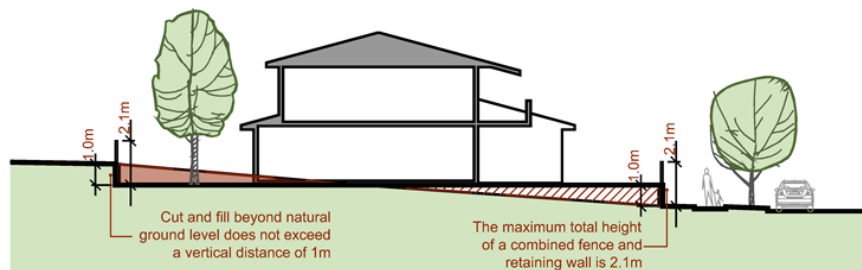
Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>21.2</sub> Existing mature trees:</p> <ul style="list-style-type: none"> <li>(a) are retained where assessed by a qualified arborist as healthy and sound and not a potential risk to people and property, and having a life expectancy greater than 10 years; and</li> <li>(b) are integrated into the site design and protected from disturbance.</li> </ul> <p>Note: An arborist report will be required to demonstrate assessment of mature trees and include advice for their protection from disturbance during construction.</p> <p>Note: Retained mature trees are eligible to contribute to minimum tree provision requirements within AO<sub>22.3</sub>.</p> <p>OR</p> <p>AO<sub>21.3</sub> New canopy trees capable of achieving a height of at least five (5) metres and canopy spread of at least six (6) metres at maturity are provided at the following rate:</p> <ul style="list-style-type: none"> <li>(a) one (1) per site with a frontage of less than 25 metres; and</li> <li>(b) two (2) per site with a frontage of 25m or more.</li> </ul> <p>AO<sub>21.4</sub> Plant species selection complements the local landscape character, is long lived, has non-invasive roots and suited to the site drainage, soil and climatic conditions.</p> <p>Note: New street trees should be provided in accordance with Toowoomba Regional Council Street Tree Master Plan</p>	<p><b>Complies:</b> Existing mature trees on the footpath have been incorporated into the design of the proposed development.</p> <p><b>Complies:</b> New trees are proposed within the front setback of each unit, as per the conceptual details in <b>Appendix D</b>.</p> <p><b>Complies:</b> Species used in landscaping will be selected to ensure that they complement the surrounding locality, contribute to drainage and are appropriate given the climatic conditions.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>22</sub> The treatment of fencing and retaining walls in all situations reinforces the high quality, amenity and safety of the development.</p> <p>Note: Details demonstrating compliance with the performance outcome should be included in the landscape design drawings submitted with any development application.</p>	<p>AO<sub>22.1</sub> The maximum total height of a combined fence and retaining wall is 2.1m, as measured from the finished surface level.</p> <p>AO<sub>22.2</sub> Where a combined fence and retaining wall is located on the front boundary, the fence has a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>AO<sub>22.3</sub> Retaining walls and fences are constructed of high quality materials and contribute to residential amenity.</p> <p>AO<sub>22.4</sub> Any fence on the front boundary has a minimum 30% transparency when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>Note: Where retaining walls are set off the property boundary by a minimum of 600mm, they are not considered to be combined with the fencing. (See Figure 21.)</p>	<p><b>N/A:</b> Refer to response to AO<sub>19.1</sub> of Table 9.3.7:1.</p> <p><b>N/A:</b> Refer to response to AO<sub>19.2</sub> of Table 9.3.7:1.</p> <p><b>N/A:</b> Refer to response to AO<sub>19.3</sub> of Table 9.3.7:1.</p> <p><b>Complies:</b> Refer to response to AO<sub>19.4</sub> of Table 9.3.7:1.</p>



Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Figure 21</b>		
<p>PO<sub>23</sub> Front fencing contributes to privacy, screening, security, and street character while maintaining a visual relationship between dwellings and public spaces.</p> <p>Note: Details demonstrating compliance with the performance outcome should be included in the landscape design drawings submitted with any development application.</p>	<p>AO<sub>23.1</sub> Fences located on a front property boundary or a common boundary with an adjacent park or within the front setback area:</p> <ul style="list-style-type: none"> <li>(a) have a maximum height of 1.2 metres; or</li> <li>(b) have a maximum height of 1.5 metres and a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).</li> </ul> <p>Note: In instances where the fence is located in conjunction with a retaining wall refer PO<sub>23</sub>.</p>	<p><b>Complies:</b> Refer to response to AO<sub>20.1</sub> of Table 9.3.7:1.</p>
<p>PO<sub>24</sub> Stormwater discharge from the site is to a lawful point of discharge as defined in the Queensland Urban Drainage Manual (QUDM), without the use of pumped or charged pipe systems, and not to private land other than to an easement for stormwater purposes befitting the site and allowing discharge to a lawful point of discharge in land over which Council has tenure or control.</p> <p>Note: Land over which Council has tenure or control does not include Council's open space network.</p>	<p>AO<sub>24.1</sub> The site:</p> <ul style="list-style-type: none"> <li>(a) has a natural surface with an elevation that is higher than the abutting road and enables stormwater to drain gravitationally to the abutting road via subsurface pipes; or</li> <li>(b) has a natural surface with an elevation that is higher than Council's stormwater drainage network in the abutting road and enables stormwater to drain gravitationally to the stormwater drainage network in the abutting road via subsurface pipes; or</li> <li>(c) has lawful access to an inter allotment drainage network with available capacity to meet the requirements of the development.</li> </ul>	<p><b>Complies:</b> Refer to response to PO<sub>21</sub> of Table 9.3.7:1.</p>
<p>PO<sub>25</sub> Where a Relocatable Home Park or Tourist Park the development site has an appropriate area and configuration to accommodate home or van sites, as well as adequate landscape and</p>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The proposed development is for a Dual Occupancy.</p>

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**Medium Density Residential Code – Assessment benchmarks for assessable development**

<b>Performance outcomes</b>	<b>Acceptable outcomes</b>	<b>Compliance summary</b>
recreation areas, ancillary amenities and facilities, car parking and service areas.		

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Environmental Standards Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Outdoor Lighting</b>		
PO <sub>1</sub> Development does not unacceptably reduce the amenity and environmental quality of environs, especially of any nearby residential premises or public spaces as a result of light spill.	AO <sub>1.1</sub> No outdoor lighting is proposed as part of the development. OR AO <sub>1.2</sub> Technical parameters, design, installation, operation and maintenance of outdoor lighting comply with the requirements of Australian Standard 'AS4282-1997 control of the obtrusive effects of outdoor lighting'. AO <sub>1.3</sub> For sporting fields and sporting courts the technical parameters, design, installation, operation and maintenance comply with the requirements of Australian Standard AS4282-1997 – Control of the obtrusive Effects of Outdoor Lighting and a compliance statement by a lighting designer has been provided in accordance with the Australian Standard (Section 4). AO <sub>1.4</sub> Where light spillage outside of the property boundary is likely to result in levels above those mentioned in AO <sub>1.3</sub> the applicant has provided a lighting proposal and impact assessment (environmental and amenity) as part of the application which has demonstrated that the lighting will not create nuisance issues for surrounding sensitive receptors. AO <sub>1.5</sub> For private sporting courts the lighting system: (a) is baffled or shielded to ensure that a light source is not directly visible from a Habitable Room window of an adjoining	<b>Complies:</b> No outdoor lighting will be provided as part of the proposed development.  <b>N/A:</b> Refer to response to AO <sub>1.1</sub> .

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Environmental Standards Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p style="text-align: center;">Dwelling; and (b) the luminaire does not exceed a height of 8m above the court surface.</p> <p>AO1.6 The alignment of streets, driveways and servicing areas avoid vehicle headlight impacts on adjacent residential dwellings.</p>	
<p>PO<sub>2</sub> Outdoor lighting (excluding street lighting, normal residential lighting and low level security lighting) situated in excess of 4m above ground level does not jeopardise the safety or well-being of any pedestrian, cyclist or motorist. Light emissions do not reduce the ability of transport system users to see essential details of the route ahead, including signalling systems and signage.</p>	<p>AO<sub>2.1</sub> Outdoor lighting situated in excess of 4m above ground level is provided in accordance with Australian Standard AS1158.1.1:2005 – Road Lighting – Vehicular Traffic (Category V) Lighting – Performance and Installation Design Requirements.</p>	<p><b>Complies:</b> No outdoor lighting will be provided as part of the proposed development.</p>
<p>PO<sub>3</sub> Outdoor Lighting does not cause unreasonable disturbance or cause detrimental impacts to any significant natural environment.</p>	<p>AO<sub>3.1</sub> The vertical illumination emanating from the outdoor lighting does not exceed one (1) lx on land within the Environmental Significance Overlay.</p>	<p><b>Complies:</b> No outdoor lighting will be provided as part of the proposed development.</p>
<p>PO<sub>4</sub> Proposed sensitive land uses adjoining existing lawful non-residential uses with significant lighting for community purposes, security or safety reasons are designed to proactively address possible obtrusive light nuisance.</p>	<p>AO<sub>4.1</sub> Proposed sensitive land uses adjoining existing lawful non-residential uses with significant lighting for community purposes, security or safety reasons are designed in a manner to mitigate any light nuisance impacts from the existing lawful use by establishing:</p> <ul style="list-style-type: none"> <li>(a) shielding or louvers on windows facing the light source;</li> <li>(b) orientating buildings and bedrooms so that external lighting does not impact on residents during night time hours; and</li> <li>(c) utilising earth embankments, landscaping</li> </ul>	<p><b>N/A:</b> The proposed development does not adjoin a non-residential use with significant lighting.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Environmental Standards Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	or other physical measures to shield existing light sources.	
<b>Odour</b>		
PO <sub>5</sub> Development does not unreasonably affect the amenity and environmental quality of environs, especially of any nearby residential premises or public spaces due to odour impacts.	AO <sub>5.1</sub> The development does not involve activities that create odorous air emissions. OR AO <sub>5.2</sub> The development does not result in air emissions that exceed any of the acceptable levels specified within the <i>Environmental Protection (Air) Policy 2008</i> . AO <sub>5.3</sub> The development will not result in the release of noxious or offensive odours beyond the boundary of the site that cause environmental nuisance at any odour sensitive place, i.e. sufficient buffering is available within the development site itself to dissipate odour issues.  <i>Note: An Odour Assessment Report provided with the application may be necessary to demonstrate compliance with AO5.3.</i>	<b>Complies:</b> The proposed development is not of a nature that will result in odorous air emissions.  <b>N/A:</b> Refer to response to AO <sub>5.1</sub> .
PO <sub>6</sub> Lot reconfigurations for residential or other environmentally sensitive land uses do not encroach upon existing or approved uses that may detrimentally impact upon the amenity of those proposed uses in terms of odour nuisance.	AO <sub>6.1</sub> Lots for residential or other environmentally sensitive land uses are not located within the distances from specific uses outlined in Table 9.4.2:2 at the end of this code. AO <sub>6.2</sub> Where lots for residential or other environmentally sensitive land uses are located within the distances from specific existing uses outlined in Table 9.4.2:2, an Odour Assessment Report has been provided to demonstrate that the development will achieve the following	<b>N/A:</b> The proposed development is associated with a Material Change of Use.

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Performance outcomes	Acceptable outcomes	Compliance summary					
	<p>thresholds therefore minimising odour nuisance.</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr style="background-color: #cccccc;"> <th style="text-align: left;">Existing Use/Activity</th> <th style="text-align: left;">Odour Level at Sensitive Receiving Environment.</th> </tr> </thead> <tbody> <tr> <td rowspan="2" style="text-align: center; vertical-align: middle;">All Activities</td> <td>2OU/m<sup>3</sup> 3 minute average, 99.5th percentile.</td> </tr> <tr> <td>4OU/m<sup>3</sup> 3 minute average, 99.9th percentile.</td> </tr> </tbody> </table>	Existing Use/Activity	Odour Level at Sensitive Receiving Environment.	All Activities	2OU/m <sup>3</sup> 3 minute average, 99.5th percentile.	4OU/m <sup>3</sup> 3 minute average, 99.9th percentile.	
Existing Use/Activity	Odour Level at Sensitive Receiving Environment.						
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	4OU/m <sup>3</sup> 3 minute average, 99.9th percentile.						
<p>PO<sub>7</sub> Putrescibles waste generated as a result of the development does not cause odour nuisance issues for adjoining land uses.</p>	<p>AO<sub>7.1</sub> The development ensures that all putrescibles waste will be stored in a manner that prevents odour nuisance and fly breeding and will be disposed of at intervals not exceeding seven (7) days.</p>	<p><b>Complies:</b> Each unit has sufficient area for the storage of domestic waste bins. Council's kerbside collection will be used to ensure bins are collected at intervals not exceeding seven (7) days.</p>					
<b>Noise</b>							
<p>PO<sub>8</sub> The generation of noise from the premises does not cause Environmental Harm or Nuisance to adjoining properties or other noise sensitive land uses.</p> <p>(a) Development:</p> <ul style="list-style-type: none"> <li>(i) is located in an appropriate zone;</li> <li>(ii) proposes best practice design and construction materials (in relation to noise attenuation); and</li> <li>(iii) proposes operational practices that will minimise noise nuisance for adjoining sensitive land uses.</li> </ul>	<p>AO<sub>8.1</sub> The development will achieve the following noise levels (when measured at the nearest sensitive receiver):</p> <ul style="list-style-type: none"> <li>(a) Background (L<sub>90</sub>) + 5dB(A) for variable noise between the hours of 7:00 am to 10:00 pm (measured at the facade of the sensitive land use);</li> <li>(b) Background (L<sub>90</sub>) + 3dB(A) for variable noise between the hours of 10:00 pm and 7:00 am (measured within bedrooms assuming open windows);</li> <li>(c) Background (L<sub>90</sub>) for continuous noise sources (measured at the facade of the</li> </ul>	<p><b>Complies:</b> The proposed development will not generate noise and will achieve the relevant noise levels in accordance with the relevant standards.</p>					

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	<p>sensitive land use between 7:00 am and 10:00 pm and within bedrooms assuming open windows from 10:00 pm – 7:00 am); and</p> <p>(d) maximum limit L<sub>Amax</sub> 45dB(A) inside dwellings; and</p> <p>The development will achieve the Acoustic Quality Objectives listed within the <i>Environmental Protection (Noise) Policy 2008</i>.</p> <p>HOWEVER</p> <p>AO<sub>8.2</sub> Where a development is unable to meet noise levels specified in AO<sub>8.1</sub> an acoustic assessment has been undertaken by a suitably qualified and skilled person which demonstrates that the development will not result in environmental nuisance at any existing or likely future residential premises (within a 10 year planning horizon).</p>	
<p>PO<sub>9</sub> Development (other than licensed premises operating under a Liquor Licensing Approval) proposing the use of amplified sound equipment is designed, constructed and operated in a manner that is sensitive to the impacts of high and low frequency noise on adjoining sensitive land uses.</p>	<p>AO<sub>9.1</sub> Where development (other than licensed premises operating under a Liquor Licensing Approval) proposes the use of amplified sound equipment, existing background octave band centre frequencies have been assessed and the development proposes the following maximum sound pressure criterion:</p>	<p><b>N/A:</b> The proposed development will not involve the use of amplified sound equipment.</p>

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<p>PO<sub>10</sub> Proposed sensitive land uses in close proximity to existing lawful land uses involving significant noise emissions such as entertainment venues, child care centres, industrial zones or other commercial premises are designed and constructed in a manner that achieves acoustic amenity for the users of the development.</p>	<p>AO<sub>10.1</sub> The development is designed to achieve the internal noise criterion (Acoustic Quality Objectives) for the particular use as specified within the <i>Environmental Protection (Noise) Policy 2008</i>.</p> <p>AO<sub>10.2</sub> Where the proposed sensitive land use is not listed in the Environmental Protection (Noise) Policy 2008, the development is designed and constructed to meet the internal sound level design criterion contained in Australian Standard AS2107:2000 Acoustics – Recommended design sound levels and reverberation times for building interiors.</p> <p>AO<sub>10.3</sub> Where the sensitive land use is within or adjoining entertainment venues utilising amplified music</p>	<p><b>Performance Outcome:</b> The proposed development is located within a Major Centre Zone, however it is located in the regional town of Pittsworth and not located in proximity to uses which generate significant noise emissions such as entertainment venues, child care centres or uses which operate outside normal business hours. The nearby uses include offices, shops and government buildings.</p>																										

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	the applicant has undertaken an acoustic assessment of existing background octave levels and designed the sensitive land use component to mitigate the impacts of low frequency noise (particularly between 31.5Hz and 125Hz).	
PO <sub>11</sub> Proposed sensitive land uses adjoining Council controlled arterial roads (other than designated 'Transport Noise Corridors') are designed and constructed in a manner that provides acoustic amenity for users/residents of the development.	<p>Where development involves a sensitive land use adjoining a Council controlled arterial road (other than designated 'Transport Noise Corridors':</p> <p>AO<sub>11.1</sub> The development is designed and constructed in a manner that achieves the internal noise Acoustic Quality Objectives listed within the <i>Environmental Protection (Noise) Policy 2008</i>.</p> <p>AO<sub>11.2</sub> The siting of buildings and selection of construction materials complies with the specifications of Australian Standard AS3671-1989 <i>Acoustics – Road traffic noise intrusion – Building siting and construction</i>.</p>	<b>N/A:</b> The proposed development does not adjoin a Council controlled arterial road.
PO <sub>12</sub> Proposed sensitive land uses adjoining 'Transport Noise Corridors' as designated by State or Local Government are designed and constructed in a manner that provides acoustic amenity for users/residents of the development.	AO <sub>12.1</sub> Proposed sensitive land uses adjoining 'Transport Noise Corridors' as designated by State or Local Government comply with the <i>Queensland Development Code Mandatory Part (MP) 4.4 'Buildings in a Transport Noise Corridor'</i> for all habitable rooms adjoining the corridor.	<b>N/A:</b> The proposed development does not adjoin a 'Transport Noise Corridor'.
PO <sub>13</sub> Air conditioning units, refrigeration units and any other form of mechanical ventilation or extraction systems do not adversely impact on the acoustic amenity of surrounding sensitive land uses.	<p>AO<sub>13.1</sub> Plant of this nature is not elevated, is acoustically shielded (if necessary) and will not be audible at adjoining sensitive receivers.</p> <p>AO<sub>13.2</sub> Roof-top mounted plant and equipment is located away from surrounding sensitive land uses and is acoustically shielded to achieve a nil</p>	<b>N/A:</b> The proposed development will not involve mechanical plants.

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	increase in background noise levels ( $L_{90}$ ) at the nearest sensitive receiver.	
PO <sub>14</sub> The construction phase of the development does not cause adverse acoustic impacts on surrounding sensitive receivers.	<p>AO<sub>14.1</sub> Building work (including excavation and filling) is only conducted between the hours of 6:30 am and 6:30 pm Monday to Saturday (excluding public holidays).</p> <p>AO<sub>14.2</sub> Where building work is proposed outside of the acceptable timeframe of 6:30 am to 6:30 pm (Monday – Saturday) the applicant has supplied a 'construction management plan' which adequately addresses noise mitigation measures.</p>	<p><b>Complies:</b> Construction of the proposed development will occur within the times outlined in AO<sub>14.1</sub>.</p> <p><b>N/A:</b> Refer to response to AO<sub>14.1</sub>.</p>
PO <sub>15</sub> Private sporting courts do not create acoustic amenity issues for surrounding sensitive receivers.	<p>AO<sub>15.1</sub> Private sporting courts are not used between 10:00 pm and 7:00 am.</p> <p>AO<sub>15.2</sub> Mechanical equipment such as ball throwing machines which create audible noise at the nearest sensitive receiver is not used between 7:00 pm and 7:00 am.</p>	<p><b>N/A:</b> The proposed development does not involve a private sporting court.</p> <p><b>N/A:</b> Refer to response to AO<sub>15.1</sub>.</p>
PO <sub>16</sub> Vibration from the development does not affect the amenity of surrounding sensitive land uses or cause environmental harm or nuisance.	<p>AO<sub>16.1</sub> The development does not result in vibration impacts outside of the development site.</p> <p>AO<sub>16.2</sub> Where vibration may impact on surrounding sensitive land uses, the proponent has provided a vibration impact assessment or alternatively included vibration within an environmental impact report for the site which demonstrates that the level of vibration will not cause adverse amenity impacts or cause environmental harm or nuisance at any sensitive land use surrounding the development.</p>	<p><b>Complies:</b> The proposed development is not of a nature that will result in vibration impacts outside of the site.</p> <p><b>N/A:</b> Refer to response to AO<sub>16.1</sub>.</p>

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<b>Dust</b>		
<p>PO<sub>17</sub> The construction phase of the development prevents or mitigates (to an acceptable level) the release of dust particles which have potential to cause environmental nuisance to adjoining sensitive receivers (including sensitive receivers along haulage routes during excavation and filling operations).</p>	<p>AO<sub>17.1</sub> Off-site release of dust particles will be strictly managed to ensure that dust emissions do not travel beyond the property boundary and environmental nuisance does not occur.</p> <p>AO<sub>17.2</sub> Areas of exposed fill, excavation and unsealed accesses on the site are watered regularly (particularly during periods of high or constant wind) to reduce dust generation.</p> <p>AO<sub>17.3</sub> Areas of fill and excavation are graded, compacted and planted and/or mulched immediately after the dumping operation is complete.</p> <p>AO<sub>17.4</sub> Stockpiles of aggregate, sand or other materials brought onto the site are sprayed with water (or treated with an alternative method) to minimise dust nuisance. The frequency of water spraying is increased during hot, dry periods or where wind conditions are such that a dust nuisance is likely to occur. Stockpiles are located away from adjoining sensitive land uses.</p> <p><i>Note: Where excavation and filling exceeds 1,000 cubic metres the development has submitted a 'construction management plan' which adequately addresses dust mitigation measures. Measures must include strategies such as progressive rehabilitation and complaints processes.</i></p>	<p><b>Complies:</b> Dust generated during the construction phase will be managed to ensure that it does not travel beyond the property boundary.</p> <p><b>N/A:</b> The proposed development is not of a nature that it will involve exposed areas of fill, excavation or unsealed accesses during construction.</p> <p><b>N/A:</b> The proposed development is not of a nature that it will involve exposed areas of fill or excavation during construction.</p> <p><b>N/A:</b> The proposed development is not of a nature that it will involve stockpiles during construction.</p>
<p>PO<sub>18</sub> Haulage activities associated with excavation</p>	<p>AO<sub>18.1</sub> Haulage routes are selected on the basis of</p>	<p><b>N/A:</b> The proposed development is not of a nature that</p>

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and filling are managed to prevent environmental nuisance issues.	using the most suitable road surface to prevent dust generation and minimising the number of dwellings or other sensitive land uses affected by potential dust nuisance.	it will involve exposed areas of fill or excavation during construction.
PO <sub>19</sub> Water used for dust suppression activities does not itself create environmental harm.	AO <sub>19.1</sub> Water approved as a method for controlling dust emissions must not be used in a manner that enables contaminated water to enter any stormwater system or natural drainage corridor outside of the site boundaries.	<b>Complies:</b> Any water utilised to control dust emissions will be managed to ensure that contaminated water does not enter any receiving waters.
PO <sub>20</sub> The ongoing operation of the development site does not create dust nuisance for adjoining landholders.	<p>AO<sub>20.1</sub> Areas within the site that are frequently used for vehicular purposes are imperviously sealed.</p> <p>AO<sub>20.2</sub> Industry-specific activities undertaken on site that create dust are performed in an enclosed shed or other structure with suitable dust extraction and filtration systems.</p> <p>AO<sub>20.3</sub> Grain facilities are equipped with semi enclosed grain receival hoppers fitted with dust extraction and filtration systems. All conveyor belts and bulk grain processing equipment are enclosed to prevent dust emission. Bunker storage without dust extraction is only permitted whereby the release of dust will not impact on surrounding sensitive receivers.</p> <p>AO<sub>20.4</sub> All development likely to generate any significant amount of dust must have an adequate water supply available at all times in order to undertake proactive dust reduction measures e.g. watering of access roads.</p> <p>AO<sub>20.5</sub> Development that is likely to create ongoing significant dust issues has submitted a 'site based</p>	<p><b>Complies:</b> The proposed development will provide imperviously sealed vehicle movement areas.</p> <p><b>N/A:</b> The proposed development is associated with a Dual Occupancy.</p> <p><b>N/A:</b> The proposed development is associated with a Dual Occupancy.</p> <p><b>N/A:</b> The proposed development is not of a nature that will result in significant dust generation.</p> <p><b>N/A:</b> The proposed development is not of a nature that will result in significant dust generation.</p>

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	management plan' which adequately addresses dust mitigation measures.	
PO <sub>21</sub> Proposed sensitive land uses are adequately separated from existing lawful land uses likely to generate dust emissions such as landfill sites, quarries, cropping land, motor sport facilities and other similar dust generating activities.	AO <sub>21.1</sub> Sensitive land uses achieve the separation distances from the nominated uses specified in Table 9.4.2:2.	<b>N/A:</b> The proposed development is not located near any land uses likely to generate dust emissions.
PO <sub>22</sub> Development does not result in dustfall quantities that are likely to impact on the health of surrounding sensitive receivers.	AO <sub>22.1</sub> Dustfall averaged over an annual period of time does not exceed 133mg/m <sup>2</sup> /day when measured at the nearest sensitive receiver.	<b>N/A:</b> The proposed development is not of a nature that will result in significant dust generation.
<b>General Emissions</b>		
PO <sub>23</sub> Air emissions resulting from development do not cause environmental harm (including environmental nuisance).	<p>AO<sub>23.1</sub> The development does not result in air emissions that exceed any of the acceptable levels specified within the <i>Environmental Protection (Air) Policy 2008</i>.</p> <p>AO<sub>23.2</sub> Where a type of air emission is not listed within the <i>Environmental Protection (Air) Policy 2008</i> the proponent can demonstrate that the level of emission is in compliance with Australian ambient air quality standards; or</p> <p>If Australian standards do not exist, an ambient air quality standard from another country or organisation may be used with appropriate justification.</p> <p>AO<sub>23.3</sub> Where a development is proposing to generate and release air emissions in excess of current air quality emission standards the proponent will provide an 'air quality impact assessment'</p>	<b>N/A:</b> The proposed development is not of a nature that will result in air emissions.

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	which adequately addresses the impact of the release and provides justification as to why the industry cannot mitigate the levels further.	
PO <sub>24</sub> Child Care Centres are well located to avoid any harmful impacts from air pollution.	AO <sub>24.1</sub> Maximum concentrations of air pollutants do not exceed those recommended by the National Health and Medical Research Council.	<b>N/A:</b> The proposed development is associated with a Dual Occupancy.
PO <sub>25</sub> Proposed sensitive land uses are adequately separated from existing lawful land uses that produce air emissions.	AO <sub>25.1</sub> Sensitive land uses in relation to air emissions are not located within the separation distances specified in Table 9.4.2:2.	<b>N/A:</b> The proposed development is not located near any land uses likely to generate air emissions.
PO <sub>26</sub> Electromagnetic radiation levels from telecommunications and other facilities do not pose health risks to the community.	AO <sub>26.1</sub> Emission levels from equipment and infrastructure comply with the relevant industry standards as demonstrated through an approved written statement or certification provided by the carrier to council i.e. Electromagnetic Energy report.	<b>N/A:</b> The proposed development is associated with a Dual Occupancy.
PO <sub>27</sub> Air emission vents or stacks are sited appropriately to ensure that surrounding land uses are not exposed to concentrated levels of air contaminants.	AO <sub>27.1</sub> Car park exhaust stacks are located away from adjoining sensitive receivers. AO <sub>27.2</sub> Emissions are discharged vertically and have an exit velocity of at least 10m/second. AO <sub>27.3</sub> Spray booth exhaust stacks are at least 8m in height or 4m higher than the adjoining ridgeline of a neighbouring building (if the building is within 40m of the emission point), whichever is the greater. AO <sub>27.4</sub> Tank venting for hydrocarbon fuel storage and LP Gas is located in accordance with AS1940-2004 <i>The Storage and handling of flammable and combustible liquids (for hydrocarbons)</i> and AS1596:2008 <i>The Storage and Handling of LP</i>	<b>N/A:</b> The proposed development will not involve a carpark exhaust stack. <b>N/A:</b> The proposed development will not involve a carpark exhaust stack. <b>N/A:</b> The proposed development will not involve a spray booth. <b>N/A:</b> The proposed development will not involve the storage or hydrocarbon fuel or LPG.

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	Gas.	
<b>Waste Management</b>		
<p>PO<sub>28</sub> The development (excluding high rise buildings in excess of three (3) stories) is designed to ensure that waste storage and collection can be undertaken in a manner that complies with Council's <i>Technical Guidelines for New Developments Waste Storage and Collection Requirements</i>.</p>	<p>AO<sub>28.1</sub> For commercial premises and industry activities (other than those premises utilising Council's wheelie bin waste collection program):</p> <ul style="list-style-type: none"> <li>(a) general waste and recycling containers are located within the curtilage of the property in an area that enables the waste collection truck to pick up the containers while entering and leaving the premises in a forward gear;</li> <li>(b) a container storage area is dedicated that is large enough to cater for the expected volume of general waste and recycling;</li> <li>(c) storage areas are screened either behind a building or using screening materials or landscaping to a minimum height of 1.5m;</li> <li>(d) where bulk bins (or alternative combined waste and recycling containers exceeding 2 cubic metres) are proposed the bin storage area is roofed and bunded, contains an impervious surface, is in close proximity to a hose cock and is graded and drained to either a wastewater system connection (requiring a trade waste approval) where sewer is available or in sewer areas, storage areas are drained to an area of significant landscaping, waste water treatment device or water quality improvement system e.g. Bioretention;</li> </ul>	<p><b>N/A:</b> The proposed development is not of a nature that will generate commercial or industrial waste.</p>

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	<p>(e) where bulk bins (or alternative combined waste and recycling containers exceeding 2 cubic metres) are proposed the bin storage area is designed to enable bins to be washed out within the storage area and drained to a sewer system (requiring trade waste approval) within sewer areas or area of significant landscaping, water treatment device or water quality improvement system e.g. Bioretention in non-sewered areas; and</p> <p>(f) bin storage areas do not pose amenity issues for surrounding sensitive receivers, including odour during storage periods or noise issues resulting from collection programs.</p>	
	<p>AO<sub>28.2</sub> For a Multiple Dwelling of three (3) – six (6) units the development satisfies one of the following criteria:</p> <p>(a) a minimum road frontage is available within the immediate road reserve adjoining the development in order to place the required number of waste and recycling containers out for collection (2 x 240L wheelie bins per tenement) when calculated at 1m/bin e.g. a development requiring eight (8) bins must have at least 8m of useable road reserve (in terms of bin collection, excluding a 1m clearance around power poles and any area below a street trees canopy where bins cannot</p>	<p><b>N/A:</b> The proposed development is associated with a Dual Occupancy.</p>

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	<p>be collected);</p> <p>OR</p> <ul style="list-style-type: none"> <li>(b) the complex includes a communal bin storage area, whereby the body corporate will implement internal procedures requiring residents to progressively fill bins and only place full bins out for collection; and</li> <li>(c) each tenement has an approved bin storage area that will not create amenity issues for surrounding sensitive land uses; and</li> <li>(d) bin storage areas are screened behind buildings for amenity purposes; and</li> <li>(e) storage areas are not within dwellings (including garages) and it is not necessary to take the bins through dwellings (including garages) for collection purposes; and</li> <li>(f) a hose cock is located in close proximity to the storage location to enable bins to be cleaned; and</li> <li>(g) where a rear storage area is not possible bins are stored in a minimum 1.5m high screened area in the front of the dwelling(s);</li> </ul> <p>OR</p> <ul style="list-style-type: none"> <li>(h) screened communal storage areas (to a minimum height of 1.5m) are proposed which contain an impervious floor, hose cock and grading/drainage towards a grassed area or other porous surface.</li> </ul>	

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	<p>AO<sub>28.3</sub> For a Multiple Dwelling above six (6) units the development satisfies one of the following criteria:</p> <p>(a) The development incorporates 'internal collection' of either bulk bins or wheelie bins (in accordance with the waste management guideline that accompanies the environmental standard);</p> <p>OR</p> <p>(b) Communal bin storage areas contain a roof, bunding and bin 'washing' provisions in the form of either a sewer connection (requiring trade waste approval) or where no sewer is available a connection to a waste water treatment device, drain to an area of significant landscaping or drain to a water quality improvement device e.g. Bioretention system is acceptable;</p> <p>OR</p> <p>(c) Where 'internal collection' is proposed the internal design complies with the waste management guideline that accompanies this environmental standard and a certification from a registered RPEQ has been provided to demonstrate that manoeuvrability is acceptable for an appropriately sized refuse vehicle.</p>	<p><b>N/A:</b> The proposed development is associated with a Dual Occupancy.</p>
	<p>AO<sub>28.4</sub> Commercial premises utilising Council's wheelie bin waste collection service to dispose of commercial waste:</p>	<p><b>N/A:</b> The proposed development is associated with a Dual Occupancy.</p>

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	<ul style="list-style-type: none"> <li>(a) utilise a maximum of four (4) wheelie bins i.e. less than 1 cubic metre;</li> <li>(b) store bins within the curtilage of the property in a designated area in close proximity to a hose cock, whereby any adjoining sensitive land uses will not experience amenity issues i.e. odour;</li> <li>(c) store bins on an impervious surface;</li> <li>(d) place bins on the road reserve for a maximum period of 24 hours during collection programs; and</li> <li>(e) store bins in an area that is screened from public view either in a building, behind a building or within a purpose built screened storage area within a 1.5m minimum height.</li> </ul>	
PO <sub>29</sub> Development for a Community Residence, Residential Care Facility or Retirement Facility utilising communal bin storage areas ensure that residents have reasonable access to waste containers, where the development is for aged care purposes.	AO <sub>29.1</sub> For development involving a Community Residence, Residential Care Facility or Retirement Facility, bin storage areas are located within reasonable proximity to all units, in accordance with Council's Environmental Guideline.	<b>N/A:</b> The proposed development is associated with a Dual Occupancy.
PO <sub>30</sub> High rise (in excess of three (3) storeys) residential developments and joint commercial and residential developments are designed to enable best practice waste management principles to be applied.	AO <sub>30.1</sub> The applicant has provided a waste management plan that as a minimum has addressed the following issues: <ul style="list-style-type: none"> <li>(a) likely waste quantity and waste type to be generated on site on a weekly basis;</li> <li>(b) likely recycling quantities to be generated on a weekly basis;</li> <li>(c) waste container and recycling container (type and volume) requirements for the</li> </ul>	<b>N/A:</b> The proposed development is associated with a Dual Occupancy.

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Environmental Standards Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>residential component (based on 240L of general waste and 240L of recycling per unit) and the commercial component (if applicable);</p> <p>(d) waste storage area locations;</p> <p>(e) dual waste chutes for general waste and recycling;</p> <p>(f) bin room specifications and hygiene practices for waste handling areas, chutes, waste containers and other applicable equipment;</p> <p>(g) collection arrangements and manoeuvring diagrams (including overhead clearances);</p> <p>(h) waste minimisation practices;</p> <p>(i) use of compactors;</p> <p>(j) an impact assessment of waste management practices on any surrounding sensitive land uses;</p> <p>(k) air extraction fans, refrigeration or associated devices for refuse storage areas to prevent odour, particularly where putrescibles are stored; and</p> <p>(l) clinical and related waste storage and collection issues (if applicable).</p>	
<p>PO<sub>31</sub> Demolition and building activities actively involve waste minimisation and waste avoidance principles including the promotion of recycling and re-use.</p>	<p>AO<sub>31.1</sub> The development will be carried out in accordance with the waste management hierarchy outlined in the <i>Technical Guideline for New Developments Waste Storage and Collection Requirements</i> and the applicant has nominated the quantity and type of materials that will be disposed of to landfill.</p>	<p><b>Complies:</b> Construction will be carried out in accordance with the relevant standards to ensure waste is minimised.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Environmental Standards Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>32</sub> Development that involves the generation of 'clinical and related waste' as per the definition of the <i>Technical Guideline for New Developments Waste Storage and Collection Requirements</i> is designed to adequately cater for legislative storage and collection requirements.</p>	<p>Where involving development that involves the generation of 'clinical and related waste' as per the definition of the <i>Technical Guideline for New Developments Waste Storage and Collection Requirements</i>:</p> <p>AO<sub>32.1</sub> The storage of 'clinical and related waste' is in accordance with the <i>Technical Guideline for New Developments Waste Storage and Collection Requirements</i> with storage locations being demonstrated on submitted site/floor plans.</p> <p>AO<sub>32.2</sub> The development has proposed a method of disposing of 'clinical and related waste' and has demonstrated that an applicable waste collection vehicle is able to manoeuvre on site, while entering and leaving the premises in a forward gear.</p>	<p><b>N/A:</b> The proposed development will not involve the generation of clinical and related waste.</p>
<p>PO<sub>33</sub> Residential development involving 'internal collection' of either bulk bins or wheelie bins is designed to a standard that enables heavy vehicle access and manoeuvring whilst providing safety to residents and the protection of infrastructure.</p>	<p>AO<sub>33.1</sub> The development is designed and certified by a RPEQ and complies with the requirements outlined in Council's <i>Technical Guidelines for Waste Storage and Collection Requirements for New Developments</i>, including:</p> <ul style="list-style-type: none"> <li>(a) appropriate manoeuvring is adequate with vehicles being able to enter and exit the property in a forward gear;</li> <li>(b) overhead clearance is adequate for the applicable refuse vehicle;</li> <li>(c) road surface is appropriate for a HRV;</li> <li>(d) side clearance is appropriate for wheelie bin collection;</li> </ul>	<p><b>N/A:</b> The proposed development is associated with a Dual Occupancy that will utilise Council's kerbside collection.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Environmental Standards Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(e) collection areas are appropriate for either bulk bins or wheelie bins;</li> <li>(f) minimum road width of 5.5m; and</li> <li>(g) internal road networks enable the refuse vehicle to traverse the site without resident safety being jeopardised.</li> </ul>	
<p>PO<sub>34</sub> Development involving refuse storage and collection external to Council's waste contract utilise waste containers and hygiene practices that prevent odour issues and remove harbourage opportunities for vermin and mosquitoes.</p>	<p>AO<sub>34.1</sub> The applicant will utilise the following control measures:</p> <ul style="list-style-type: none"> <li>(a) putrescibles waste will be removed from the property at intervals not exceeding seven (7) days (putrescibles will be refrigerated where possible and appropriate);</li> <li>(b) tight fitting lid assemblies will be utilised on all waste containers to prevent the pooling of rainwater, thus minimising mosquito breeding opportunities; and</li> <li>(c) bins will be secured to ensure that vermin and pest animals do not have access to a potential food source; and</li> <li>(d) bins will be cleaned on an 'as needed' basis if odour is identified as an issue.</li> </ul>	<p><b>N/A:</b> The site will utilise Council's residential kerbside service.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Integrated Water Cycle Management Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes or compliance outcomes if made applicable to compliance assessable development	Compliance summary
<b>Stormwater Management</b>		
PO <sub>1</sub> Development does not adversely impact on the quality of receiving waters by avoiding or minimising pollutants entering and being transported with stormwater.	AO <sub>1.1</sub> Stormwater quality treatment measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .  AO <sub>1.2</sub> Pollutant load reductions are achieved in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies:</b> Stormwater quality treatment measures will be implemented in accordance with the relevant standards.  <b>Complies:</b> Pollutant load reductions will be achieved in accordance with the relevant standards.
PO <sub>2</sub> Adverse impacts of construction activities on stormwater quality are avoided or minimised using best practice environmental management for erosion and sediment control.	AO <sub>2.1</sub> Sediment and erosion control measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies:</b> Sediment and erosion control measures will be implemented in accordance with the relevant standards where appropriate.
PO <sub>3</sub> Stormwater management incorporates water sensitive urban design techniques and avoids adverse impacts from water quantity, flow rates and duration and frequency in receiving waters, having regard to: (a) channel, bed and bank stability; (b) aquatic and riparian ecosystems; and (c) hydrological functions.	AO <sub>3.1</sub> Stormwater flow control measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies:</b> Stormwater flow control measures will be implemented in accordance with the relevant standards; refer to the SWMP at <b>Appendix E</b> .
<b>Waste Water Management</b>		
PO <sub>4</sub> Development does not discharge waste water to a waterway or external to the site unless demonstrated to be best practice environmental management for that site and has appropriate regard for:	AO <sub>4.1</sub> Waste water management measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies:</b> Wastewater will be managed in accordance with the relevant standards.

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Integrated Water Cycle Management Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes or compliance outcomes if made applicable to compliance assessable development	Compliance summary
<ul style="list-style-type: none"> <li>(a) cumulative effects;</li> <li>(b) the applicable water quality objectives for the receiving waters;</li> <li>(c) adverse impact on ecosystem health of receiving waters; and</li> <li>(d) in waters mapped as being of high ecological value, the adverse impacts of such releases and their offset.</li> </ul>		
<b>Artificial Waterways and Water Bodies</b>		
PO <sub>5</sub> The waterway or water body is designed to integrate multiple functions, including: <ul style="list-style-type: none"> <li>(a) aesthetics, landscaping, and recreation;</li> <li>(b) flood management;</li> <li>(c) stormwater management;</li> <li>(d) water conservation and reuse;</li> <li>(e) community health; and</li> <li>(f) pest management.</li> </ul>	AO <sub>5.1</sub> Artificial waterways or water bodies are designed in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development will not involve artificial waterways or water bodies.
PO <sub>6</sub> The waterway is located and designed to be responsive to natural drainage features.	AO <sub>6.1</sub> Artificial waterways or water bodies are designed in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development will not involve artificial waterways or water bodies.
PO <sub>7</sub> The waterway or body is designed to minimise whole of life cycle costs.	AO <sub>7.1</sub> Artificial waterways or water bodies are designed in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development will not involve artificial waterways or water bodies.
<b>Flooding and Drainage</b>		

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Integrated Water Cycle Management Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes or compliance outcomes if made applicable to compliance assessable development	Compliance summary
PO <sub>8</sub> Flooding and drainage characteristics upstream or downstream of the site are not worsened.	AO <sub>8.1</sub> Development is undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies:</b> The proposed development will be designed in accordance with the relevant standards.
PO <sub>9</sub> The drainage network has sufficient capacity to safely convey stormwater run-off from the site.	AO <sub>9.1</sub> Development is undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies:</b> The proposed development will be designed in accordance with the relevant standards.
PO <sub>10</sub> Stormwater resulting from roofed areas is collected and discharged in a manner that does not adversely affect the stability of buildings or the use of adjacent land.	AO <sub>10.1</sub> Roof water is collected and discharged in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies:</b> The proposed development will be designed in accordance with the relevant standards.
<b>Water Cycle Management</b>		
PO <sub>11</sub> The design and management of the development integrates water cycle elements so that: (a) water is used efficiently and potable water demand is reduced; (b) wastewater production is minimised; (c) stormwater peak discharges and runoff volumes are not worsened; (d) natural drainage lines and hydrological regimes are maintained as far as possible; (e) large, uninterrupted impervious surfaces are minimised; (f) reuse of stormwater and grey-water is encouraged where public health and safety will not be compromised; and (g) water is used efficiently.	AO <sub>11.1</sub> Integrated water management practices and infrastructure are implemented in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies:</b> Integrated water management practices and infrastructure will be implemented in accordance with the relevant standards.

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Landscaping Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>1</sub> Landscape design is developed by a suitably qualified landscape professional and demonstrates an integrated approach to planning/development issues and documents both hard and soft works proposed for the development.	AO1.1 Landscape documentation is prepared by the landscape professional identified in Table 9.4.4:2.	<b>Complies:</b> Due to the site's Major Centre Zone location and the expectation that a high site impervious coverage is achieved, a landscaping plan has not been provided as part of this application. Documentation prepared by a relevant professional will be provided following approval of the proposed development, if required.
PO <sub>2</sub> Landscape construction is undertaken by a suitably qualified landscape professional.	AO <sub>2.1</sub> Landscape construction is carried out by a member of the Queensland Association of Landscape Industries.	<b>Complies:</b> Landscaping will be carried out by a suitably qualified person.
PO <sub>3</sub> Landscape design reflects the local context and incorporates cohesive and desirable aspects of the prevailing landscape character. (Desirable aspects are those considered necessary to maintain and enhance the character, setting and/or ambience, and ecological values of the location.)	AO <sub>3.1</sub> Where a street or locality has an identifiable character derived from existing vegetation, similar or identical plant species are used. AO <sub>3.2</sub> Existing desirable landscape elements and treatments are incorporated into landscaping to integrate the development into the existing character of the area. AO <sub>3.3</sub> Existing site trees are integrated into the development. AO <sub>3.4</sub> Species selection is reflective of cool temperate species.	<b>Complies:</b> Landscaping will be designed to incorporate species that are consistent with the surrounding locality. <b>Complies:</b> Landscaping will be designed to integrate with the established character of the surrounding locality. <b>Complies:</b> The proposed development integrates existing site trees where possible. The proposed development has also been designed to retain the existing street trees. <b>Complies:</b> Species used in landscaping will be selected to reflect a cool temperate climate.
PO <sub>4</sub> Where the development involves the creation of a new road street tree planting is undertaken having consideration of:	Where the development involves the creation of a new road:	<b>N/A:</b> The proposed development does not involve the creation of a new road.

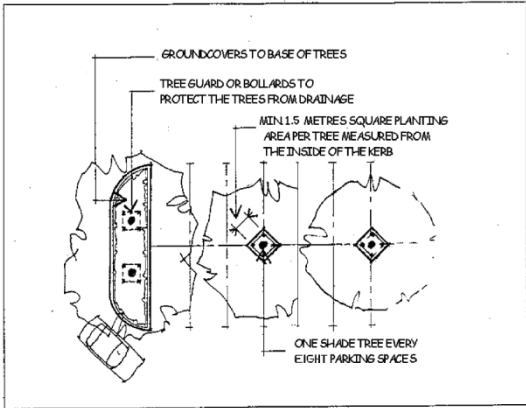
## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Landscaping Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(a) the hierarchy and function of the street;</li> <li>(b) selection of appropriate species;</li> <li>(c) avoidance of conflict between the street tree and utilities and services within the road reserve;</li> <li>(d) soil conditions;</li> <li>(e) existing street trees;</li> <li>(f) solar access; and</li> <li>(g) driveway access.</li> </ul>	<p>AO<sub>4.1</sub> Street planting is carried out in accordance with the requirements of <i>SC6.2 PSP No. 2 Engineering Services Infrastructure Roads and Drainage</i>.</p> <p>AO<sub>4.2</sub> Species and materials are used that minimise the use of potable water.</p> <p>AO<sub>4.3</sub> Street tree planting is in accordance with PSP No.8 – Street Trees.</p>	
<p>PO<sub>5</sub> Fencing design and acoustic barriers:</p> <ul style="list-style-type: none"> <li>(a) are compatible with the existing streetscape and proposed development type; and</li> <li>(b) provide visual interest and address the street.</li> </ul>	<p>AO<sub>5.1</sub> Front fences longer than 15m and greater than 1,400mm in height are visually fragmented with recesses at least 1.2m deep and 1.2m wide at 15m intervals, planted with at least one tree and groundcovers.</p> <p>AO<sub>5.2</sub> All planting and recesses along a fence are located within the property boundary and planting recesses are accessible from within the site.</p> <p>AO<sub>5.3</sub> Where acoustic fencing is required by the planning scheme it is designed by an acoustic engineer and incorporates a minimum 3m vegetated buffer on either side of the fence with vegetation having a mature height equal to or above the height of the acoustic fencing.</p>	<p><b>Complies:</b> Fencing along the front boundary has a height of 1.2m.</p> <p><b>Complies:</b> The front fencing will be located within the property boundary.</p> <p><b>N/A:</b> The proposed development does not involve acoustic fencing.</p>
<p>PO<sub>6</sub> Location, design and provision of planting in carparks and internal roadways achieve a high degree of shade, amenity and safety.</p>	<p>AO<sub>6.1</sub> Landscaping visually fragments and shades carparking areas with regular tree planting in individual planting bays evenly distributed throughout the car parking area at the rate of one planting bay per eight (8) carparking spaces.</p>	<p><b>N/A:</b> The proposed development does not involve carparking areas or internal roadways.</p>

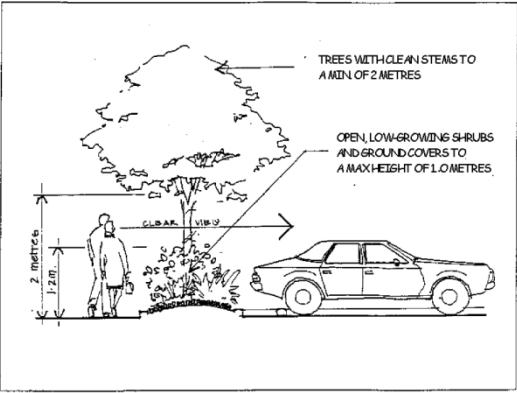
Toowoomba Regional Planning Scheme 2012 (v28.0)

Landscaping Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>6.2</sub> Individual planting bays have a minimum dimension of 1,500 x 1,500mm with permeable surface treatments and are flush with the finished surface levels of the car park.</p> <p>AO<sub>6.3</sub> No raised kerbing is provided around planting bays. Wheelstops or bollards are used to delineate planting bays where necessary and finished carpark surface levels fall toward planting areas.</p> <p>AO<sub>6.4</sub> Planting bays incorporate ground covers less than 1,000mm height that allow unobstructed surveillance.</p>	
		

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Landscaping Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
		
<p>PO<sub>7</sub> Location and habit of tree planting must not interfere with the function and accessibility of any adjacent utility services.</p>	<p>AO<sub>7.1</sub> Species mature height and siting must not interfere with or compromise overhead and underground utility assets including stormwater inlet pits.</p> <p>AO<sub>7.2</sub> Tree planting must be a minimum of 2m from any mains water easements and offset 4m from any sewer main or inspection chamber.</p>	<p><b>Complies:</b> Species used in landscaping will be selected to ensure that interference with infrastructure assets does not occur.</p> <p><b>Complies:</b> New tree planting will achieve relevant setbacks to underground services.</p>
<p>PO<sub>8</sub> Maintenance access points must be considered and accommodated for in the site planning and design process.</p>	<p>AO<sub>8.1</sub> Access by appropriate maintenance or utility vehicles must be demonstrated with ground surface treatments that are stable and usable in all weather.</p> <p>AO<sub>8.2</sub> Functional maintenance vehicle circulation and access gates to be provided.</p>	<p><b>N/A:</b> The proposed development will not require access by maintenance or utility vehicles.</p>
<p>PO<sub>9</sub> On-site stormwater harvesting is to be maximised for irrigating landscaping in development with reuse measures and amelioration of stormwater impacts provided.</p>	<p>AO<sub>9.1</sub> Landscape design takes advantage of the flow of water along overland flow paths.</p>	<p><b>Complies:</b> Landscaping will be designed to take advantage of overland flow where possible.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Landscaping Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>9.2</sub> Landscaping is used to help maximise opportunities for on-site stormwater infiltration by:</p> <ul style="list-style-type: none"> <li>(a) minimising impervious surfaces and incorporating semi-permeable paving products;</li> <li>(b) falling hard surfaces towards pervious surfaces such as turf or mulched areas;</li> <li>(c) maximise opportunities for turf and planting areas;</li> <li>(d) align planting areas parallel to contours to slow the flow of surface water; and</li> <li>(e) ensure planting palette comprises canopy tree species.</li> </ul> <p>AO<sub>9.3</sub> Provision for drainage is incorporated through treatments such as subsurface drains, swales, ponds and infiltration cells.</p> <p>AO<sub>9.4</sub> Sediment and erosion control measures are provided.</p> <p>AO<sub>9.5</sub> Planter boxes on podiums and building forecourts are plumbed to stormwater.</p>	<p><b>Complies:</b> Landscaping will be designed where possible to integrate opportunities for stormwater infiltration as outlined in AO<sub>9.2</sub>.</p> <p><b>N/A:</b> The proposed development does not involve the provision of subsurface drains, swales, ponds or infiltration cells.</p> <p><b>Complies:</b> Sediment and erosion control measures can be provided as required.</p> <p><b>N/A:</b> The proposed development does not involve planter boxes.</p>
PO <sub>10</sub> Landscape design is integrated with any existing urban design theme within the surrounding area and coordinates paving, planting, street furniture, lighting, signage and other elements to reflect that theme and assist in the creation of a sense of place.	No acceptable outcome is nominated.	<b>Complies:</b> Landscaping will be designed to integrate with the existing urban design theme of the surrounding area where appropriate.
PO <sub>11</sub> Design of pedestrian paths and places reinforces the desired character of the area and/or place and includes features to enhance	AO <sub>11.1</sub> Design complies with AS1428 parts 1, 2, 3, and 4 – <i>Design for Access and Mobility</i>	<b>N/A:</b> The proposed development does not involve pedestrian pathways.

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Landscaping Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>their use that are of universal design to ensure non-discriminatory access and use.</p>		
<p>PO<sub>12</sub> Risks to personal safety and the potential for crime, vandalism and fear are reduced through landscape design that has been informed by <i>Crime Prevention Through Environmental Design</i> (CPTED) principles in relation to:</p> <ul style="list-style-type: none"> <li>(a) Surveillance.</li> <li>(b) Access control.</li> <li>(c) Territorial reinforcement.</li> <li>(d) Space management.</li> </ul>	<p>Landscape design incorporates the following design measures:</p> <p>AO<sub>12.1</sub> The attractiveness of crime targets is minimised by providing opportunities for effective surveillance through: clear sight lines from private to public space, reducing concealment or entrapment opportunities, public facilities (toilets, shelters etc) located to promote use, dual access points, avoiding blind corners, and lighting where appropriate.</p> <p>AO<sub>12.2</sub> Barriers are used to attract, channel or restrict the movement of people by: clear spatial definition and legibility, optimising opportunity for public interaction, visually permeable screens and fencing, appropriate use of mechanical measures that correspond to actual risk.</p> <p>AO<sub>12.3</sub> Reinforcing definition of territory and ownership of private, semi-public and public spaces through: clear design cues for use and activities, transitions and boundaries between public and private, design that encourages public interaction and ownership, legible universal signage.</p> <p>AO<sub>12.4</sub> Space Management: ensuring that public spaces are appropriately utilised and maintained by the use of vandal- and graffiti-</p>	<p><b>Complies:</b> Landscaping will be designed to ensure that the attractiveness of crime targets is minimised and will ensure that clear sightlines throughout the development are provided.</p> <p><b>Complies:</b> Landscaping can be designed to incorporate barriers where relevant.</p> <p><b>N/A:</b> The proposed development does not involve semi-public and public spaces.</p> <p><b>N/A:</b> The proposed development does not involve public spaces.</p>

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Landscaping Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	resistant materials, easily accessed and maintained fixtures.	

Toowoomba Regional Planning Scheme 2012 (v28.0)

**Transport, Access and Parking Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Driveway Crossovers</b>		
<p>PO<sub>1</sub> Vehicular access arrangements, including driveway crossovers:</p> <p>(a) are appropriate for:</p> <p>(i) the capacity of the parking area;</p> <p>(ii) the volume, frequency and type of vehicle useage; and</p> <p>(iii) the function and configuration of the access road;</p> <p>(b) minimise any potentially adverse impact on:</p> <p>(i) the safety and efficiency of the road and pedestrian/cycle paths;</p> <p>(ii) the safety and efficiency of the road and footpath users;</p> <p>(iii) the integrity of any infrastructure within the road reserve; and</p> <p>(iv) the safety of access to adjacent properties.</p> <p>(c) protect the amenity of premises in the vicinity by:</p> <p>(i) maintaining the predominant vehicular access pattern in the street, including consistent width, grade and location;</p> <p>(ii) preserving the residential amenity of the streetscape, including noise and visual impact, and consideration of existing landscaping by considering:</p>	<p>AO<sub>1.1</sub> Vehicular access and driveway crossovers are not:</p> <p>(a) an additional site/property access;</p> <p>(b) to a State-controlled Road or a road with bluestone kerbing;</p> <p>(c) within 25 m of a signalised road intersection;</p> <p>(d) within 20m of an unsignalised road intersection in a Commercial or Industrial Area;</p> <p>(e) within 10m of an unsignalised road intersection in a Community, Residential, Rural or Other Area;</p> <p>(f) within 1m of any infrastructure, including street signage, power poles, street lights, manholes, stormwater gully pits, or other Council/public utility asset;</p> <p>(g) within the Tree Protection Zone, as defined by Australian Standard 4970- 2009;</p> <p>(h) for a lot with a frontage of 10m or less;</p> <p>(i) greater than 4m in width when for a lot with a frontage / width of more than 10m but less than 20m; and</p> <p>(j) greater than 6m in width when for a lot with a frontage / width of greater than 20m.</p> <p><i>Note: An additional site access is considered to be more than one site access.</i></p>	<p><b>Complies:</b> The proposed development incorporates a single 5m wide vehicular crossover from Short Street. The access is not an additional property access and is not located on a State-controlled road or a road containing bluestone kerbing. The crossover is located greater than 10m from the nearest unsignalised intersection, is clear of existing infrastructure and public utility assets, and is not within a Tree Protection Zone. Vehicular access from Railway Street is not practical due to the presence of established street trees and insufficient separation distances from the intersection with Short Street.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

**Transport, Access and Parking Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(A) use of materials which integrate with the streetscape (e.g. existing crossovers and driveways, etc);</p> <p>(B) minimising the width and grade of the access;</p> <p>(C) minimising impacts on the appearance of the streetscape by retaining existing vegetation, including approved landscaping; and</p> <p>(D) locating the access to minimise the impact of vehicle noise on neighbouring/adjointing properties.</p>	<p>AO1.2 Except where in a Rural Zone, vehicular access and driveway crossovers:</p> <p>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</p> <p>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the roadway;</p> <p>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, bus stop, or other structure within the road reserve</p> <p>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</p> <p>(e) do not have access restricted by an access restriction strip or link reserve;</p> <p>(f) do not access an unformed or unkerbed road;</p> <p>(g) are constructed from reinforced concrete;</p> <p>(h) are perpendicular to the road edge; and</p> <p>(i) are provided in accordance with the <i>Australian Standard AS 2890.1 – Off Street Car Parking</i> and <i>Australian Standard AS 2890.2 (where relevant)</i> and the relevant standard drawing in SC6.2 PSP No. 2 <i>Engineering Standards – Roads and Drainage Infrastructure</i>.</p>	

Toowoomba Regional Planning Scheme 2012 (v28.0)

**Transport, Access and Parking Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO1.3 Where in a Rural Zone, vehicular access and driveway crossovers:</p> <ul style="list-style-type: none"> <li>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</li> <li>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the roadway;</li> <li>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, loading bay, bus stop, or other structure within the road reserve;</li> <li>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</li> <li>(e) do not have access restricted by an access restriction strip or link reserve;</li> <li>(f) are sealed where accessing a sealed formed road;</li> <li>(g) are perpendicular to the road edge; and</li> <li>(h) are provided in accordance with the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure.</li> </ul>	

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**Transport, Access and Parking Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Car Parking Provision</b>		
<p>PO<sub>2</sub> Provision is made for on-site vehicle parking to meet the demand likely to be generated by the development and to avoid on-street parking where that would adversely impact on the safety or capacity of the road network or unduly impact on local amenity.</p> <p><i>Note: Where the development does not meet the acceptable outcomes, or where no acceptable outcome is specified, a parking demand analysis report prepared by a suitably qualified person may assist in demonstrating compliance with the performance outcome.</i></p>	<p>AO<sub>2.1</sub> Where in the Principal Centre Zone or Mixed Use Zone Car parking is provided at the rate of:</p> <ul style="list-style-type: none"> <li>(a) Non-Residential Use one (1) parking space per 50m<sup>2</sup> of GFA; and</li> <li>(b) Residential Use - one (1) parking space per dwelling.</li> </ul> <p>AO<sub>2.2</sub> Where not in the Principal Centre Zone or Mixed Use Zone Car parking is provided at the rates set out in Table 9.4.6:3 to this Code.</p> <p><i>Note: Where a parking rate for a use is unspecified in Table 9.4.6:3 – no acceptable outcome is provided.</i></p> <p><i>Note: If the number of car parking spaces calculated in accordance with AO<sub>2.1</sub> and AO<sub>2.2</sub> is not a whole number, the number of parking spaces to be provided is rounded-up to next highest whole number.</i></p> <p><i>Note: Where application is made for establishment of two or more uses on the same premises, the parking demand is calculated by totalling the requirements for each use.</i></p>	<p><b>N/A:</b> The site is not located within the Mixed Use or Principal Centre Zones.</p> <p><b>Complies:</b> The proposed development is provided with carparking that complies with Table 9.4.6:3.</p>

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Transport Network</b>		
PO <sub>1</sub> The development is located on roads that are appropriate for the nature of traffic generated, having regard to the safety and efficiency of the transport network, and the functions and characteristics identified in the transport network hierarchy contained in SC 6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	No acceptable outcome is nominated.	<b>Complies:</b> The proposed development is located on Collector and Local Roads which are considered sufficient to cater for the traffic expected to be generated by the use. It is noted that access is provided to Short Street in accordance with Council's advice obtained during the Prelodgement phase of the project.
PO <sub>2</sub> Development does not compromise the orderly provision or upgrading of the transport network.	No acceptable outcome is nominated.	<b>Complies:</b> The proposed development is not of a nature that would compromise the future upgrading of traffic networks.
PO <sub>3</sub> Onsite transport network infrastructure (including roads, parking, access and public transport, pedestrian and cyclist facilities) appropriately integrates with surrounding networks and facilitates the orderly development of adjoining land.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development does not involve onsite transport network infrastructure
PO <sub>4</sub> Development is designed to encourage travel by public transport, walking and cycling. This may include integrated access between adjoining sites and/or the provision of mid-block connections which are safe, functional and legible for potential users.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development is not of a nature that warrants the use of public or active transport modes. There is no public transport service available in Pittsworth.
PO <sub>5</sub> Car parking areas, pathways and other elements of transport network infrastructure are designed to enhance public safety by discouraging crime and anti-social behaviour, having regard to:	AO <sub>5.1</sub> Car parking areas, pathways and other elements of transport network infrastructure are designed in accordance with <i>Crime Prevention Through Environmental Design (CPTED) Guidelines</i> .	<b>N/A:</b> The proposed development does not involve carparking areas, pathways or other transport network infrastructure.

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(a) provision of opportunities for casual surveillance;</li> <li>(b) provision of lighting;</li> <li>(c) the use of fencing to define public and private spaces, whilst allowing for appropriate sightlines;</li> <li>(d) minimising potential concealment points and assault locations;</li> <li>(e) minimising opportunities for graffiti and other vandalism; and</li> <li>(f) restricting unlawful access to buildings and between buildings.</li> </ul>		
<p>PO<sub>6</sub> Directional signage is provided within a development site to assist legibility and way-finding, including for pedestrians and cyclists.</p>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The proposed development does not warrant the provision of directional signage.</p>
<p><b>Access</b></p>		
<p>PO<sub>7</sub> Vehicle access arrangements and queuing areas are appropriate for:</p> <ul style="list-style-type: none"> <li>(a) the capacity of the parking area;</li> <li>(b) the volume, frequency and type of vehicle usage; and</li> <li>(c) the function and characteristics of the access road and adjoining road network.</li> </ul>	<p>AO<sub>7.1</sub> Access driveways and queuing areas are located and designed in accordance with the provisions of <i>Australian Standard AS 2890.1 Part 1: Off Street Carparking</i>.</p>	<p><b>Complies:</b> Access driveways will be designed in accordance with the relevant standards. It is noted that the proposed development does not involve queuing areas.</p>
<p>PO<sub>8</sub> Access arrangements minimise any adverse impact on:</p> <ul style="list-style-type: none"> <li>(a) the integrity of any infrastructure within the road reserve;</li> <li>(b) the safety and convenience of pedestrians and cyclists;</li> </ul>	<p>No acceptable outcome is nominated.</p>	<p><b>Complies:</b> Access arrangements will be designed to ensure that they do not compromise the integrity of infrastructure or street trees within the road reserve. Furthermore, access will not result in safety or amenity impacts; refer to the swept paths and sight line assessment at <b>Appendix F</b>.</p>

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(c) the safety and convenience of access to adjacent properties;</li> <li>(d) the amenity of premises in the vicinity; and</li> <li>(e) street trees in the road reserve.</li> </ul>		
<p>PO<sub>9</sub> Where the nature of the proposed development creates a demand due to the frequency and volume of vehicle movements for the set-down and pick-up of passengers, provision is made for set-down and pick-up facilities by bus, taxis or private vehicle.</p>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The proposed development does not involve a set-down or pick-up area.</p>
<p>PO<sub>10</sub> Where set-down and pick-up facilities for bus, taxis or private vehicles are provided as part of development they are:</p> <ul style="list-style-type: none"> <li>(a) safe for pedestrians, cyclists and vehicles;</li> <li>(b) conveniently connected to the main component of the development by pedestrian pathway; and</li> <li>(c) designed to provide for pedestrian priority and clear sightlines.</li> </ul>	<p>AO<sub>10.1</sub> Bus pick-up/set-down areas:</p> <ul style="list-style-type: none"> <li>(a) allow a bus, based on the Long Rigid Bus (12m) in Austroads/Standards Australia HB72 – Design Vehicles and Turning Path Templates, to turn and manoeuvre in and out of the area in an easy and safe manner;</li> <li>(b) afford maximum safety for passengers boarding or alighting buses;</li> <li>(c) avoid standing or queuing buses from obstructing access to car parking spaces or circulation within the Site; and</li> <li>(d) avoid on-street queuing or boarding/alighting of buses that would reduce traffic flow or safety on the road network. One clear traffic lane in each direction should be maintained.</li> </ul> <p>AO<sub>10.2</sub> Car and taxi pick-up/set-down areas:</p> <ul style="list-style-type: none"> <li>(a) allow a car to manoeuvre in and out of the area in an easy and safe manner;</li> </ul>	<p><b>N/A:</b> The proposed development does not involve a set-down or pick-up area.</p>

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(b) afford maximum safety for passengers boarding or alighting cars;</li> <li>(c) avoid standing or queuing cars from obstructing access to car parking spaces or circulation within the site; and</li> <li>(d) avoid on-street queuing or boarding/alighting of cars that would reduce traffic flow or safety on the road network. One clear traffic lane in each direction should be maintained.</li> </ul>	
<b>Pedestrian and Cycle Facilities</b>		
PO <sub>11</sub> Provision is made for the safe and convenient movement of pedestrians on site and external to the site, having regard to desire lines, legibility, weather protection and the needs of people with disabilities.	AO <sub>11.1</sub> Pedestrian pathways and crossings are provided in accordance with SC6.2 PSP No.2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> . AO <sub>11.2</sub> Access for cyclists and pedestrians is clearly distinguished from vehicle access. AO <sub>11.3</sub> Pedestrian paths of a minimum width of 1.5m are provided through each car parking row and connect to the main entrance(s) to the building(s).	<b>N/A:</b> The proposed development does not involve pedestrian or cyclist facilities.
PO <sub>12</sub> Provision is made for safe and convenient cycle movement to the site and within the site having regard to desire lines, users' needs and legibility.	AO <sub>12.1</sub> Shared paths and on-road cycle lane facilities are provided in accordance with SC6.2 PSP No.2 – <i>Engineering Standards Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development does not involve pedestrian or cyclist facilities.
<b>Parking and Circulation</b>		
PO <sub>13</sub> Car parking areas are designed to be: (a) clearly defined, marked and signed;	AO <sub>13.1</sub> The entry to the car park is clearly signposted.	<b>N/A:</b> The proposed development does not involve a carparking area.

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(b) convenient, safe and accessible; and            (c) safe for vehicles, pedestrians and cyclists and minimise vehicle/pedestrian conflicts by providing clear access lines for pedestrians movement within car park areas.</p>	<p>AO<sub>13.2</sub> Parking spaces are freely available for use by the development's occupants and visitors during the business hours of the use.</p> <p>AO<sub>13.3</sub> Visitor or customer parking spaces are located in the most accessible position to the main entrance of the building and signed as such.</p> <p>AO<sub>13.4</sub> Unless otherwise specified in another code relevant to the development, 60% of the parking spaces for non-residential development are clearly visible from the street.</p> <p>AO<sub>13.5</sub> Public Safety:</p> <ul style="list-style-type: none"> <li>(a) The car park is located where it can be monitored by passers-by or the occupiers of the development.</li> <li>(b) Where the car park is open to the public at night, lighting is provided throughout the car park and along pedestrian access paths in compliance with Australian Standard AS 1158.3.1 – <i>Road Lighting – Pedestrian Area (Category P) Lighting – Performance and Installation Design Requirements</i>.</li> <li>(c) Except in the case of residential development:               <ul style="list-style-type: none"> <li>(i) where the car park is not required at night, entry to the car park is physically restricted; and</li> <li>(ii) where the car park is enclosed, the walls are finished in a light coloured material that reflects light.</li> </ul> </li> <li>(d) Landscaping throughout the car park is provided in a manner, as indicated in the</li> </ul>	

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	Landscaping Code that allows surveillance and minimises the risk of crime.	
PO <sub>14</sub> Car parking areas are designed to provide spaces which meet the needs of people with disabilities.	AO <sub>14.1</sub> Parking spaces for people with disabilities are provided at the rates specified in Appendix C of Australian Standard <i>AS2890.1 Part 1: Off Street Carparking</i> . AO <sub>14.2</sub> Car parking spaces for people with disabilities are located as near as possible to the entrance or entrances of the facility or use they serve. AO <sub>14.3</sub> Parking spaces for people with disabilities are designed in accordance with the provisions of Australian Standard <i>AS2890.1 Part 1: Off Street Carparking</i> . AO <sub>14.4</sub> Pathways and ramps between parking areas and the entrances to buildings are designed in accordance with the provisions of Australian Standard <i>AS1428.1: Design for Access and Mobility</i> . AO <sub>14.5</sub> Parking spaces for people with disabilities are identified by a sign incorporating the International Symbol specified in Australian Standard <i>AS1428.1: Design for Access and Mobility</i> . AO <sub>14.6</sub> The sign is readily visible from a vehicle at the entrance to the carpark, or guide signs are provided to indicate the direction of the disabled parking spaces.	<b>N/A:</b> The proposed development does not involve a carparking area.
PO <sub>15</sub> Car parking areas for non-residential development on a site in, or adjoining, a residential zone, are designed to minimise any	For non-residential development on a site in, or adjoining, a residential zone:	<b>N/A:</b> The proposed development is for a Dual Occupancy.

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Transport, Access and Parking Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
adverse impact on the amenity of premises in the vicinity.	<p>AO<sub>15.1</sub> Car parking and driveway areas are setback a minimum distance of 3m from a side boundary that is common with a residential use in a residential zone.</p> <p>AO<sub>15.2</sub> Landscape planting is used between the car park and driveway areas and the side boundary to soften the visual impacts of car park areas and to provide shade.</p> <p>AO<sub>15.3</sub> An acoustic fence of 1.8m height is provided along the property boundary that is common with a residential use in a residential zone.</p>	
PO <sub>16</sub> Car parking and associated access arrangements are located and designed to avoid dominating the road frontage of the site or otherwise detracting from streetscape character.	AO <sub>16.1</sub> Car parking is provided either at the rear of the development or beneath buildings.	<b>N/A:</b> The proposed development does not involve a carparking area.
PO <sub>17</sub> Above ground or multi-level car parking areas are designed, articulated and finished to make a positive contribution to the local streetscape character.	AO <sub>17.1</sub> Above ground or multi-level parking areas are designed, articulated and finished to a quality equal to or better than adjoining buildings.	<b>N/A:</b> The proposed development does not involve a carparking area.
PO <sub>18</sub> Landscaping is provided to soften the visual impact of car parking areas and to provide shading and protection from glare.	<p>AO<sub>18.1</sub> Aesthetics, glare, heat absorption and re-radiation.</p> <p>(a) Landscaping is provided throughout the car park in the manner and at the rate indicated in the Landscaping Code; and</p> <p>(b) Unless otherwise specified in a zone, precinct or use code, where the car park adjoins a street frontage, or a boundary with a Residential or other sensitive land use,</p>	<b>N/A:</b> The proposed development does not involve a carparking area.

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	a landscaped strip of minimum 3 m width is provided along the frontage/boundary.	
PO <sub>19</sub> Any parking, access and any other vehicle access/manoeuvring areas incorporate design measures to avoid dust nuisance to surrounding properties.	AO <sub>19.1</sub> Car parking, access and any other vehicle access/manoeuvring areas vehicle manoeuvring areas are imperviously sealed.	<b>N/A:</b> The proposed development does not involve a carparking area.
PO <sub>20</sub> Noise impacts from vehicle movement areas on any adjoining residential or other sensitive land use are mitigated.	AO <sub>20.1</sub> A solid, good quality brick, timber or masonry fence of a minimum 1.8m height is constructed between any vehicle movement areas and a boundary to an adjoining residential or other sensitive land use.	<b>N/A:</b> The proposed development does not involve a carparking area.
PO <sub>21</sub> Any part of the parking area designated as a vehicle cleaning or repair area is designed and constructed to avoid adverse impact on water quality or Council's wastewater or stormwater infrastructure.	AO <sub>21.1</sub> The development is capable of meeting the requirements of Council's <i>Trade Waste Policy</i> and the Trade Waste Environmental Management Plan.	<b>N/A:</b> The proposed development does not involve a carparking area.
<b>Servicing</b>		
PO <sub>22</sub> Provision is made for the on-site loading, unloading, manoeuvring and access by service vehicles that: (a) is adequate to meet the demands generated by the development; (b) is able to accommodate the design service vehicle requirements; and (c) does not unduly impede vehicular, cyclist and pedestrian safety and convenience within the site.	AO <sub>22.1</sub> The service bays provided and access to them, can accommodate, at any one time, the types and numbers of service vehicles detailed in Table 9.4.6:3. AO <sub>22.2</sub> Service bays provided wholly or partly within a building are physically separated from the rest of the buildings floor space in manner that makes it impractical to use them as storage or work areas. AO <sub>22.3</sub> The design and provision of access driveways, manoeuvring areas and loading and unloading facilities for service vehicles complies with	<b>N/A:</b> The proposed development will not require access by service vehicles.

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>Australian Standard AS 2890.2 – 1989 – <i>Off Street Parking – Commercial Vehicle Facilities.</i></p> <p>AO<sub>22.4</sub> Vehicles being loaded or unloaded with goods stand completely on-site and do not impede access to more than 6 parking spaces or 50% of the on-site parking spaces (whichever is the lesser) while doing so.</p> <p>AO<sub>22.5</sub> Service vehicles can enter and leave the site in a forward gear.</p>	
PO <sub>23</sub> Refuse collection vehicles are able to access on-site refuse collection facilities.	AO <sub>23.1</sub> Where an on-site refuse area is provided, access and manoeuvring areas are designed and provided to enable access by refuse collection vehicle based on the Design Service Vehicle in Austroads/Standards Australia HB72 – Design Vehicles and Turning Path Templates.	<b>N/A:</b> The proposed development will not require access by service vehicles.
PO <sub>24</sub> Servicing arrangements minimise any adverse impact the amenity of premises in the vicinity.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development will not require access by service vehicles and will utilise Council's existing kerbside collection service.
PO <sub>25</sub> Servicing arrangements are located and designed to avoid dominating the road frontage of the site or otherwise detracting from streetscape character.	AO <sub>25.1</sub> Areas used for servicing are not located at the front of developments, or are otherwise screened to minimise visual intrusion in the streetscape.	<b>N/A:</b> The proposed development will not require access by service vehicles.

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### Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Utilities</b>		
<p>PO<sub>1</sub> A water supply is provided that is adequate for the current and future needs of the intended use.</p>	<p>AO<sub>1.1</sub> Where within a water supply area, the development is connected to Council's reticulated water supply system in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>OR</p> <p>AO<sub>1.2</sub> Where not in a water supply area, the development is provided with an on site water supply in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>AO<sub>1.3</sub> Water supply systems and connections are designed and constructed in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p>	<p><b>Complies:</b> The proposed development will be connected to Council's reticulated water supply infrastructure.</p>
<p>PO<sub>2</sub> Wastewater treatment and disposal is provided that is appropriate for the level of demand generated, protects public health and avoids environmental harm.</p>	<p>AO<sub>2.1</sub> Where within a wastewater area, the development is connected to the Council's reticulated wastewater system in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>OR</p> <p>AO<sub>2.2</sub> Where not within a wastewater area, on-site wastewater treatment and disposal is provided which complies with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>AO<sub>2.3</sub> Waste water systems and connections are designed and constructed in accordance with</p>	<p><b>Complies:</b> The proposed development will be connected to Council's reticulated wastewater infrastructure.</p>

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### Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure.</i>	
PO <sub>3</sub> The development is equipped with an adequate energy supply approved by and installed in accordance with the standards of the relevant energy regulatory authority.	AO <sub>3.1</sub> Premises are connected to an electricity supply approved by the relevant energy regulatory authority.	<b>Complies:</b> The proposed development will be connected to electricity infrastructure.
PO <sub>4</sub> Premises are connected to a telecommunications service approved by the relevant telecommunication regulatory authority.	AO <sub>4.1</sub> The development is connected to telecommunications infrastructure in accordance with the standards of the relevant regulatory authority.	<b>Complies:</b> The proposed development will be connected to telecommunications infrastructure.
PO <sub>5</sub> Provision is made for future telecommunications services (e.g. fibre optic cable).	AO <sub>5.1</sub> Conduits are provided in accordance with <i>SC6.2 PSP No.2 Engineering Standards – Roads and Drainage Infrastructure.</i>	<b>N/A:</b> The site is in an area that is fully serviced.
PO <sub>6</sub> Development near utility services does not: (a) adversely affect the function of the service; or (b) place an additional load on the service; and (c) protects the infrastructure from physical damage; and (d) allows ongoing necessary access for maintenance purposes.	AO <sub>6.1</sub> Setbacks and loadings comply with <i>the Queensland Development Code QDC MP1.4.</i>	<b>Performance Outcome:</b> Unit 1 is proposed to be built over the sewer which runs east-west through the site. Alternative layouts were considered, however the sewer is not located close enough to the northern boundary to be within the front setback, nor centrally-enough to be located between two buildings. The proposed dwelling is a Class 1a and as such, Council's practice has been to allow construction over sewers by Class 1a dwellings.
PO <sub>7</sub> Infrastructure is integrated with and efficiently extends existing networks.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development does not involve extensions to infrastructure networks.
PO <sub>8</sub> Water meter/s are installed and located for easy access by the relevant authority.	AO <sub>8.1</sub> Water meter/s are installed in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure.</i>	<b>Complies:</b> Water meters will be installed in accordance with the relevant standards.

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### Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Movement Networks</b>		
PO <sub>9</sub> Premises are provided with the following works along the full extent of the road frontage and to a standard that is appropriate to the function of the road and the character of the locality: <ul style="list-style-type: none"> <li>(a) appropriate roadway treatment;</li> <li>(b) appropriate pavement edging (including kerb and channel);</li> <li>(c) safe vehicular access;</li> <li>(d) safe footpaths and bikeways;</li> <li>(e) street scaping or street tree planting;</li> <li>(f) stormwater drainage; and</li> <li>(g) street lighting systems.</li> </ul>	AO <sub>9.1</sub> Design and construction of external road works are undertaken in accordance with <i>SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure</i> .  AO <sub>9.2</sub> Footpaths and bikeways are provided in accordance with the <i>Austrroads Guide to Road Design – Part 6A: Pedestrian and Cyclist Paths (Austrroads 2009m)</i> .	<b>Complies:</b> External roadworks will be undertaken in accordance with the relevant standards.  <b>N/A:</b> The proposed development does not involve the provision of footpaths or bikeways.
PO <sub>10</sub> Provision is made in the road reserve for street scaping, pedestrians and cyclists in a manner consistent with: <ul style="list-style-type: none"> <li>(a) the current and projected level of usage;</li> <li>(b) the desired streetscape character; and</li> <li>(c) activities which are anticipated to occur within the verge.</li> </ul>	AO <sub>10.1</sub> Street scaping works, footpaths and cycle paths are provided in accordance with <i>PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure</i> .  AO <sub>10.2</sub> Footpaths and bikeways are provided in accordance with the <i>Austrroads Guide to Road Design – Part 6A: Pedestrian and Cyclist Paths (Austrroads 2009m)</i> .	<b>N/A:</b> The proposed development does not involve street scaping works, footpaths or cycle paths.  <b>N/A:</b> The proposed development does not involve the provision of footpaths or bikeways.
PO <sub>11</sub> Parking areas are constructed in a manner that is sufficiently durable for the intended function, maintains all-weather access and ensures the safe passage of vehicles, pedestrians and cyclists.	AO <sub>11.1</sub> Parking area design and construction is undertaken in accordance with the Transport, Access and Parking Code.	<b>N/A:</b> The proposed development does not involve the provision of a carparking area.

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### Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>12</sub> Movement networks can be easily and efficiently maintained.	AO <sub>12.1</sub> Infrastructure is provided in accordance with SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure.	<b>N/A:</b> The proposed development does not involve the provision of movement network infrastructure.
<b>Vehicular Access – Non-residential</b>		
PO <sub>13</sub> Non-residential vehicular access arrangements to a public roadway, including driveway crossovers: <ul style="list-style-type: none"> <li>(a) are safe and do not compromise the efficiency, function, convenience of use or capacity of the road network;</li> <li>(b) are located and designed to:               <ul style="list-style-type: none"> <li>(i) avoid damage to utility services, pathways, krebs, road pavement and seal and other council/public infrastructure;</li> <li>(ii) prevent conflict between vehicles and cyclists and pedestrians; and</li> <li>(iii) avoid damage to existing street trees and retain space for the future planting of street trees within the verge.</li> </ul> </li> <li>(c) minimise the number of vehicular access per site to avoid the loss of on-street carparking spaces, and are appropriately separated from other vehicular accesses and side property boundaries to prevent interference with:               <ul style="list-style-type: none"> <li>(i) the safety, capacity and operations of the existing or planned road network;</li> <li>(ii) any existing vehicular accesses;</li> <li>(iii) adjoining properties; and</li> </ul> </li> </ul>	AO <sub>13.1</sub> Non-residential vehicular access and driveway crossovers to a public road are not: <ul style="list-style-type: none"> <li>(a) an additional site/property access;</li> <li>(b) to a State-controlled Road or a road with bluestone kerbing;</li> <li>(c) within 25m of a signalised road intersection;</li> <li>(d) within 20m of an unsignalised road intersection in a Commercial or Industrial Area;</li> <li>(e) within 10m of an unsignalised road intersection in a Community, Residential, Rural or Other Area;</li> <li>(f) for a lot with a frontage of 10m or less;</li> <li>(g) within 1m of any infrastructure, including street signage, power poles, street lights, manholes, stormwater gully pits, or other Council/public utility asset; and</li> <li>(h) within the Tree Protection Zone, as defined by Australian Standard 4970-2009.</li> </ul> <p><i>Note: An additional site access is considered to be more than one site access.</i></p> AO <sub>13.2</sub> Non-residential vehicular access and driveway crossovers:	<b>N/A:</b> The proposed development is for a Dual Occupancy.

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### Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(iv) cycleways or pedestrian footpaths and their users.</li> <li>(d) protect the amenity of premises in the vicinity and surrounding streetscape by:               <ul style="list-style-type: none"> <li>(i) use of materials which integrate with the streetscape (e.g. existing crossovers and driveways, etc);</li> <li>(ii) minimising the width and grade of the access; and</li> <li>(iii) minimising impacts on the appearance of the streetscape by retaining existing vegetation, including approved landscaping.</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</li> <li>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the road reserve;</li> <li>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, loading bay, bus stop, or other structure within the road reserve;</li> <li>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</li> <li>(e) do not have access restricted by an access restriction strip or link reserve;</li> <li>(f) do not access an unformed or unkerbed road;</li> <li>(g) are constructed from reinforced concrete;</li> <li>(h) are perpendicular to the road edge;</li> <li>(i) retain space for the planting of street trees at a minimum rate of one (1) per lot frontage; and</li> <li>(j) are provided in accordance with the Australian Standard AS 2890.1 – Off Street Car Parking and Australian Standard AS 2890.2 (where relevant), the relevant</li> </ul>	

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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure and Australian Standard AS 4970-2009 – Protection of Trees on Development Sites.</p>	
<b>Vehicular Access – Residential</b>		
<p>PO<sub>14</sub> Residential vehicular access arrangements, including driveway crossovers:</p> <ul style="list-style-type: none"> <li>(a) are appropriate for:               <ul style="list-style-type: none"> <li>(i) the capacity of the parking area;</li> <li>(ii) the volume, frequency and type of vehicle useage; and</li> <li>(iii) the function and configuration of the access road.</li> </ul> </li> <li>(b) minimise any potentially adverse impact on:               <ul style="list-style-type: none"> <li>(i) the safety and efficiency of the road and pedestrian/cycle paths;</li> <li>(ii) the safety and efficiency of the road and footpath users;</li> <li>(iii) the integrity of any infrastructure within the road reserve; and</li> <li>(iv) the safety of access to adjacent properties.</li> </ul> </li> <li>(c) protect the amenity of premises in the vicinity by:               <ul style="list-style-type: none"> <li>(i) maintaining the predominant vehicular access pattern in the street, including consistent width, grade and location;</li> </ul> </li> </ul>	<p>AO<sub>14.1</sub> Residential vehicular access and driveway crossovers are not:</p> <ul style="list-style-type: none"> <li>(a) an additional site/property access;</li> <li>(b) to a State-controlled Road or a road with bluestone kerbing;</li> <li>(c) within 25 m of a signalised road intersection;</li> <li>(d) within 20m of an unsignalised road intersection in a Commercial or Industrial Area;</li> <li>(e) within 10m of an unsignalised road intersection in a Community, Residential, Rural or Other Area;</li> <li>(f) within 1m of any infrastructure, including street signage, power poles, street lights, manholes, stormwater gully pits, or other Council/public utility asset;</li> <li>(g) within the Tree Protection Zone, as defined by Australian Standard 4970-2009;</li> <li>(h) for a lot with a frontage of 10m or less;</li> <li>(i) greater than 4m in width when for a lot with a frontage/width of more than 10m but less than 20m; and</li> </ul>	<p><b>Complies:</b> The proposed development incorporates a single 5m wide vehicular crossover from Short Street. The access is not an additional property access and is not located on a State-controlled road or a road containing bluestone kerbing. The crossover is located greater than 10m from the nearest unsignalised intersection, is clear of existing infrastructure and public utility assets, and is not within a Tree Protection Zone.</p> <p>Vehicular access from Railway Street is not practical due to the presence of established street trees and insufficient separation distances from the intersection with Short Street.</p>

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### Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(ii) preserving the residential amenity of the streetscape, including noise and visual impact, and consideration of existing landscaping by considering:</p> <p>(A) use of materials which integrate with the streetscape (e.g. existing crossovers and driveways, etc);</p> <p>(B) minimising the width and grade of the access;</p> <p>(C) minimising impacts on the appearance of the streetscape by retaining existing vegetation, including approved landscaping; and</p> <p>(D) locating the access to minimise the impact of vehicle noise on neighbouring/adjoining properties.</p>	<p>(j) greater than 6m in width when for a lot with a frontage/width of greater than 20m.</p> <p><i>Note: An additional site access is considered to be more than one site access.</i></p> <p>AO<sub>14.2</sub> Except where in a Rural Zone, residential vehicular access and driveway crossovers:</p> <p>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</p> <p>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the roadway;</p> <p>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, bus stop, or other structure within the road reserve;</p> <p>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</p> <p>(e) do not have access restricted by an access restriction strip or link reserve; or</p> <p>(f) do not access an unformed or unkerbed road;</p> <p>(g) are constructed from reinforced concrete;</p> <p>(h) are perpendicular to the road edge; and</p>	

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### Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(i) are provided in accordance with the <i>Australian Standard AS 2890.1 – Off Street Car Parking and Australian Standard AS 2890.2 (where relevant), the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure and Australian Standard AS 4970-2009 –Protection of Trees on Development Sites.</i></p> <p>AO<sub>14.3</sub> Where in a Rural Zone, residential vehicular access and driveway crossovers:</p> <ul style="list-style-type: none"> <li>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</li> <li>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the roadway;</li> <li>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, loading bay, bus stop, or other structure within the road reserve;</li> <li>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</li> <li>(e) do not have access restricted by an access restriction strip or link reserve;</li> </ul>	

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### Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	(f) are sealed where accessing a sealed formed road; (g) are perpendicular to the road edge; and (h) are provided in accordance with the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure.	
<b>Earthworks and Retaining Walls</b>		
PO <sub>15</sub> Earthworks result in stable landforms and structures.	AO <sub>15.1</sub> Earthworks and the construction of retaining walls and batters are undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies:</b> Earthworks will be undertaken in accordance with the relevant standards.
PO <sub>16</sub> Earthworks do not result in the contamination of land or water and avoid risk to people and property.	AO <sub>16.1</sub> Earthworks are undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies:</b> Earthworks will be undertaken in accordance with the relevant standards.
PO <sub>17</sub> Earthworks are undertaken in a manner that: (a) maintains natural landforms; (b) minimises height of retaining walls and batter faces; (c) does not unduly impact on the amenity or privacy for occupants of the site or on adjoining land; and (d) does not unduly impact on the amenity of the streetscape.	AO <sub>17.1</sub> Earthworks and the construction of retaining walls and batters are undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies:</b> Earthworks will be undertaken in accordance with the relevant standards.
PO <sub>18</sub> Earthworks do not create or worsen any flooding or drainage problems on the site or on neighbouring properties.	AO <sub>18.1</sub> Earthworks and the construction of retaining walls and batters are undertaken in	<b>Complies:</b> Earthworks will be undertaken in accordance with the relevant standards.

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### Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	
PO <sub>19</sub> Earthworks do not prevent access or create difficult access to the property.	AO <sub>19.1</sub> Earthworks and the construction of retaining walls and batters ensure driveways can be provided in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies:</b> Earthworks will be undertaken in accordance with the relevant standards.
PO <sub>20</sub> Earthworks do not cause a significant impact on the amenity of the locality or along routes taken to transport material as a result of truck volumes, dust or noise.	AO <sub>20.1</sub> Earthworks are undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies:</b> Earthworks will be undertaken in accordance with the relevant standards.
PO <sub>21</sub> The transportation of material minimises adverse impact on the road system.	AO <sub>21.1</sub> Material is transported in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies:</b> Material will be transported to the site in accordance with the relevant standards.
<b>Waste Management</b>		
PO <sub>22</sub> Where relevant, the development is capable of providing for the storage, collection, treatment and disposal of trade waste such that: <ul style="list-style-type: none"> <li>(a) off-site releases of contaminants do not occur;</li> <li>(b) the health and safety of people and the environment are protected; and</li> <li>(c) the performance of the wastewater system is not put at risk.</li> </ul>	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development will not involve the generation of trade waste.
PO <sub>23</sub> Appropriate refuse container storage areas are provided which are:	AO <sub>23.1</sub> Refuse container storage areas are provided which:	<b>Complies:</b> Each unit will be provided with refuse container storage areas that are screened from public view by fencing and can be imperviously sealed as

## Toowoomba Regional Planning Scheme 2012 (v28.0)

### Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(a) in a building or enclosing structure or screened from public view;</li> <li>(b) of adequate size to accommodate the expected amount of refuse to be generated by the use;</li> <li>(c) in a position that is conveniently accessible for collection; and</li> <li>(d) able to be kept in a clean state at all times.</li> </ul>	<ul style="list-style-type: none"> <li>(a) are in a building, outbuilding or other enclosed structure, or otherwise screened from public view, by a minimum 1.5 m high solid fence or wall or dense vegetation;</li> <li>(b) are provided with an imperviously sealed pad, on which to stand the bin(s), that is drained to an approved waste disposal system;</li> <li>(c) are within normal hose length of a hose cock;</li> <li>(d) are large enough to accommodate at least one (1) standard sized container per dwelling and, in commercial and industrial premises, one (1) or more industrial bins of a size appropriate to the nature and scale of use; and</li> <li>(e) are situated not closer than 6m to a road or 2m to any site boundary.</li> </ul> <p>AO<sub>23.2</sub> On sites greater than 2,000m<sup>2</sup> in area, provision is made for refuse collection vehicles to access the collection area and to enter and leave the site in a forward direction without having to make more than a 3-point turn.</p> <p>AO<sub>23.3</sub> For multiple dwelling and retirement facility, container storage areas are located not more than 50m from any dwelling.</p>	<p>required. Storage areas will be of a sufficient size to accommodate each required container.</p>
<p>PO<sub>24</sub> Where the use is non-residential and generates recyclable waste, provision is made for conveniently located recycling bins on the</p>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The proposed development is for a Dual Occupancy.</p>

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### Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
premises, including in the refuse container storage area.		
<b>Construction Management</b>		
PO <sub>25</sub> Work is undertaken in a manner which minimises adverse impacts on vegetation that is to be retained.	AO <sub>25.1</sub> Works include, at a minimum: (a) installation of protective fencing around retained vegetation during construction; (b) erection of advisory signage; (c) no disturbance, due to earthworks or storage of plant, materials and equipment, of ground level and soils below the canopy of any retained vegetation; and (d) removal from the site of all declared noxious weeds and environmental weeds.	<b>N/A:</b> The proposed development will not retain the vegetation on the site during construction.
PO <sub>26</sub> Work is undertaken in a manner which does not cause unacceptable impacts on surrounding areas as a result of dust, odour, noise or lighting.	AO <sub>26.1</sub> Construction is undertaken in accordance with the Environmental Standards Code.	<b>Complies:</b> The proposed development will be constructed in accordance with the Environmental Standards Code.
PO <sub>27</sub> While undertaking development works, the site and adjoining road are maintained in a tidy, safe and hygienic manner.	AO <sub>27.1</sub> Construction is undertaken in accordance with the Environmental Standards Code.	<b>Complies:</b> The proposed development will be constructed in accordance with the Environmental Standards Code.
PO <sub>28</sub> Traffic, parking and delivery of construction materials generated during construction are managed to minimise impact on the amenity of the surrounding area and to manage the safety of pedestrians, cyclists and motorists.	AO <sub>28.1</sub> Construction is undertaken in accordance with the Environmental Standards Code.	<b>Complies:</b> The proposed development will be constructed in accordance with the Environmental Standards Code.
PO <sub>29</sub> Council and state infrastructure is not damaged by construction activities.	AO <sub>29.1</sub> Construction, alterations and any repairs to infrastructure is undertaken in accordance with the SC6.2 PSP No.2 Engineering Standards –	<b>N/A:</b> The proposed development will not involve any construction, alterations or repairs to infrastructure.

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**Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p><i>Roads and Drainage Infrastructure, Queensland Development Code QDC MP1.4, and, where applicable, in consultation with the relevant service authority.</i></p> <p>AO<sub>29.2</sub> Construction, alterations and any repairs to State-controlled roads and rail corridors are undertaken in accordance with the <i>Transport Infrastructure Act 1994</i>.</p>	<p><b>N/A:</b> The proposed development will not involve any changes to a State-controlled road or rail corridor.</p>

# State code 1: Development in a state-controlled road environment

State Development Assessment Provisions guideline - State Code 1: Development in a state-controlled road environment. This guideline provides direction on how to address State Code 1.

**Table 1.1 Development in general**

Performance outcomes	Acceptable outcomes	Response
<b>Buildings, structures, infrastructure, services and utilities</b>		
<b>PO1</b> The location of the development does not create a safety hazard for users of the <b>state-controlled road</b> .	<b>AO1.1</b> Development is not located in a <b>state-controlled road</b> .  AND <b>AO1.2</b> Development can be maintained without requiring access to a <b>state-controlled road</b> .	<b>Complies:</b> The proposed development will not be located in a state-controlled road.  <b>Complies:</b> The proposed development will not be located in a state-controlled road.
<b>PO2</b> The design and construction of the development does not adversely impact the <b>structural integrity</b> or physical condition of the <b>state-controlled road</b> or <b>road transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development will not impact the structural integrity or physical condition of a state-controlled road or transport infrastructure.
<b>PO3</b> The location of the development does not obstruct <b>road transport infrastructure</b> or adversely impact the operating performance of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development will not obstruct or impact the operating performance of a state-controlled road or road infrastructure.
<b>PO4</b> The location, placement, design and operation of advertising devices, visible from the <b>state-controlled road</b> , do not create a	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development does not involve advertising devices.

Performance outcomes	Acceptable outcomes	Response
safety hazard for users of the <b>state-controlled road</b> .		
<b>PO5</b> The design and construction of buildings and <b>structures</b> does not create a safety hazard by distracting users of the <b>state-controlled road</b> .	<p><b>AO5.1</b> Facades of buildings and <b>structures</b> fronting the <b>state-controlled road</b> are made of non-reflective materials.</p> <p>AND</p> <p><b>AO5.2</b> Facades of buildings and <b>structures</b> do not direct or reflect point light sources into the face of oncoming traffic on the <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO5.3</b> External lighting of buildings and <b>structures</b> is not directed into the face of oncoming traffic on the <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO5.4</b> External lighting of buildings and <b>structures</b> does not involve flashing or laser lights.</p>	<p><b>Complies:</b> The proposed development will be designed and constructed as to not cause a safety hazard. The proposed development will not be visible to the state-controlled road.</p> <p><b>Complies:</b> The proposed development will be designed and constructed as to not cause a safety hazard. The proposed development will not be visible to the state-controlled road.</p> <p><b>Complies:</b> The proposed development will be designed and constructed as to not cause a safety hazard. The proposed development will not be visible to the state-controlled road.</p> <p><b>Complies:</b> The proposed development will not involve flashing or laser lights. The proposed development will not be visible to the state-controlled road.</p>
<b>PO6</b> Road, pedestrian and bikeway bridges over a <b>state-controlled road</b> are designed and constructed to prevent projectiles from being thrown onto the <b>state-controlled road</b> .	<b>AO6.1</b> Road, pedestrian and bikeway bridges over the <b>state-controlled road</b> include throw protection screens in accordance with section 4.11 of the Design Criteria for Bridges and Other Structures Manual, Department of Transport and Main Roads, 2020.	<b>N/A:</b> The proposed development will not involve road, pedestrian or bikeway bridges over a state-controlled road.
<b>Landscaping</b>		
<b>PO7</b> The location of landscaping does not create a safety hazard for users of the <b>state-controlled road</b> .	<b>AO7.1</b> Landscaping is not located in a <b>state-controlled road</b> .	<b>Complies:</b> The proposed development will not involve landscaping within a state-controlled road.

Performance outcomes	Acceptable outcomes	Response
	<p>AND</p> <p><b>AO7.2</b> Landscaping can be maintained without requiring access to a <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO7.3</b> Landscaping does not block or obscure the sight lines for vehicular access to a <b>state-controlled road</b>.</p>	<p><b>Complies:</b> The proposed development will not involve landscaping that requires access to a state-controlled road for maintenance.</p> <p><b>Complies:</b> The proposed development will not involve landscaping that will block or obscure sight lines to a state-controlled road.</p>
<b>Stormwater and overland flow</b>		
<b>PO8</b> Stormwater run-off or overland flow from the development site does not create or exacerbate a safety hazard for users of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development will not involve stormwater run-off or overland flow to a state-controlled road or road infrastructure.
<b>PO9</b> Stormwater run-off or overland flow from the development site does not result in a material worsening of the operating performance of the <b>state-controlled road</b> or <b>road transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development will not involve stormwater run-off or overland flow to a state-controlled road or road infrastructure.
<b>PO10</b> Stormwater run-off or overland flow from the development site does not adversely impact the <b>structural integrity</b> or physical condition of the <b>state-controlled road</b> or <b>road transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development will not involve stormwater run-off or overland flow to a state-controlled road or road infrastructure.
<b>PO11</b> Development ensures that stormwater is lawfully discharged.	<p><b>AO11.1</b> Development does not create any new points of discharge to a <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO11.2</b> Development does not concentrate flows to a <b>state-controlled road</b>.</p> <p>AND</p>	<p><b>Complies:</b> The proposed development will not involve stormwater run-off or overland flow to a state-controlled road or road infrastructure.</p> <p><b>Complies:</b> The proposed development will not involve stormwater run-off or overland flow to a state-controlled road or road infrastructure.</p>

State Development Assessment Provisions v3.5

State code 1: Development in a state-controlled road environment

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Performance outcomes	Acceptable outcomes	Response
	<p><b>AO11.3</b> Stormwater run-off is discharged to a <b>lawful point of discharge</b>.</p> <p>AND</p> <p><b>AO11.4</b> Development does not worsen the condition of an existing <b>lawful point of discharge</b> to the <b>state-controlled road</b>.</p>	<p><b>Complies:</b> The proposed development will involve stormwater run-off to a lawful point of discharge.</p> <p><b>N/A:</b> The proposed development does not involve an existing lawful point of discharge to a state-controlled road.</p>
<b>Flooding</b>		
<p><b>PO12</b> Development does not result in a material worsening of flooding impacts within a <b>state-controlled road</b>.</p>	<p><b>AO12.1</b> For all flood events up to 1% <b>annual exceedance probability</b>, development results in negligible impacts (within +/- 10mm) to existing flood levels within a <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO12.2</b> For all flood events up to 1% <b>annual exceedance probability</b>, development results in negligible impacts (up to a 10% increase) to existing peak velocities within a <b>state-controlled road</b>.</p> <p>AND</p> <p><b>AO12.3</b> For all flood events up to 1% <b>annual exceedance probability</b>, development results in negligible impacts (up to a 10% increase) to existing time of submergence of a <b>state-controlled road</b>.</p>	<p><b>Complies:</b> The proposed development does not involve stormwater discharge to a state-controlled road or any works on-site that will channel flows toward the state-controlled road.</p> <p><b>Complies:</b> See response to AO12.1.</p> <p><b>Complies:</b> See response to AO12.1.</p>

State Development Assessment Provisions v3.5

State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
<b>Drainage Infrastructure</b>		
<b>PO13</b> Drainage infrastructure does not create a safety hazard for users in the <b>state-controlled road</b> .	<b>AO13.1</b> Drainage infrastructure is wholly contained within the development site, except at the <b>lawful point of discharge</b> .  AND <b>AO13.2</b> Drainage infrastructure can be maintained without requiring access to a <b>state-controlled road</b> .	<b>Complies:</b> The proposed development will involve all drainage infrastructure contained within the development site.  <b>Complies:</b> The proposed development will not involve drainage infrastructure that requires access to a state-controlled road for maintenance.
<b>PO14</b> Drainage infrastructure associated with, or within, a <b>state-controlled road</b> is constructed, and designed to ensure the <b>structural integrity</b> and physical condition of existing drainage infrastructure and the surrounding drainage network.	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development does not involve drainage infrastructure with or within a state-controlled road.

**Table 1.2 Vehicular access, road layout and local roads**

Performance outcomes	Acceptable outcomes	Response
<b>Vehicular access to a state-controlled road or within 100 metres of a state-controlled road intersection</b>		
<b>PO15</b> The location, design and operation of a <b>new or changed access</b> to a <b>state-controlled road</b> does not compromise the safety of users of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development does not involve access to a state-controlled road and the dual occupancy will not compromise the safety of users of the state-controlled road.
<b>PO16</b> The location, design and operation of a <b>new or changed access</b> does not adversely impact the <b>functional requirements</b> of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development does not involve access to a state-controlled road and the minor scale of the development will not adversely impact the functional requirements of the road.
<b>PO17</b> The location, design and operation of a <b>new or changed access</b> is consistent with the <b>future intent</b> of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development does not involve access to a state-controlled road or impact future upgrades or plans for the road.

Performance outcomes	Acceptable outcomes	Response
<p><b>PO18 New or changed access</b> is consistent with the access for the relevant <b>limited access road policy</b>:</p> <ol style="list-style-type: none"> <li><b>LAR 1</b> where direct access is prohibited; or</li> <li><b>LAR 2</b> where access may be permitted, subject to assessment.</li> </ol>	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development does not involve access to a state-controlled road or to a limited access road.
<p><b>PO19 New or changed access</b> to a <b>local road</b> within 100 metres of an intersection with a <b>state-controlled road</b> does not compromise the safety of users of the <b>state-controlled road</b>.</p>	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development does not involve access to a state-controlled road and the new driveway within 100m of the road will not compromise safety of users of the SCR.
<p><b>PO20 New or changed access</b> to a <b>local road</b> within 100 metres of an intersection with a <b>state-controlled road</b> does not adversely impact on the operating performance of the intersection.</p>	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development does not involve access to a state-controlled road and the new driveway within 100m of the road will not impact the operating performance of the SCR.
<b>Public passenger transport and active transport</b>		
<p><b>PO21</b> Development does not compromise the safety of users of <b>public passenger transport infrastructure, public passenger services and active transport infrastructure</b>.</p>	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development is not in proximity to public passenger or active transport infrastructure.
<p><b>PO22</b> Development maintains the ability for people to access <b>public passenger transport infrastructure, public passenger services and active transport infrastructure</b>.</p>	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development is not in proximity to public passenger or active transport infrastructure.
<p><b>PO23</b> Development does not adversely impact the operating performance of <b>public passenger transport infrastructure, public passenger services and active transport infrastructure</b>.</p>	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development is not in proximity to public passenger or active transport infrastructure.
<p><b>PO24</b> Development does not adversely impact the <b>structural integrity</b> or physical condition of <b>public passenger transport infrastructure and active transport infrastructure</b>.</p>	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development is not in proximity to public passenger or active transport infrastructure.

### Table 1.3 Network impacts

Performance outcomes	Acceptable outcomes	Response
<b>PO25</b> Development does not compromise the safety of users of the <b>state-controlled road network</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development does not involve activities that will impact on the safety of users on the state-controlled road.
<b>PO26</b> Development ensures <b>no net worsening</b> of the operating performance of the <b>state-controlled road network</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development does not involve activities that will impact on the operating performance of the state-controlled road.
<b>PO27</b> Traffic movements are not directed onto a <b>state-controlled road</b> where they can be accommodated on the <b>local road network</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development does not involve traffic movements directed onto a state-controlled road where they can be accommodated on the local road network.
<b>PO28</b> Development involving haulage exceeding 10,000 tonnes per year does not adversely impact the pavement of a <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development does not involve haulage.
<b>PO29</b> Development does not impede delivery of <b>planned upgrades of state-controlled roads</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development is not in proximity to a planned upgrade to a state-controlled road.
<b>PO30</b> Development does not impede delivery of <b>corridor improvements</b> located entirely within the <b>state-controlled road corridor</b> .	No acceptable outcome is prescribed.	<b>Complies:</b> The proposed development is not in proximity to a planned upgrade to a state-controlled corridor.

### Table 1.4 Filling, excavation, building foundations and retaining structures

Performance outcomes	Acceptable outcomes	Response
<b>PO31</b> Development does not create a safety hazard for users of the <b>state-controlled road or road transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development does not involve activities that will create a safety hazard for users of the state-controlled road or transport infrastructure.
<b>PO32</b> Development does not adversely impact the operating performance of the <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development does not involve activities that will adversely impact the operating performance of the state-controlled road.

Performance outcomes	Acceptable outcomes	Response
<b>PO33</b> Development does not undermine, damage or cause subsidence of a <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development does not involve activities that will undermine, damage or cause subsidence of the state-controlled road.
<b>PO34</b> Development does not cause ground water disturbance in a <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development does not involve activities that will cause ground water disturbance of the state-controlled road.
<b>PO35</b> Excavation, boring, piling, blasting and fill compaction do not adversely impact the physical condition or <b>structural integrity</b> of a <b>state-controlled road</b> or <b>road transport infrastructure</b> .	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development does not involve excavation, boring, piling, blasting or fill compaction that will adversely impact state transport infrastructure.
<b>PO36</b> Filling and excavation associated with the construction of <b>new or changed access</b> do not compromise the operation or capacity of existing drainage infrastructure for a <b>state-controlled road</b> .	No acceptable outcome is prescribed.	<b>N/A:</b> The proposed development does not involve a new or changed access to a state-controlled road.

## Table 1.5 Environmental emissions

Statutory note: Where a **state-controlled road** is co-located in the same transport corridor as a railway, the development should instead comply with Environmental emissions in State code 2: Development in a railway environment.

Performance outcomes	Acceptable outcomes	Response
<b>Reconfiguring a lot</b>		
<b>Involving the creation of 5 or fewer new residential lots adjacent to a state-controlled road or type 1 multi-modal corridor</b>		
<b>PO37</b> Development minimises free field noise intrusion from a <b>state-controlled road</b> .	<b>AO37.1</b> Development provides a noise barrier or earth mound which is designed, sited and constructed: <ol style="list-style-type: none"> <li>1. to achieve the maximum free field acoustic levels in reference table 2 (item 2.1);</li> <li>2. in accordance with: <ol style="list-style-type: none"> <li>a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic</li> </ol> </li> </ol>	<b>N/A:</b> The subject site is not located adjacent to a SCR.

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Performance outcomes	Acceptable outcomes	Response
	<p>Noise), Department of Transport and Main Roads, 2013;</p> <p>b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</p> <p>c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020.</p> <p>OR</p> <p><b>AO37.2</b> Development achieves the maximum free field acoustic levels in reference table 2 (item 2.1) by <b>alternative noise attenuation measures</b> where it is not practical to provide a noise barrier or earth mound.</p> <p>OR</p> <p><b>AO37.3</b> Development provides a <b>solid gap-free fence</b> or other <b>solid gap-free structure</b> along the full extent of the boundary closest to the <b>state-controlled road</b>.</p>	<p><b>N/A:</b> The subject site is not located adjacent to a SCR.</p> <p><b>N/A:</b> The subject site is not located adjacent to a SCR.</p>
<b>Involving the creation of 6 or more new residential lots adjacent to a state-controlled road or type 1 multi-modal corridor</b>		
<p><b>PO38</b> Reconfiguring a lot minimises free field noise intrusion from a <b>state-controlled road</b>.</p>	<p><b>AO38.1</b> Development provides noise barrier or earth mound which is designed, sited and constructed:</p> <ol style="list-style-type: none"> <li>1. to achieve the maximum free field acoustic levels in reference table 2 (item 2.1);</li> <li>2. in accordance with: <ol style="list-style-type: none"> <li>a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic</li> </ol> </li> </ol>	<p><b>N/A:</b> The subject site is not located adjacent to a SCR.</p>

Performance outcomes	Acceptable outcomes	Response
	<p>Noise), Department of Transport and Main Roads, 2013;</p> <p>b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</p> <p>c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020.</p> <p>OR</p> <p><b>AO38.2</b> Development achieves the maximum free field acoustic levels in reference table 2 (item 2.1) by <b>alternative noise attenuation measures</b> where it is not practical to provide a noise barrier or earth mound.</p>	
<b>Material change of use (accommodation activity)</b>		
<b>Ground floor level requirements adjacent to a state-controlled road or type 1 multi-modal corridor</b>		
<p><b>PO39</b> Development minimises noise intrusion from a <b>state-controlled road in private open space</b>.</p>	<p><b>AO39.1</b> Development provides a noise barrier or earth mound which is designed, sited and constructed:</p> <ol style="list-style-type: none"> <li>1. to achieve the maximum free field acoustic levels in reference table 2 (item 2.2) for <b>private open space</b> at the ground floor level;</li> <li>2. in accordance with: <ol style="list-style-type: none"> <li>a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013;</li> <li>b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</li> </ol> </li> </ol>	<p><b>N/A:</b> The subject site is not located adjacent to a SCR.</p>

Performance outcomes	Acceptable outcomes	Response
	<p>c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020.</p> <p>OR</p> <p><b>AO39.2</b> Development achieves the maximum free field acoustic level in reference table 2 (item 2.2) for <b>private open space</b> by <b>alternative noise attenuation measures</b> where it is not practical to provide a noise barrier or earth mound.</p>	
<p><b>PO40</b> Development (excluding a <b>relevant residential building</b> or <b>relocated building</b>) minimises noise intrusion from a <b>state-controlled road</b> in <b>habitable rooms</b> at the facade.</p>	<p><b>AO40.1</b> Development (excluding a <b>relevant residential building</b> or <b>relocated building</b>) provides a noise barrier or earth mound which is designed, sited and constructed:</p> <ol style="list-style-type: none"> <li>1. to achieve the maximum building façade acoustic level in reference table 1 (item 1.1) for <b>habitable rooms</b>;</li> <li>2. in accordance with: <ol style="list-style-type: none"> <li>a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013;</li> <li>b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</li> <li>c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020.</li> </ol> </li> </ol> <p>OR</p>	<p><b>N/A:</b> The subject site is not located adjacent to a SCR.</p>

Performance outcomes	Acceptable outcomes	Response
	<b>AO40.2</b> Development (excluding a <b>relevant residential building</b> or <b>relocated building</b> ) achieves the maximum building façade acoustic level in reference table 1 (item 1.1) for <b>habitable rooms</b> by <b>alternative noise attenuation measures</b> where it is not practical to provide a noise barrier or earth mound.	
<b>PO41 Habitable rooms</b> (excluding a <b>relevant residential building</b> or <b>relocated building</b> ) are designed and constructed using materials to achieve the maximum internal acoustic level in reference table 3 (item 3.1).	No acceptable outcome is provided.	<b>N/A:</b> The subject site is not located adjacent to a SCR.
<b>Above ground floor level requirements (accommodation activity) adjacent to a state-controlled road or type 1 multi-modal corridor</b>		
<b>PO42</b> Balconies, podiums, and roof decks include: 1. a continuous <b>solid gap-free structure</b> or balustrade (excluding gaps required for drainage purposes to comply with the Building Code of Australia); 2. highly acoustically absorbent material treatment for the total area of the soffit above balconies, podiums, and roof decks.	No acceptable outcome is provided.	<b>N/A:</b> The subject site is not located adjacent to a SCR.
<b>PO43 Habitable rooms</b> (excluding a <b>relevant residential building</b> or <b>relocated building</b> ) are designed and constructed using materials to achieve the maximum internal acoustic level in reference table 3 (item 3.1).	No acceptable outcome is provided.	<b>N/A:</b> The subject site is not located adjacent to a SCR.
<b>Material change of use (other uses)</b>		
<b>Ground floor level requirements (childcare centre, educational establishment, hospital) adjacent to a state-controlled road or type 1 multi-modal corridor</b>		
<b>PO44</b> Development: 1. provides a noise barrier or earth mound that is designed, sited and constructed: a. to achieve the maximum free field acoustic level in reference table 2 (item	No acceptable outcome is provided.	<b>N/A:</b> The subject site is not located adjacent to a SCR.

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Performance outcomes	Acceptable outcomes	Response
<p>2.3) for all <b>outdoor education areas</b> and <b>outdoor play areas</b>;</p> <p>b. in accordance with:</p> <ul style="list-style-type: none"> <li>i. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013;</li> <li>ii. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</li> <li>iii. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020; or</li> </ul> <p>2. achieves the maximum free field acoustic level in reference table 2 (item 2.3) for all <b>outdoor education areas</b> and <b>outdoor play areas</b> by <b>alternative noise attenuation measures</b> where it is not practical to provide a noise barrier or earth mound.</p>		
<p><b>PO45</b> Development involving a <b>childcare centre</b> or <b>educational establishment</b>:</p> <ul style="list-style-type: none"> <li>1. provides a noise barrier or earth mound that is designed, sited and constructed:</li> <li>2. to achieve the maximum building facade acoustic level in reference table 1 (item 1.2);</li> <li>3. in accordance with: <ul style="list-style-type: none"> <li>a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic</li> </ul> </li> </ul>	<p>No acceptable outcome is provided.</p>	<p><b>N/A:</b> The subject site is not located adjacent to a SCR.</p>

Performance outcomes	Acceptable outcomes	Response
<p>Noise), Department of Transport and Main Roads, 2013;</p> <p>b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019;</p> <p>c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020; or</p> <p>4. achieves the maximum building facade acoustic level in reference table 1 (item 1.2) by <b>alternative noise attenuation measures</b> where it is not practical to provide a noise barrier or earth mound.</p>		
<p><b>PO46</b> Development involving:</p> <p>1. <b>indoor education areas</b> and <b>indoor play areas</b>; or</p> <p>2. sleeping rooms in a <b>childcare centre</b>; or</p> <p>3. <b>patient care areas</b> in a <b>hospital</b> achieves the maximum internal acoustic level in reference table 3 (items 3.2-3.4).</p>	No acceptable outcome is provided.	<b>N/A:</b> The subject site is not located adjacent to a SCR.
<b>Above ground floor level requirements (childcare centre, educational establishment, hospital) adjacent to a state-controlled road or type 1 multi-modal corridor</b>		
<p><b>PO47</b> Development involving a <b>childcare centre</b> or <b>educational establishment</b> which have balconies, podiums or elevated <b>outdoor play areas</b> predicted to exceed the maximum free field acoustic level in reference table 2 (item 2.3) due to noise from a <b>state-controlled road</b> are provided with:</p> <p>1. a continuous <b>solid gap-free structure</b> or balustrade (excluding gaps required for drainage purposes to comply with the Building Code of Australia);</p>	No acceptable outcome is provided.	<b>N/A:</b> The subject site is not located adjacent to a SCR.

Performance outcomes	Acceptable outcomes	Response
2. highly acoustically absorbent material treatment for the total area of the soffit above balconies or elevated <b>outdoor play areas</b> .		
<b>PO48</b> Development including: <ol style="list-style-type: none"> <li>1. <b>indoor education areas</b> and <b>indoor play areas</b> in a <b>childcare centre</b> or <b>educational establishment</b>; or</li> <li>2. sleeping rooms in a <b>childcare centre</b>; or</li> <li>3. <b>patient care areas</b> in a <b>hospital</b> located above ground level, is designed and constructed to achieve the maximum internal acoustic level in reference table 3 (items 3.2-3.4).</li> </ol>	No acceptable outcome is provided.	<b>N/A:</b> The subject site is not located adjacent to a SCR.
<b>Air, light and vibration</b>		
<b>PO49</b> Private open space, outdoor education areas and outdoor play areas are protected from air quality impacts from a <b>state-controlled road</b> .	<b>AO49.1</b> Each dwelling or unit has access to a <b>private open space</b> which is shielded from a <b>state-controlled road</b> by a building, <b>solid gap-free fence</b> , or other <b>solid gap-free structure</b> .  OR  <b>AO49.2</b> Each <b>outdoor education area</b> and <b>outdoor play area</b> is shielded from a <b>state-controlled road</b> by a building, <b>solid gap-free fence</b> , or other <b>solid gap-free structure</b> .	<b>N/A:</b> The subject site is not located adjacent to a SCR.

Performance outcomes	Acceptable outcomes	Response
<p><b>PO50 Patient care areas</b> within <b>hospitals</b> are protected from vibration impacts from a <b>state-controlled road</b> or <b>type 1 multi-modal corridor</b>.</p>	<p><b>AO50.1 Hospitals</b> are designed and constructed to ensure vibration in the patient treatment area does not exceed a vibration dose value of 0.1m/s<sup>1.75</sup>.</p> <p>AND</p> <p><b>AO50.2 Hospitals</b> are designed and constructed to ensure vibration in the ward of a <b>patient care area</b> does not exceed a vibration dose value of 0.4m/s<sup>1.75</sup>.</p>	<p><b>N/A:</b> The subject site is not located adjacent to a SCR.</p>
<p><b>PO51</b> Development is designed and sited to ensure light from infrastructure within, and from users of, a <b>state-controlled road</b> or <b>type 1 multi-modal corridor</b>, does not:</p> <ol style="list-style-type: none"> <li>1. intrude into buildings during night hours (10pm to 6am);</li> <li>2. create unreasonable disturbance during evening hours (6pm to 10pm).</li> </ol>	<p>No acceptable outcomes are prescribed.</p>	<p><b>N/A:</b> The subject site is not located adjacent to a SCR.</p>

**Table 1.6: Development in a future state-controlled road environment**

Performance outcomes	Acceptable outcomes	Response
<p><b>PO52</b> Development does not impede delivery of a <b>future state-controlled road</b>.</p>	<p><b>AO52.1</b> Development is not located in a <b>future state-controlled road</b>.</p> <p>OR ALL OF THE FOLLOWING APPLY:</p> <p><b>AO52.2</b> Development does not involve filling and excavation of, or material changes to, a <b>future state-controlled road</b>.</p>	<p><b>N/A:</b> The proposed development is not in proximity to a future state-controlled road.</p>

Performance outcomes	Acceptable outcomes	Response
	<p>AND</p> <p><b>AO52.3</b> The intensification of lots does not occur within a <b>future state-controlled road</b>.</p> <p>AND</p> <p><b>AO52.4</b> Development does not result in the landlocking of parcels once a <b>future state-controlled road</b> is delivered.</p>	
<p><b>PO53</b> The location and design of <b>new or changed access</b> does not create a safety hazard for users of a <b>future state-controlled road</b>.</p>	<p><b>AO53.1</b> Development does not include <b>new or changed access</b> to a <b>future state-controlled road</b>.</p>	<p><b>N/A:</b> The proposed development is not in proximity to a future state-controlled road.</p>
<p><b>PO54</b> Filling, excavation, building foundations and <b>retaining structures</b> do not undermine, damage or cause subsidence of a <b>future state-controlled road</b>.</p>	<p>No acceptable outcome is prescribed.</p>	<p><b>N/A:</b> The proposed development is not in proximity to a future state-controlled road.</p>
<p><b>PO55</b> Development does not result in a material worsening of stormwater, flooding, overland flow or drainage impacts in a <b>future state-controlled road</b> or <b>road transport infrastructure</b>.</p>	<p>No acceptable outcome is prescribed.</p>	<p><b>N/A:</b> The proposed development is not in proximity to a future state-controlled road.</p>
<p><b>PO56</b> Development ensures that stormwater is lawfully discharged.</p>	<p><b>AO56.1</b> Development does not create any new points of discharge to a <b>future state-controlled road</b>.</p> <p>AND</p> <p><b>AO56.2</b> Development does not concentrate flows to a <b>future state-controlled road</b>.</p> <p>AND</p>	<p><b>N/A:</b> The proposed development is not in proximity to a future state-controlled road.</p> <p><b>N/A:</b> The proposed development is not in proximity to a future state-controlled road.</p>

Performance outcomes	Acceptable outcomes	Response
	<p><b>AO56.3</b> Stormwater run-off is discharged to a <b>lawful point of discharge</b>.</p> <p>AND</p> <p><b>AO56.4</b> Development does not worsen the condition of an existing <b>lawful point of discharge</b> to the <b>future state-controlled road</b>.</p>	<p><b>N/A:</b> The proposed development is not in proximity to a future state-controlled road.</p> <p><b>N/A:</b> The proposed development is not in proximity to a future state-controlled road.</p>