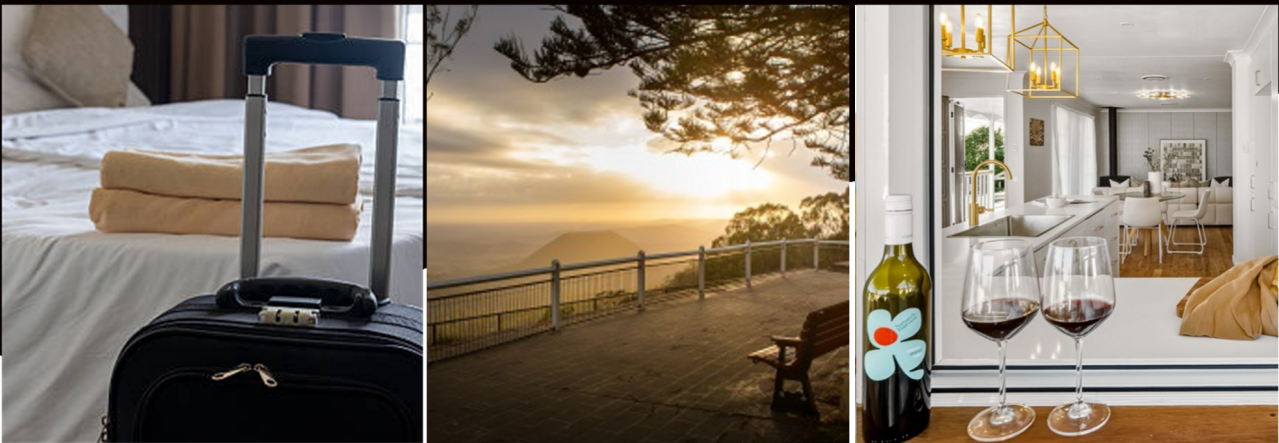


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URBAN PLANNING



## **DEVELOPMENT APPLICATION**

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Development Permit – Material Change of Use

# Short Term Accommodation

**22 Rowbotham Street, Rangeville**

PLANNING REPORT

**JUNE 2026**

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Prepared For: WAK Cattle Co  
Project Name: Short Term Accommodation  
Project Reference: 2026-262  
Date: 15 June 2026

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### Document History

Version	Date	Description of Revision	Prepared By	Reviewed By
Draft	11/06/2026	Draft for Client Review	SF	KR
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## EXECUTIVE SUMMARY

### SITE DETAILS

<b>Address of Site</b>	22 Rowbotham Street, Rangeville
<b>Real Property Description</b>	Lot 10 RP108184
<b>Area of Site</b>	539m <sup>2</sup>
<b>Road Frontages</b>	Primary: South Street Secondary: Rowbotham Street
<b>Easements</b>	N/A
<b>Registered Owners</b>	William Thomas Twidale and Kirsty Lee Twidale

### PLANNING INSTRUMENTS

<b>Regional Plan</b>	South East Queensland and Darling Downs Regional Plans
<b>Regional Plan Designation</b>	Urban Footprint
<b>Planning Scheme</b>	<i>Toowoomba Regional Planning Scheme 2012</i>
<b>Zone</b>	Low Density Residential Zone
<b>Zone Precinct</b>	General
<b>Overlays</b>	Airport Environs Overlay
<b>Regulated Vegetation</b>	N/A
<b>Existing Use</b>	Dwelling House
<b>Current Approvals</b>	Nil (as available on Council's Development.i)

### APPLICATION OVERVIEW

<b>Application</b>	Development Application
<b>Approval Sought</b>	Development Permit
<b>Development Type</b>	Material Change of Use
<b>Land Use Definition</b>	Short Term Accommodation

<b>Level of Assessment</b>	Impact Assessable
<b>Relevant Referral/Technical Agencies</b>	N/A

### PROPOSED DEVELOPMENT

<b>Development Summary</b>	Short Term Accommodation
<b>Gross Floor Area</b>	Approximately 205m <sup>2</sup>
<b>Total Building Area</b>	Approximately 350m <sup>2</sup>
<b>Carparking</b>	Two (2) covered parking spaces
<b>Operational Parameters</b>	Maximum Guests: Eight (8)

### APPLICANT DETAILS

<b>Applicant</b>	WAK Cattle Co c/- Precinct Urban Planning PO Box 3038 <b>TOOWOOMBA QLD 4350</b>
<b>Contact Person</b>	<b>Kim Reeve</b> <b>Phone:</b> 07 4632 2535 <b>Mobile:</b> 0427 875 871 <b>Email:</b> <a href="mailto:kim@precinctplan.com.au">kim@precinctplan.com.au</a>
<b>Our Reference</b>	2026-262

## 1.0 INTRODUCTION

This report has been prepared in support of a Development Application for a Development Permit for Material Change of Use for Short Term Accommodation on land at 22 Rowbotham Street, Rangeville, described as Lot 10 RP108184 (**the site**). The proposed development involves the start of a new use of the premises and accordingly comprises a Material Change of Use under Schedule 2 of the *Planning Act 2016* (**the Act**).

This town planning report addresses the merits of the development having regard to the provisions of the *Planning Act 2016* and the *Toowoomba Regional Planning Scheme 2012*. This planning report has been prepared on behalf of the applicant, WAK Cattle Co.

This Development Application for a Development Permit for Material Change of Use is Impact Assessable development under the *Toowoomba Regional Planning Scheme 2012*. The following report demonstrates the suitability of the proposal within the locality and its compliance with the relevant provisions of the *Toowoomba Regional Planning Scheme 2012*.



Improvements on the site include a Dwelling House and ancillary structures. The features of the site and surrounding land uses are illustrated in **Figure 3**.

**FIGURE 3 - AERIAL IMAGERY**



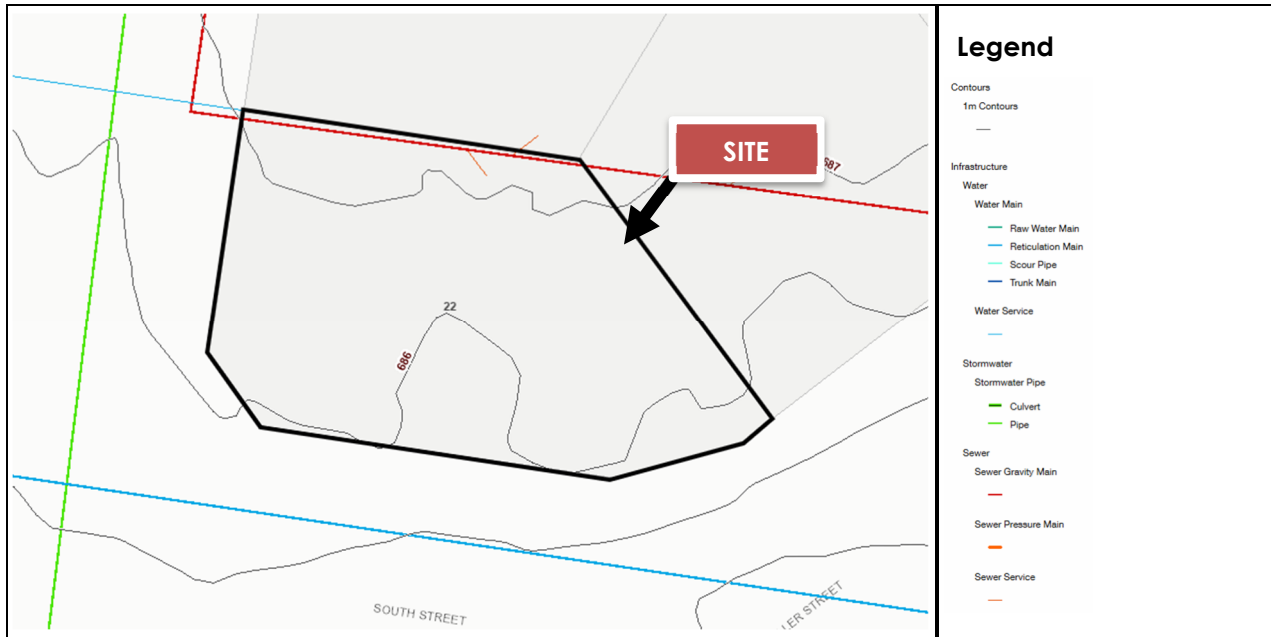
## 2.2 SITE CHARACTERISTICS

The site is characterised by the following features:

- The site has primary frontage to South Street and secondary frontage to Rowbotham Street.
  - South Street is a two-lane sealed carriageway with concrete kerb and channel on either side of the roadway. South Street is identified as a Local Road on Council's Road Hierarchy.
  - Rowbotham Street is a two-lane sealed carriageway with concrete kerb and channel on either side of the roadway. Rowbotham Street is identified as a Distributor Road on Council's Road Hierarchy.
- Lands in the locality drain in a westerly direction towards East Creek. East Creek is located approximately 1.3km west of the site and comprises the area's primary drainage feature.
- The site experiences a gradual decline in land elevation between the northern and southern boundaries of the site; refer to **Figure 4**. The highest point of the site is located in the north-west corner at approximately 687.21m AHD, with the lowest point of the site located adjacent to the southern boundary at approximately 685.60m AHD. This translates to a fall in natural ground level of approximately 1.61m across the site.
- The site is predominately clear of mature vegetation and does not contain mapped areas of remnant vegetation or ecological significance.

- The site is not burdened by, nor does it benefit from any existing easements.
- The site is serviced by Council's reticulated water supply, sewerage and stormwater drainage infrastructure; refer to **Figure 4**. The site is also serviced by reticulated electricity and telecommunications supply networks.

**FIGURE 4 - CONTOUR AND INFRASTRUCTURE MAPPING**



**Figure 4** confirms that the site experiences a decline in land elevation between the northern and southern boundaries of the site. The figure also illustrates the locations of Council's reticulated water, sewerage and stormwater networks in relation to the site.

The features of the site are illustrated in **Photographs 1-2**.



**PHOTOGRAPH 1** - View of the site from South Street.



PHOTOGRAPH 2 - View of the site from Rowbotham Street.

## 2.3 SURROUNDING LAND USE

### 2.3.1 SURROUNDING LOCALITY

The site is situated in a locality that is characterised by residential land uses. The scale and character of the built form in the locality is predominately of a residential character.

### 2.3.2 ADJOINING PROPERTIES

The nature and character of land use on surrounding properties may be summarised as follows:

- Land to the **north** of the site, located at 20 Rowbotham Street, comprises a residential premises containing a Dwelling House and ancillary structures.



PHOTOGRAPH 3 - View of the residential premises to the north of the site from Rowbotham Street.

- Land to the **east** of the site, located at 23 Heller Street, comprises a residential premises containing a Dwelling House and ancillary structures.



PHOTOGRAPH 4 - View of the residential premises to the east of the site from Heller Street.

- Land to the **south** of the site, located at 4D and 6 South Street, comprises residential premises containing Dwelling Houses and ancillary structures.



PHOTOGRAPH 5 - View of the residential premises to the south of the site from South Street.

- Land to the **west** of the site, located on the opposite side of Rowbotham Street, comprises a residential premises containing a Dwelling House and ancillary structures.



**PHOTOGRAPH 6** - View of the residential premises to the west of the site from Rowbotham Street.

## 2.4 CURRENT DEVELOPMENT APPROVALS

The site does not currently benefit for any existing Development Permits as publicly available on Council's website.

### 3.0 PROPOSAL

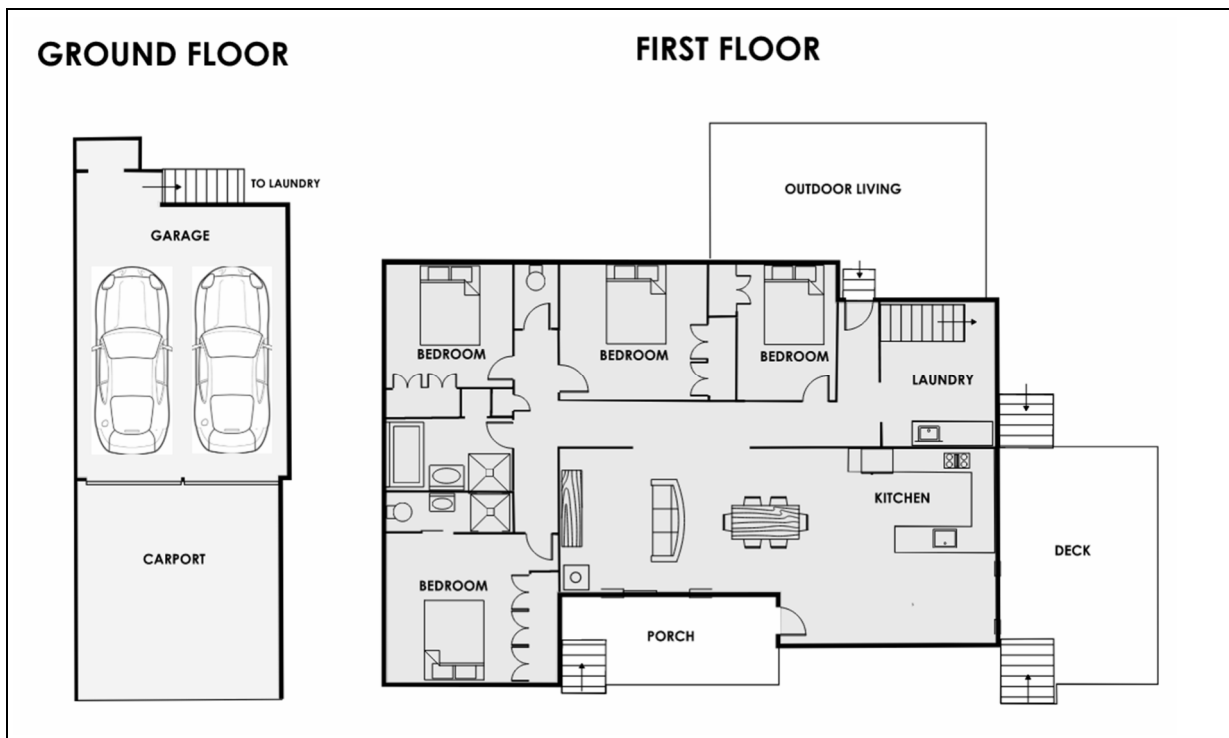
This Development Application seeks approval for a Development Permit for Material Change of Use for Short Term Accommodation on the site. The proposal involves the reuse of an existing dwelling for the purposes of providing short-term accommodation to visitors and tourists of the surrounding region.

The proposed development involves the reuse of an existing residential building, which has a gross floor area (GFA) of approximately 205m<sup>2</sup> and a total building area of approximately 350m<sup>2</sup>.

The existing building is a split-level dwelling that comprises four (4) bedrooms, two (2) bathrooms, an open plan kitchen, dining and living area and a laundry. The dwelling also benefits from a large deck to the east, adjacent to the living area and outdoor living space at the rear of the dwelling. There are no changes proposed to the internal configuration of the dwelling.

The internal configuration of the proposed development will be generally in accordance with the Ground and First Floor Plans, prepared by Precinct Urban Planning, attached at **Appendix C** and reproduced in **Figure 5**.

**FIGURE 5 - EXTRACT OF EXISTING FLOOR PLANS**



The proposed development will not involve any changes to the external appearance of the building and accordingly, will ensure the character of the area is retained. The existing building consists of building treatments including external painted brick façade and a tiled roof.

The proposed development will utilise existing onsite carparking arrangements within the enclosed garage and carport, with access provided via the existing vehicle crossover to South Street. The nature of the development is such that the existing residential arrangements are suitable to cater for the demands generated by the change of use. The premises will also be serviced by a cleaning/laundry service between bookings and will involve a single passenger vehicle only. The existing on-site carparking arrangements will sufficiently accommodate the single service vehicle.

The proposed development will utilise the existing kerbside bin collection arrangements. The dwelling benefits from one (1) general and one (1) recyclable wheelie bin. The bins will be relocated to the kerb by the property manager for collection by Council's wheelie bin collection service in accordance with existing arrangements to prevent odour issues and vermin harbouring. The bins will be stored in an accessible location out of public view.

The premises is intended to be rented on a short-term basis for periods of limited duration via internet short stay accommodation providers such as "Airbnb" or similar, aimed toward families visiting hospital patients or children in boarding school. The premises will be rented to one (1) group at any time and accommodate up to a maximum of eight (8) guests and two (2) vehicles. A single booking will be a minimum of two (2) nights and maximum of two (2) weeks. Strict booking policies will be in place to ensure residential amenity is maintained, including no pets and no parties. A Noise Complaints Management Procedure has been developed and will be implemented to ensure that potential impact on neighbouring properties can be suitably managed; refer to **Appendix D**.

The development will utilise existing connections to Council's reticulated water supply, sewerage and stormwater drainage infrastructure and electricity and telecommunications supply networks. No changes are required to infrastructure servicing arrangements.

## 4.0 PLANNING FRAMEWORK

### 4.1 THE PLANNING ACT 2016

The purpose of the *Planning Act 2016* (PA) is to establish an efficient, effective, transparent, integrated, coordinated, and accountable system of land use planning (planning), development assessment and related matters that facilitates the achievement of ecological sustainability.

The proposed development involves the start of a new use of the premises and accordingly comprises a Material Change of Use under Schedule 2 of the *Planning Act 2016* (**the Act**). A Development Permit for Material Change of Use must be obtained prior to the commencement of the use of premises. Assessment against the *Toowoomba Regional Planning Scheme 2012* indicates that this development application is Impact Assessable in accordance with Section 45(5) of the Act.

### 4.2 STATE PLANNING POLICY

Pursuant to the provisions of section 45(5) of the *Planning Act 2016*, the Assessment Manager, in considering an Impact Assessable Development Application, must assess the application in respect of the State Planning Policy.

The *State Planning Policy July 2017 (SPP)* commenced on 3 July 2017 and replaces *State Planning Policy April 2016*. The SPP expresses the state's interests in land use planning and development and promotes these interests through plan making and development decisions of state and local government. The SPP applies, to the extent relevant, when:

- making or amending a local planning instrument;
- designating premises for infrastructure;
- making or amending a regional plan;
- development assessment by local government, if its planning scheme has not yet appropriately integrated the relevant SPP state interests policies; and
- an assessment manager or referral agency other than local government is assessing a development application.

The SPP addresses seventeen (17) state interests categorised under the following themes:

- (i) Liveable communities and housing.
- (ii) Economic growth.
- (iii) Environment and heritage.
- (iv) Safety and Resilience to Hazards.
- (v) Infrastructure.

A state interest is defined under Schedule 2 of the *Planning Act 2016* (the Act):

- (a) an interest that the Minister considers affects an economic or environmental interest of the State or a part of the State; or
- (b) an interest that the Minister considers affects the interest of ensuring this Act's purpose is achieved.

The proposed development has been assessed against the state interests included under *Part E: State Interest Policies and Assessment Benchmarks* of the SPP as follows:

#### 4.2.1 STATE INTEREST POLICIES AND ASSESSMENT BENCHMARKS

Part E of the SPP contains state interest policies and where relevant, the assessment benchmarks for each state interest. The assessment benchmarks for each relevant state interest are listed below. For each of these state interests, Part E of the SPP advises when the assessment benchmarks apply and if so, what matters the application must be assessed against, to the extent the SPP has not been identified in a local planning instrument as being appropriately integrated.

Part 2 of the *Toowoomba Regional Planning Scheme 2012* identifies the State interests under the State Planning Policy that have been integrated into the Planning Scheme. In this instance, the *Toowoomba Regional Planning Scheme 2012* confirms that some of the State interests relevant to the Toowoomba Regional Council region have been appropriately incorporated into the Planning Scheme.

The Planning Scheme incorporates the following State interests:

- Safety and Resilience to Hazards (as it relates to flood hazards).

Accordingly, where the proposed development achieves compliance with the assessment benchmarks of the Planning Scheme, the development is considered to comply with the provisions of the State Planning Policy as it relates to the above State interests. The development is therefore also required to be assessed against the assessment benchmarks under the State Planning Policy for the relevant State interests not integrated into the Planning Scheme. An assessment of each of the relevant State interests is provided below.

##### 1. Liveable Communities

These provisions apply to a development application in an urban area involving premises that is, or will be, accessed by common private title, for a material change of use or reconfiguring a lot where the application involves buildings (attached or detached) that are not covered by other legislation or planning provisions mandating fire hydrants.

Where these circumstances apply, the development:

- (i) ensures fire hydrants are installed and located to enable fire services to access water safely, effectively and efficiently;
- (ii) road widths, and construction within the development, are adequate for fire emergency vehicles to gain access to a safe working area close to buildings and near water supplies whether or not on-street parking spaces are occupied; and
- (iii) fire hydrants are suitably identified so that fire services can locate them at all hours.

In this instance, the proposed development does not involve any of the above circumstances, and accordingly, these provisions are not relevant to the assessment of this application.

##### 2. Mining and Extractive Resources

These provisions relate to the following developments:

- (1) Reconfiguring a Lot within a Key Resource Area (**KRA**);
- (2) A Material Change of Use within the resource/processing area of a KRA or the separation area for the resource/processing area of a KRA; or
- (3) A Material Change of Use within the transport route separation area of a KRA that will result in an increase in the number of people working or residing in the transport route separation area.

In this instance, the proposed development does not involve any of the above circumstances, and accordingly, these provisions are not relevant to the assessment of this application.

### 3. Water Quality

These provisions relate to the following matters of state interest:

- (1) Receiving waters - including development applications for:
  - (a) a material change of use for urban purposes that involves a land area of 2,500m<sup>2</sup> or greater that will result in:
    - (i) an impervious area greater than 25% of the net developable area; or
    - (ii) six or more dwellings; or
  - (b) reconfiguring a lot for urban purposes that involves a land area 2,500m<sup>2</sup> or greater and will result in six or more lots; or
  - (c) operational works for urban purposes that disturb 2,500m<sup>2</sup> or greater of land area.
- (2) For water supply buffer areas – a development application located wholly outside an urban area and relating to premises that is within, or partly within, a water supply buffer area, that involves: (i) material change of use activities listed in Part E of the SPP, or (ii) reconfiguring a lot to create five or more additional lots where one or more lots are less than 16ha and any of the created lots rely on on-site wastewater treatment.

In this instance the proposed development involves a Material Change of Use on land with an area less than 2,500m<sup>2</sup> and does not involve changes to existing impervious areas or site cover. Accordingly, the assessment provisions for this State interest are not relevant to the assessment of this application

### 4. Natural Hazards, Risk and Resilience

These provisions relate to development applications for a material change of use, reconfiguring a lot or operational work on land within:

- (i) a flood hazard area;
- (ii) a bushfire hazard area;
- (iii) a landslide hazard area,
- (iv) storm tide inundation areas; and
- (v) erosion prone area.

In this instance, the site is identified as being impacted by natural hazards. In particular, the site is impacted by the Flood Hazard Area – Local Government Flood Mapping Area under the State Planning Policy mapping.

Part 2 of the *Toowoomba Regional Planning Scheme 2012* confirms that matters associated with this State Interest, as it relates to flood hazards, are appropriately integrated within the Planning Scheme. Under the provisions of the Planning Scheme, the subject site is not impacted by the Flood Hazard Overlay. Accordingly, the assessment provisions for this State interest are not relevant to the assessment of this application.

### 5. Strategic Airports and Aviation Facilities

These provisions relate to development applications that involve land located within a local government area that contains/impacted by a strategic airport identified in the SPP Part E, Table 2, or an aviation facility identified in Appendix 2 of the *strategic airports and aviation facilities guidance material* and involve:

- (i) a material change of use of premises that will result in work encroaching into the operational airspace of a strategic airport, or on land within the light restriction zone, lighting area buffer

- or the wildlife hazard buffer zone of a strategic airport, or results in work encroaching into the building restricted area of an aviation facility; or
- (ii) a material change of use or reconfiguring a lot where any part of the land is within the 20ANEF contour or greater, or the public safety area of a strategic airport; or
  - (iii) building work not associated with a material change of use that will result in work intruding into the operational airspace of a strategic airport, or where any part of the premises is within the light restriction zone of lighting area buffer of a strategic airport, or results in work encroaching into the building restricted area of an aviation facility; or
  - (iv) operational work not associated with a material change of use where any work or associated activity will intrude into the operational airspace of a strategic airport, or where any part of the premises is within the light restriction zone of lighting area buffer of a strategic airport, or results in work encroaching into the building restricted area of an aviation facility.

Where these circumstances apply, the development application is assessed against the strategic airports and aviation facilities assessment benchmarks listed under the SPP.

In this instance, the site is not impacted by a Strategic Airport. Accordingly, the assessment provisions for this State interest are not relevant to the assessment of this application.

The proposal has been assessed to comply with all applicable matters of state interest included in the State Planning Policy.

### 4.3 STATE REFERRAL AGENCIES

Section 54 of the *Planning Act 2016* and Section 22 and Schedule 10 of the *Planning Regulation 2017 (the Regulation)* are of relevance for the purposes of determining applicable referral agencies and their jurisdiction in terms of being advice or concurrence agencies as well as the relevant assessment benchmarks that are to be addressed.

The State Assessment and Referral Agency (**SARA**), established under the auspices of the Department of State Development, Infrastructure and Planning (**DSDIP**) is the assessment manager or referral agency for development applications where the State has a jurisdiction pursuant to Schedule 8 or 10 of the Regulation. Schedule 10 regulates when applications are referable. In accordance with Schedule 8, DSDIP will not be the assessment manager for this development application.

**TABLE 1 - REFERRAL TRIGGER ASSESSMENT**

Part	Application Involving	Applicable	Comment
Part 1	Airport land	No	The proposed development is not located on airport land.
Part 2A	Caboolture West Interim Structure Plan	No	The site is not located within the Caboolture West investigation or growth areas.
Part 3	Clearing native vegetation	No	The proposed development does not involve clearing native vegetation or give rise to accepted clearing.
Part 4	Contaminated land	No	The site is not identified as contaminated land.
Part 5	Environmentally relevant activities	No	An approval for an ERA is not sought.
Part 6	Fisheries	No	The proposed development is not associated with: <ul style="list-style-type: none"> <li>• aquaculture; or</li> </ul>

Part	Application Involving	Applicable	Comment
			<ul style="list-style-type: none"> <li>the removal, destruction or damage of marine plants; or</li> <li>the constructing or raising of waterway barrier works; and</li> <li>is not within a declared fish habitat area.</li> </ul>
Part 7	Hazardous chemical facilities	No	The proposed development is not for a hazardous chemical facility.
Part 8	Heritage places	No	The proposed development is not associated with a locally listed place. In addition, the site is not associated with, nor does it adjoin a property on the Queensland heritage register.
Part 9	Infrastructure-related	No	<p>The following relates to infrastructure-related referrals:</p> <ul style="list-style-type: none"> <li>The proposed development does not exceed the thresholds identified at Schedule 20.</li> <li>The site is not associated with a designated premises.</li> <li>The site is not located within 25m of a State transport corridor and does not involve new or changed access to the State transport corridor.</li> <li>The site is not located adjacent to a local road that intersects with a State controlled road and is not within 100m of a State-controlled intersection.</li> <li>The site is not identified as a future State-controlled road.</li> <li>The site is not located within 100m of an electricity substation.</li> <li>The site is not associated with an easement for oil, gas or electricity infrastructure.</li> <li>The site is not in proximity to a State controlled transport tunnel (both existing and/or future).</li> </ul>
Part 10	Koala habitat area	No	The site is not mapped as containing koala habitat areas.
Part 11	Noise attenuation land	No	The site is not identified as being noise attenuation land and the site is not within proximity of an off-road motorcycle facility. The proposed development is not associated with prohibited development.
Part 12	Operational works for reconfiguring a lot	No	The proposed development does not involve Operational Works.
Part 13	Ports	No	The site is not associated with port land.
Part 14	Reconfiguring a lot under Land Title Act	No	The proposed development does not involve Reconfiguring a Lot under the Land Title Act.
Part 15	SEQ Development area	No	The site is not located within an SEQ development area.
Part 16	SEQ regional landscape and rural production area and SEQ rural living area	No	The site is not located within the SEQ regional landscape and rural production area or the SEQ rural living area.

Part	Application Involving	Applicable	Comment
Part 16AA	Solar Farms	No	The proposed development does not involve a solar farm.
Part 16A	Southport Spit	No	The site is not located in the Southport Spit.
Part 16B	SEQ Northern Inter-Urban Break	No	The site is not located within the identified SEQ Northern Inter-Urban Break area.
Part 17	Tidal works or work in a coastal management district	No	The proposed development does not involve tidal works or works within a coastal management district.
Part 18	Urban design	No	The proposed development does not involve an increase in GFA that is greater than 25,000m <sup>2</sup> .
Part 19	Water-related development	No	The proposed development does not involve: <ul style="list-style-type: none"> <li>the taking or interfering of water; or</li> <li>removing quarry material from a watercourse or lake; or</li> <li>relates to a dam; or</li> <li>the construction of a levee.</li> </ul>
Part 20	Wetland protection area	No	The site is not identified as being within a wetland protection area.
Part 21	Wind farms	No	The proposed development does not involve a wind farm

The above assessment of the referral triggers under the *Planning Regulation 2017* confirms that the proposed development will not trigger referral in this instance.

#### 4.4 STATE DEVELOPMENT ASSESSMENT PROVISIONS

The State Development Assessment Provisions (**SDAP**) provide assessment benchmarks for the assessment of development applications where the chief executive administering the *Planning Act 2016 (the Act)* is the assessment manager or a referral agency. The chief executive through the State Assessment and Referral Agency (**SARA**) uses the SDAP to deliver a coordinated, whole-of-government approach to the state's assessment of development applications. The SDAP applies throughout the State and contains the matters of State interest the chief executive may have regard to when assessing/deciding a development application as either the assessment manager or referral agency.

The SDAP contains criteria for assessment within State Codes to clarify when the State is to be involved in the assessment of an application and the specific matters of State interest that apply. The SDAP is a specific assessment benchmark that a development must be assessed against as prescribed under the Regulation. In this instance, the proposed development does not trigger assessment against State Codes.

#### 4.5 REGIONAL PLANS

The site is located within the Toowoomba Urban Extent and therefore the provisions of the South East Queensland and Darling Downs Regional Plan apply in accordance with the provisions of the *Planning Regulation 2017*. An assessment of the development against the regional plan provisions is provided below.

#### 4.5.1 SOUTH-EAST QUEENSLAND REGIONAL PLAN

The *South East Queensland Regional Plan 2023 (ShapingSEQ 2023)* provides a regional framework for growth management, which sets the long-term planning direction for sustainable growth, a globally competitive economy, and high-quality living. *ShapingSEQ 2023* informs local planning across the region's twelve (12) local government areas and guides future infrastructure planning and investment to ensure regional growth is well-supported and contributes to a prosperous and liveable region.

*ShapingSEQ 2023* seeks to set a clear direction for the region through a series of Regional Priorities. These priorities are implemented through identified outcomes and strategies and sub-regional directions. These Regional Priorities are:

- A sustainable growth pattern;
- More homes, faster – supply, diversity and affordability;
- More social and affordable homes;
- Well-designed communities;
- Integrating land use and infrastructure;
- New economic opportunities and jobs;
- A sufficient pipeline of industrial land;
- Incorporating Aboriginal and Torres Strait Islander knowledge, culture, traditions and aspirations;
- Regional approach to natural hazards; and
- Better biodiversity outcomes for growth areas.

*ShapingSEQ* defines the desired future settlement pattern that aims to use land more efficiently, while protecting the values important to the community. The regional growth pattern designates land in SEQ in one of three regional land use classifications:

- Urban Footprint;
- Rural Living Area; or
- Regional Landscape and Rural Production Area.

The SEQ regulatory provisions ensure that land use planning and development throughout the region support *ShapingSEQ 2023's* desired regional growth pattern, goals, elements and strategies.

Under the settlement pattern identified in *ShapingSEQ 2023*, the site has been included within the **Urban Footprint**. The Urban Footprint identifies land sufficient to provide for the region's urban development needs over the life of the plan. The area includes land intended to accommodate the full range of acceptable urban uses, such as housing, industry, business, infrastructure, community facilities and urban open space.

The proposed development involves an urban use in an area suitable for such purposes. Accordingly, the application complies with the provisions of *ShapingSEQ 2023*.

#### 4.5.2 DARLING DOWNS REGIONAL PLAN

The Darling Downs Regional Plan was adopted on 14 October 2013 and covers the local authority areas of Balonne Shire Council and the Regional Councils of Goondiwindi, Maranoa, Southern Downs, Toowoomba and Western Downs.

The policies contained in the regional plan contribute towards the protection of strategic areas of priority agricultural land use from potentially incompatible resource activities and seek to maximise opportunities for co-existence of resources and agricultural land use.

The regional plan also safeguards areas required for the growth of towns in the regions through the establishment of Priority Living Areas while providing for resource activities to locate within these areas where it meets communities' expectations as determined by the relevant local government.

The site is located within the City of Toowoomba which is identified as Restricted Area (RA) 384 under Appendix 2 of the Regional Plan. This designation reflects the boundaries of the SEQ Regional Plan referred to under section 4.5.1 above. The development satisfies the requirements of the SEQ Regional Plan and accordingly will not compromise the outcomes sought under the Darling Downs Regional Plan.

## 4.6 TOOWOOMBA REGIONAL PLANNING SCHEME 2012

### 4.6.1 INTRODUCTION

Pursuant to the provisions of section 45(5) of the *Planning Act 2016*, an Impact Assessable Development Application must be assessed against the applicable assessment benchmarks of the applicable categorising instrument. The applicable categorising instrument in this instance is the *Toowoomba Regional Planning Scheme 2012*. A summary of the assessment of the proposal against the provisions of this instrument is outlined below.

### 4.6.2 DEFINITIONS

Under the provisions of the *Toowoomba Regional Planning Scheme 2012*, the proposed use is defined as "Short Term Accommodation". The relevant use definition is as follows:

**Short-term accommodation—**

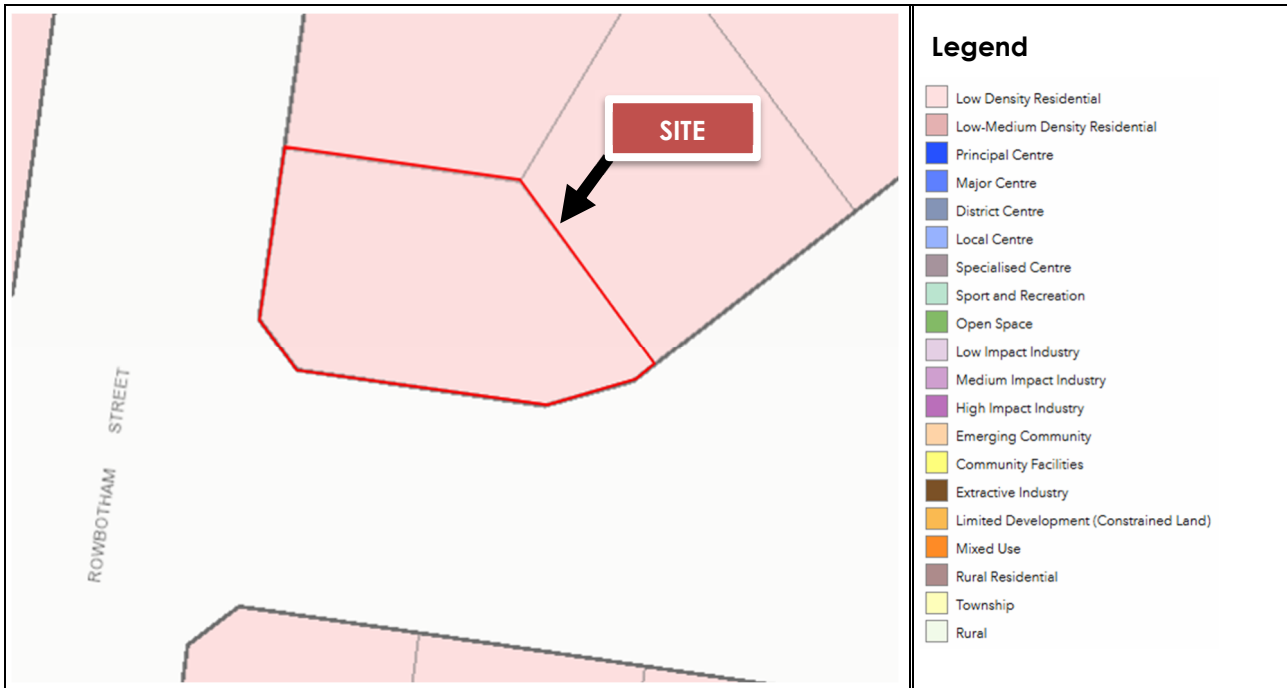
- (a) means the use of premises for
  - (i) providing accommodation of less than 3 consecutive months to tourists or travellers; or
  - (ii) a manager's residence, office, or recreation facilities for the exclusive use of guests, if the use is ancillary to the use in subparagraph (i); but
- (b) does not include a hotel, nature-based tourism, resort complex or tourist park.

### 4.6.3 ZONING

Under the *Toowoomba Regional Planning Scheme 2012*, the region is divided into twenty (20) land use zones. Certain zones are further divided into precincts for the purposes of conveying preferred land use intent or in order to assign assessment status to individual uses.

The site is predominately located within the **Low Density Residential Zone**. The zoning of the site and surrounding locality is illustrated in **Figure 6**.

**FIGURE 6 - ZONING MAPPING**



Under the assessment tables applicable to the Low Density Residential Zone at section 5.5 of the Planning Scheme, a Material Change of Use for a Short Term Accommodation is identified as **Impact Assessable** development.

#### 4.6.4 OVERLAY MAPPING

The Planning Scheme includes overlay maps that identify land characterised by particular features or subject to physical constraints that are likely to influence the use and development potential of affected areas. Overlay maps also identify those lands subject to assessment against specific area codes. In this instance, the subject land is impacted by the Airport Environs Overlay.

The applicability of this overlay is illustrated in **Figure 7**.

FIGURE 7 - AIRPORT ENVIRONS OVERLAY MAPPING

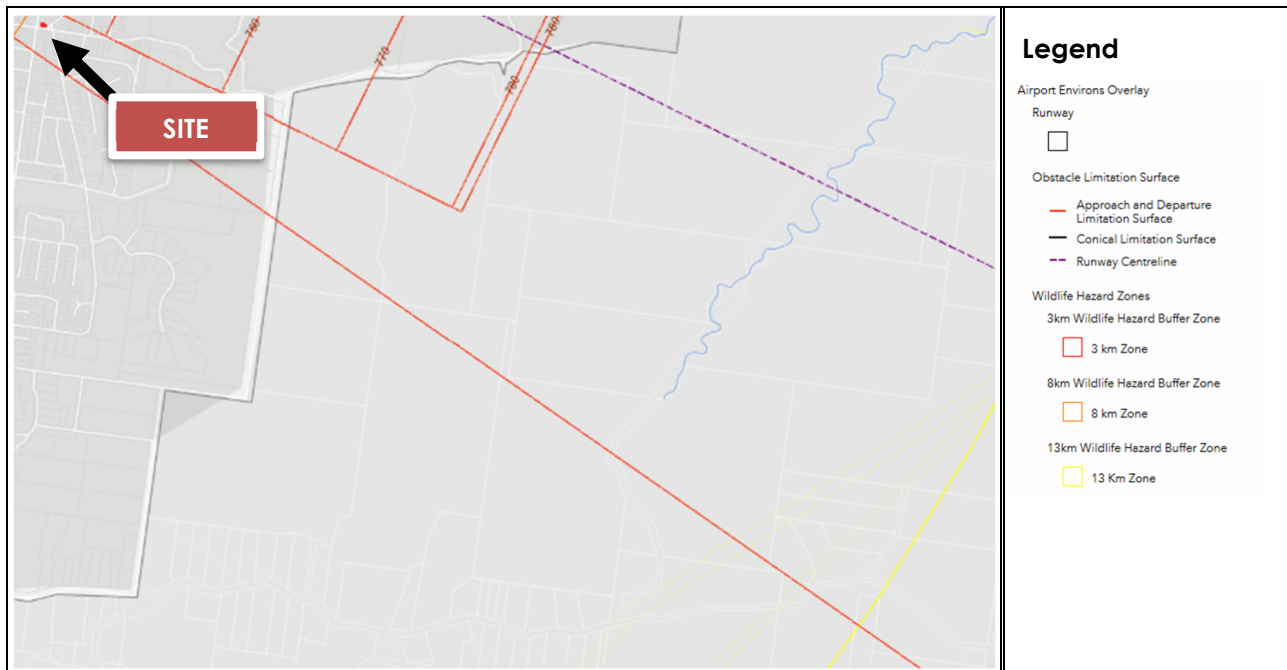


Figure 7 confirms that the site is impacted by the Airport Environs Overlay. In particular, the site is located within the 13km Wildlife Hazard Buffer Area of the Toowoomba Airport. Under the overlay assessment tables at section 5.10 of the *Toowoomba Regional Planning Scheme 2012*, the level of assessment for a Material Change of Use for a Short Term Accommodation does not change and triggers assessment against the Airport Environs Overlay Code. The development has been considered against this code at section 4.7.3.

## 4.7 ASSESSMENT BENCHMARKS

As identified in section 4.6, this Development Application is subject to Impact Assessment, and therefore requires assessment against the following provisions of the *Toowoomba Regional Planning Scheme 2012*.

- Strategic Framework; refer to section 4.7.1.
- Overall Outcomes – Low Density Residential Zone; refer to section 4.7.2.
- Applicable Codes; refer to section 4.7.3.

### 4.7.1 STRATEGIC FRAMEWORK

Part 3 of the *Toowoomba Regional Planning Scheme 2012* provides the Strategic Framework that sets the policy direction for the planning scheme and forms the basis for ensuring development occurs at appropriate locations within the planning scheme area. The structure of the Strategic Framework comprises:

- seven (7) themes supported by strategic outcomes;
- elements that further refine the strategic outcomes sought; and
- specific outcomes and land use strategies for each of the elements.

The development has been assessed against the applicable provisions of the Strategic Framework and a summary of this assessment is provided in the table attached at **Appendix A**. An assessment of the proposal against the provisions of the Strategic Framework indicates the proposal complies with the integrated policy direction sought for the planning scheme area and will not compromise the relevant strategic outcomes/elements sought for the individual themes.

#### 4.7.2 OVERALL OUTCOMES – LOW DENSITY RESIDENTIAL ZONE

Section 6.2.1.2 of the *Toowoomba Regional Planning Scheme 2012* outlines the general intent for development within the Low Density Residential Zone through the collective identification of a series of overall outcomes for the zone. The proposed development generally complies with the intent of the Low Density Residential Zone as detailed in **Table 2**.

**TABLE 2 - ASSESSMENT OF OVERALL OUTCOMES**

Overall Outcome	Response
(a) the provision of detached dwelling houses as the primary form of housing within the zone and dual occupancy as a secondary form of housing, on a range of lot sizes greater than 500m <sup>2</sup> ;	<b>Complies:</b> The proposed development does not involve new building work or additional dwellings. The proposed development will be leased to one group at a time, resulting in a consistent and compatible scale with other forms of residential accommodation in the locality.
(b) medium density forms of residential development are supported within greenfield areas in this zone where within walking distance of a local, district or major centre providing a wide range of goods, services and facilities, including provision of goods and services to meet daily convenience needs or where adjacent or opposite a district or regional park offering a high level of residential amenity.	<b>N/A:</b> The proposed development is for Short Term Accommodation and does not involve provision of medium density development.
(c) retirement villages are established in the Low Density Residential Zone only where they are designed to achieve a transition in scale and density from one that is consistent with the surrounding area to a more dense form consistent with a typical retirement village;	<b>N/A:</b> The proposed development does not involve a retirement village.
(d) dual occupancy development is dispersed throughout residential neighbourhoods and does not become the dominant housing form in the street;	<b>N/A:</b> The proposed development does not involve dual occupancies.
(e) lot reconfiguration within greenfield areas in the zone achieves a minimum dwelling yield of 15 dwellings per hectare;	<b>N/A:</b> The proposed development is not located in a greenfield area.
(f) new hatchet lots are not created within this zone and existing hatchet lots are occupied by a single dwelling house only;	<b>N/A:</b> The proposed development does not involve a hatchet lot.
(g) development provides for an efficient land-use pattern and is well connected to other parts of the local government area;	<b>Complies:</b> The proposed development is well-connected to the surrounding locality through the road network and access to public and active transport.

Overall Outcome	Response
(h) development is designed to provide safe and walkable neighbourhoods that connect residents to desirable destinations including schools, parks, shops and community facilities;	<b>Complies:</b> The proposed development is within walking distance to schools, parks, shops and community facilities.
(i) small scale short-term accommodation occurs only on land with direct access to a road at the distributor, sub-arterial and regional arterial level in the road hierarchy and in proximity to existing development of a similar nature, in buildings of a scale that is consistent with the surrounding residential area, all car parking needs can be met on site, and the use does not detract from the residential amenity of the area.	<b>Complies:</b> The proposed development involves reuse of an existing dwelling at a similar scale to the surrounding area with sufficient carparking spaces on-site. The proposed development has direct access to a distributor road. The proposed development involves management measures such as strict booking policies and the Noise Complaints Management Procedure to manage any impacts to the residential amenity in the area; refer to <b>Appendix D</b> .
(j) development maintains a high level of residential amenity avoiding uses that introduce adverse impacts such as excessive traffic, noise, dust, odour, lighting and other locally specific impacts;	<b>Complies:</b> The proposed development has been designed to maintain a high level of residential amenity by managing potential impacts through measures such as a Noise Complaints Management Procedure; refer to <b>Appendix D</b> . The proposed development is of a similar scale and nature to the surrounding locality and is unlikely to introduce adverse impacts.
(k) development reflects and enhances the existing low density scale and character of the area, and is no more than 2 storeys above ground level;	<b>Complies:</b> The proposed development involves re-use of an existing dwelling and does not involve new building work. The proposed development will be a similar scale and intensity to the surrounding area.
(l) development is designed to incorporate sustainable practices including maximising energy efficiency, water conservation and encourage sustainable transport use such as walking, cycling and public transport use;	<b>Complies:</b> The proposed development has been designed to incorporate sustainable principles where possible.
(m) development is supported by necessary community facilities, open space and recreational areas and appropriate infrastructure including utility installations to support the needs of the local community;	<b>Complies:</b> The proposed development is in proximity to community facilities, open space and recreational areas and maintains existing infrastructure connections that are sufficient for the nature of the use.
(n) non-residential uses occur only where such uses: (i) primarily function to directly support the day to day convenience needs of the immediate local residential community; (ii) provide a local community or limited business function, and include: a) Child care centre. b) Club. c) Community care centre. d) Community use. e) Educational establishment. f) Emergency services	<b>N/A:</b> The proposed development does not involve a non-residential use.

Overall Outcome	Response
<p>g) The following uses only where reuse of an existing building used for an existing business activity:</p> <ul style="list-style-type: none"> <li>i. Food and drink outlet</li> <li>ii. Shop (only convenience).</li> <li>iii. Health care services.</li> <li>iv. Place of worship.</li> <li>v. Sales office.</li> </ul> <p>vi. Veterinary services.</p>	
<p>(o) non-residential uses only occur within the zone where they:</p> <ul style="list-style-type: none"> <li>(i) are accessible to the immediate local residential community they serve;</li> <li>(ii) are located on land with direct access to a road at the distributor, sub - arterial and regional arterial level in the road hierarchy and minimises intrusion of through traffic into local residential streets;</li> <li>(iii) have a built form that is compatible with surrounding residential character and amenity, including: <ul style="list-style-type: none"> <li>a) reuse of existing non -residential premises or</li> <li>b) extensions and/or new buildings have height, bulk and scale consistent with intended low density residential built form intent of the zone and compatible with existing streetscape character;</li> </ul> </li> <li>(iv) do not have a significant detrimental impact on the neighbourhood amenity expected within a predominantly low density residential environment having regard to sunlight and solar access, privacy, hours of operation, generation of odours, noise, waste products, dust, traffic, electrical interference, lighting and visual impacts;</li> <li>(v) ensure all car parking needs can be met on site, and off-street car parking areas do not dominate the appearance in the streetscape and are landscaped to provide an attractive frontage that maximise pedestrian activity and safety.</li> </ul>	<p><b>N/A:</b> The proposed development does not involve a non-residential use.</p>
<p>(p) Development responds to the natural topography and environmental constraints of the land and avoids or where this is not possible, mitigates any adverse impacts on areas of ecological significance, including creeks, gullies, steep land, waterways, wetlands, habitats and vegetation and bushland through location, design, operation and management.</p>	<p><b>N/A:</b> The proposed development is not in proximity to areas of ecological significance.</p>
<p>(q) Development for a sensitive use on land within 250m of land within the Medium Impact Industry Zone is designed to mitigate industrial air, noise</p>	<p><b>N/A:</b> The proposed development is not within 250m of land in the Medium Impact Industry Zone.</p>

Overall Outcome	Response
and odour emissions with potential to adversely impact on human health, amenity and wellbeing.	

The above assessment demonstrates that the proposed development generally satisfies the intent and outcomes sought for land included in the Low Density Residential Zone.

### 4.7.3 APPLICABLE CODES

The codes applicable to the assessment of the proposed development are identified in **Table 3** with a detailed assessment against the applicable assessment benchmarks at **Appendix B**.

**TABLE 3 - APPLICABLE CODES**

#### Zone Code

- Low Density Residential Zone Code

#### Overlay Codes

- Airport Environs Overlay Code

#### Development Codes

- Medium Density Residential Code
- Environmental Standards Code
- Integrated Water Cycle Management Code
- Landscaping Code
- Transport, Access and Parking Code
- Works and Services Code

## **5.0 CONSULTATION**

### **5.1 STATUTORY NOTIFICATION**

This Development Application is Impact Assessable and accordingly is required to be publicly notified under the provisions of the *Planning Act 2016* and its associated regulations. Public notification will be undertaken at the appropriate stage of the assessment process, as set out in the Development Assessment Rules prescribed under the *Planning Regulation 2017*.

### **5.2 PRE-LODGEMENT MEETINGS**

The proposed development was not the subject of pre-lodgement discussions.

## 6.0 CONCLUSION

This Development Application seeks approval for a Material Change of Use for Short Term Accommodation on land at 22 Rowbotham Street, Rangeville. The assessment that has been undertaken has demonstrated the following:

- The development is of a low scale and intensity and is consistent and compatible with the surrounding locality and intent of the zone.
- The proposed development has been designed to ensure potential sources of nuisance and impact can be appropriately managed as to not affect the amenity of the adjoining residential uses.
- The proposed development involves reuse of an existing dwelling and does not involve new building work or any changes to the character of the dwelling or surrounding locality.
- The proposed development is appropriately serviced by existing infrastructure connections and does not require new connections to facilitate the development.
- The proposed development achieves compliance with the benchmarks within the relevant zone, overlay and development codes (as outlined in section 4.7).

Having regard to the matters and issues raised in this report it is recommended that Council support this Development Application for a Development Permit for Material Change of Use.

The proposal warrants approval subject to the imposition of reasonable and relevant conditions.

## **APPENDIX A - STRATEGIC FRAMEWORK**

*Toowoomba Regional Planning Scheme 2012*

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

Theme/Element	Relevance Yes/No	Assessment
<b>3.3 SETTLEMENT PATTERN</b>		
3.3.1 Strategic Outcomes	<b>Yes</b>	The proposed development involves short term accommodation within an existing dwelling which will contribute to the strong economic base in Toowoomba. The proposed development will contribute to thriving and diverse neighbourhoods within close proximity to schools, parkland and other services. The proposed development will not involve inappropriate densification or impacts to the residential amenity of the area.
3.3.2 Element – network of towns	<b>Yes</b>	The proposed development will contribute to Toowoomba's diverse economy. The proposed development will be located in a well-connected and accessible part of the region.
3.3.3 Element – compact urban form	<b>Yes</b>	The proposed development will contribute to diverse neighbourhoods within an existing urban area. The proposed development will be well-located to connect people and places conveniently and effectively.
3.3.4 Element – suburban neighbourhoods	<b>Yes</b>	The proposed development involves short term accommodation within an existing dwelling and is at a scale compatible and consistent with the surrounding locality. The proposed development is well-connected with options for active transport.
3.3.5 Element – urban neighbourhoods	No	
3.3.6 Element – centres of activity	No	
3.3.7 Element – central business district (CBD)	No	
3.3.8 Element – sustainable urban development	<b>Yes</b>	The proposed development will be at a scale consistent and compatible with the surrounding area, unlikely to impact on established residential amenity. The proposed development is located in an area with a range of transport options including active transport to support sustainable transport.
3.3.9 Element – rural landscape	No	
3.3.10 Element – natural places	No	
3.3.11 Element – development constraints	No	
3.3.12 Element – incompatible land uses	<b>Yes</b>	The proposed development will be at a scale consistent and compatible with the surrounding area and unlikely to impact on the established residential amenity. The proposed development involves strategies such as a Noise Complaints Management Procedure to mitigate impacts to the surrounding dwellings.

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

Theme/Element	Relevance Yes/No	Assessment
<b>3.4 NATURAL ENVIRONMENT</b>		
3.4.1 Strategic Outcomes	<b>Yes</b>	The proposed development does not involve any changes to the existing dwelling or new building work that will impact on natural environmental values. The proposed development will not be located in proximity to areas of ecological significance.
3.4.2 Element - protect ecosystems with biodiversity values	No	
3.4.3 Element – waterways, wetlands and aquifers	No	
3.4.4 Element - air quality	<b>Yes</b>	The proposed development will be at a scale consistent and compatible with the surrounding area and unlikely to impact on the established residential amenity. The proposed development involves strategies such as a Noise Complaints Management Procedure to mitigate impacts to the surrounding dwellings.
3.4.5 Element – climate change	<b>Yes</b>	The proposed development is located in an area with a range of transport options including active transport to support sustainable transport. The proposed development is located within an established urban area for efficiency of infrastructure servicing.
3.4.6 Element – environmental offsets	No	
<b>3.5 COMMUNITY IDENTITY AND DIVERSITY</b>		
3.5.1 Strategic Outcomes	<b>Yes</b>	The proposed development will contribute to the diverse activities within the neighbourhood area. The proposed development is located in an area well-serviced by community facilities. The proposed development has been designed to cater for families visiting hospital patients or children at boarding schools and contribute to the wider economy.
3.5.2 Element – rural community identity	No	
3.5.3 Element – Toowoomba City identity	No	
3.5.4 Element – urban design	<b>Yes</b>	The proposed development contributes to the range and mix of activities within urban areas, within a walkable and well-connected suburb.
3.5.5 Element – community facilities and services	<b>Yes</b>	The proposed development will contribute to the range of accommodation facilities available in the region. The proposed development is located in an area that is well-connected to parks, schools and local facilities.
3.5.6 Element – sport and recreation	No	
3.5.7 Element – urban parks and public places	No	

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

Theme/Element	Relevance Yes/No	Assessment
3.5.8 Element – housing diversity and affordability	No	
3.5.9 Element – cultural diversity and heritage	No	
<b>3.6 NATURAL RESOURCES AND LANDSCAPE</b>		
3.6.1 Strategic Outcomes	<b>Yes</b>	The proposed development involves short term accommodation within an existing dwelling and will not impact on natural resources or landscape values.
3.6.2 Element – scenic amenity	<b>Yes</b>	The proposed development involves short term accommodation within an existing dwelling that benefits from high scenic amenity values, and allows the opportunity for visitors to the area to appreciate the scenic landscapes to the east.
3.6.3 Element – water resources	No	
3.6.4 Element – sustainable production	No	
3.6.5 Element – natural economic resources	No	
3.6.6 Element – stock routes	No	
<b>3.7 ACCESS AND MOBILITY</b>		
3.7.1 Strategic Outcomes	<b>Yes</b>	The proposed development involves short term accommodation within an existing dwelling in an area with practical and integrated transport options. The proposed development is of a scale consistent and compatible with the surrounding area and is unlikely to introduce inappropriate traffic to the local area, or impact on the neighbourhood amenity. The proposed development involves existing parking arrangements that are sufficient for the nature of the use.
3.7.2 Element – integrated transport system	<b>Yes</b>	The proposed development will contribute to the mix of land uses in the neighbourhood and will be well-serviced by transport options.
3.7.3 Element – active transport	No	
3.7.4 Element – public transport	No	
3.7.5 Element – road network	<b>Yes</b>	The proposed development is of a scale consistent and compatible with the surrounding area and is unlikely to introduce inappropriate traffic to the local area, or impact on the neighbourhood amenity. The proposed development has access to a road network that will sufficiently service the development.

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

Theme/Element	Relevance Yes/No	Assessment
<b>3.8 INFRASTRUCTURE AND SERVICES</b>		
3.8.1 Strategic Outcomes	<b>Yes</b>	The proposed development involves short term accommodation within an existing dwelling that is serviced by all appropriate infrastructure for the scale of the use.
3.8.2 Element – coordinated infrastructure planning and delivery	<b>Yes</b>	The proposed development involves short term accommodation within an existing dwelling that is serviced by all appropriate infrastructure for the scale of the use.
3.8.3 Element – integrating water management and infrastructure	<b>Yes</b>	The proposed development involves short term accommodation within an existing dwelling that is serviced by water infrastructure and does not involve any changes to stormwater runoff.
3.8.4 Element – waste water management infrastructure and services	<b>Yes</b>	The proposed development involves short term accommodation within an existing dwelling that is serviced by wastewater infrastructure. There are no changes proposed to the existing service arrangements.
3.8.5 Element – utility infrastructure and services	<b>Yes</b>	The proposed development involves short term accommodation within an existing dwelling that is serviced by existing telecommunications and energy infrastructure connections.
3.8.6 Element – waste management and recycling	<b>Yes</b>	The proposed development involves strategies to minimise the off-site impacts of waste including weekly kerbside collection organised by the property manager.
<b>3.9 ECONOMIC DEVELOPMENT</b>		
3.9.1 Strategic Outcomes	<b>Yes</b>	The proposed development involves short term accommodation that contributes to the diverse mix of business and tourism activities in the region.
3.9.2 Element - economic growth	No	
3.9.3 Element – activity centres and employment	No	
3.9.4 Element – creative and knowledge-based industries	No	
3.9.5 Element – tourism	<b>Yes</b>	The proposed development involves short term accommodation that contributes to the range of accommodation options available to support local tourism.

## **APPENDIX B - ASSESSMENT BENCHMARKS**

*Toowoomba Regional Planning Scheme 2012*

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Low Density Residential Zone Code – requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Caretaker's Accommodation</b>		
<p>PO<sub>1</sub> Development provides for the accommodation of a caretaker, and their family members, involved in the running of a non-residential use, in a manner that:</p> <ul style="list-style-type: none"> <li>(a) does not compromise the productivity of the use;</li> <li>(b) is safe and comfortable for the amenity of residents; and</li> <li>(c) has regard to the landscape and private recreation needs of the residents.</li> </ul>	<p>AO<sub>1.1</sub> A caretaker's accommodation is:</p> <ul style="list-style-type: none"> <li>(a) (a) separated from significant levels of emissions (adverse to human health or amenity) generated by the non-residential use/s of the site by at least 6m;</li> <li>(b) (b) provided with a private landscape and recreation area which:               <ul style="list-style-type: none"> <li>(i) is directly accessible from a habitable room;</li> <li>(ii) if at ground level, has a minimum area of 16m<sup>2</sup> with minimum dimensions of 4m; and</li> <li>(iii) where provided as a balcony, verandah or deck has a minimum area of 8m<sup>2</sup> with a minimum dimension of 2.4m.</li> </ul> </li> </ul> <p>AO<sub>1.2</sub> No more than one (1) caretaker's accommodation is established per non-residential use.</p>	<p><b>N/A:</b> The proposed development involves short term accommodation and does not involve caretaker's accommodation.</p>
<b>Dwelling Unit</b>		
<p>PO<sub>2</sub> Development incorporating a dwelling unit is provided in a manner that:</p> <ul style="list-style-type: none"> <li>(a) integrates visually with the non-residential use through the use of materials, colours and finishes, architectural treatments and landscaping treatments;</li> <li>(b) ensures residents have a high level of safety, security, privacy and amenity;</li> </ul>	<p>AO<sub>2.1</sub> Separate entrances are provided to the dwelling unit and non-residential uses on the same site.</p> <p>AO<sub>2.2</sub> Entries to the dwelling unit are clearly identifiable from the street and have a defined pathway.</p> <p>AO<sub>2.3</sub> Dwelling units are readily distinguishable from the non-residential use for emergency service providers.</p>	<p><b>N/A:</b> The proposed development involves short term accommodation and does not involve a dwelling unit.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Low Density Residential Zone Code – requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(c) ensures residents are not exposed to noise and light nuisance from the non-residential use; and</p> <p>(d) provides residents access to private outdoor recreation spaces directly from the dwelling unit.</p>	<p>AO<sub>2.4</sub> The dwelling unit number is clearly displayed on the unit and letter box.</p> <p>AO<sub>2.5</sub> The dwelling unit is provided with a private landscape and recreation area which:</p> <p>(a) is directly accessible from a habitable room;</p> <p>(b) if at ground level, has a minimum area of 16m<sup>2</sup> with minimum dimensions of 4m; and</p> <p>(c) if a balcony, a veranda or a deck, has a minimum area of 8m<sup>2</sup> with minimum dimensions of 2.4m.</p> <p>AO<sub>2.6</sub> Habitable rooms of the dwelling unit are protected from overlooking of the non-residential use/area by:</p> <p>(a) separation of 9m from a window or activity area of the non-residential use; or</p> <p>(b) screening the outlook from windows, balconies or activity area of the non-residential use within a direct view of 9m to the habitable room or private open space of the dwelling unit.</p>	
<b>Non-residential Uses and building work – Scale of use where involving the reuse of an existing building</b>		
<p>PO<sub>3</sub> The non-residential use is of a small scale and intensity that is compatible with the character of the streetscape and the low density residential built form appearance of the locality.</p>	<p>AO<sub>3.1</sub> The use:</p> <p>(a) is carried out in an existing building;</p> <p>(b) only increases the Gross Floor Area of the existing building by a maximum of 25m<sup>2</sup> and does not reduce existing front or side setbacks;</p> <p>(c) is a single tenancy only;</p> <p>(d) has a maximum gross floor area of 200m<sup>2</sup>;</p> <p>(e) is conducted wholly within an enclosed building; and</p>	<p><b>N/A:</b> The proposed development involves short term accommodation and does not involve a non-residential use or building work.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Low Density Residential Zone Code – requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	(f) does not involve outdoor dining or drive through facility.	
<b>Non-residential Uses and building work – Noise Amenity</b>		
PO <sub>4</sub> The non-residential use does not adversely impact on the amenity of the surrounding residential land uses and/or the intended residential streetscape character.	AO <sub>4.1</sub> New building plant or air-conditioning equipment is located central to the building and screened from view of the street or adjoining residential uses.	<b>N/A:</b> The proposed development involves short term accommodation and does not involve a non-residential use or building work.
<b>Non-residential Uses and building work – Privacy and Screening</b>		
PO <sub>5</sub> Non-residential uses provide adequate separation, buffering and screening from adjoining residential premises so that the privacy and amenity of residential premises is protected.	AO <sub>5.1</sub> A 2m wide vegetated buffer is provided to any vehicle movement and parking areas that adjoin a residential boundary. AO <sub>5.2</sub> A 1.8m high solid screen fence and 1.5m wide strip of screen landscaping are provided along all boundaries shared with an adjoining residential use. AO <sub>5.3</sub> Windows that have direct views into adjoining residential buildings are provided with fixed screening that is a maximum of 75% transparent to obscure views into the adjoining residential building and maintain privacy for those residents.	<b>N/A:</b> The proposed development involves short term accommodation and does not involve a non-residential use or building work.
<b>Non-residential Uses and building work – Outdoor Lighting</b>		
PO <sub>6</sub> Outdoor lighting for non-residential uses maintains the amenity of the surrounding residential area and does not adversely impact the safety for vehicles or pedestrians on the adjoining street as a result of light emissions, either directly or by reflection.	AO <sub>6.1</sub> Outdoor lighting for non-residential uses is restricted to low level security lighting only. AO <sub>6.2</sub> Outdoor lighting is designed, installed and maintained in accordance with the parameters and requirements of AS4282 – <i>Control of the Obtrusive Effects of Outdoor Lighting</i> .	<b>N/A:</b> The proposed development involves short term accommodation and does not involve a non-residential use or building work.

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Low Density Residential Zone Code – requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Utilities and Stormwater</b>		
PO <sub>7</sub> A water supply is provided that is adequate for the current and future needs of the intended use.	AO <sub>7.1</sub> Where within a water supply area, the development is connected to Council's reticulated water supply system in accordance with SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure.	<b>Complies:</b> The proposed development has an existing connection to reticulated water.
PO <sub>8</sub> Wastewater treatment and disposal is provided that is appropriate for the level of demand generated, protects public health and avoids environmental harm.	AO <sub>8.1</sub> Where within a wastewater area, the development is connected to the Council's reticulated wastewater system in accordance with SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure.	<b>Complies:</b> The proposed development has an existing connection to reticulated wastewater.
PO <sub>9</sub> The development is equipped with an adequate energy supply approved by and installed in accordance with the standards of the relevant energy regulatory authority.	AO <sub>9.1</sub> Premises are connected to an electricity supply approved by the relevant energy regulatory authority.	<b>Complies:</b> The proposed development has an existing connection to an electricity supply.
PO <sub>10</sub> Stormwater resulting from roofed areas and impervious surfaces is collected and discharged in a manner that does not adversely affect the stability of buildings or the use of adjacent land.	AO <sub>10.1</sub> Roof water and impervious surfaces water runoff is collected and discharged in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>N/A:</b> The proposed development does not involve new building work or changes to the existing dwelling.
<b>Waste Management</b>		
PO <sub>11</sub> Appropriate refuse container storage areas are provided which are: (a) in a building or enclosing structure or screened from public view; (b) of adequate size to accommodate the expected amount of refuse to be generated by the use; (c) in a position that is conveniently accessible for collection; and	AO <sub>11.1</sub> Refuse container storage areas are provided that: (a) are located behind the building line and screened from public view: (i) in a building, outbuilding or other enclosed structure; (ii) screened by a minimum 1.5 m high solid fence or wall that is surrounded by minimum 1m wide landscaping (excluding container storage access	<b>Performance Solution:</b> The proposed development will involve storage of refuse containers consistent with the scale and nature of the use, and out of sight from the street where possible. The proposed development will involve relocation of container storage to the kerbside for collection every week by the property manager.

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**Low Density Residential Zone Code – requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(d) able to be kept in a clean state at all times, and waste is captured and discharges to an approved collection point.</p>	<p>point) where not adjoining a residential boundary; or;</p> <p>(iii) screened by a minimum 1.8m high solid fence where adjoining a residential boundary</p> <p>(b) are provided with an imperviously sealed pad, on which to stand the bin(s), that is drained to an approved waste disposal system;</p> <p>(c) are within normal hose length of a hose cock; and</p> <p>(d) are large enough to accommodate at least one (1) standard/sized container per dwelling and, in commercial and industrial premises, one (1) or more industrial bins of a size appropriate to the nature and scale of use.</p>	
<p><b>Non-residential uses and building work – Access, on-site car parking and manoeuvring</b></p>		
<p>PO<sub>12</sub> Provision is made for on-site vehicle parking to meet the demand likely to be generated by the development and:</p> <p>(a) to avoid on-street parking where that would adversely impact on the safety or capacity of the road network or unduly impact on local amenity.</p> <p>(b) to ensure that off-street car parking areas do not dominate the appearance in the streetscape.</p>	<p>PO<sub>12.1</sub> Where not involving the reuse of an existing premises used for a business activity car parking is provided in accordance with the Transport, Access and Parking Code.</p> <p>PO<sub>12.2</sub> Where involving re-use of premises used for a business activity:</p> <p>(a) There is no reduction in existing or previously approved on-site car parking;</p> <p>(b) There are no alterations to the location of existing or previously approved access (driveways and vehicle cross-overs) and on-site car parking and manoeuvring areas.</p>	<p><b>N/A:</b> The proposed development involves short term accommodation and does not involve a non-residential use or building work.</p>

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Low Density Residential Zone Code – requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Non-residential uses and building work – Landscaping</b>		
PO <sub>13</sub> Landscaping makes a positive contribution to the site and the amenity of the surrounding area and existing landscaping is not diminished.	PO <sub>13.1</sub> Where involving the reuse of a premises there is no reduction in the area or quantity of established or previously approved on-site landscaping.	<b>N/A:</b> The proposed development involves short term accommodation and does not involve a non-residential use or building work.

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**Low Density Residential Zone Code – requirements for accepted development and assessment benchmarks for assessable development where not involving Building Work (not associated with a Material Change of Use)**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Non-residential uses - Hours of Operation</b>		
PO <sub>1</sub> Hours of operation are controlled so that the non-residential use does not impact on the amenity or privacy of adjoining residential uses.	AO <sub>1.1</sub> Hours of operation, including for deliveries, are limited to hours between 6:00 am to 9:00 pm.	<b>N/A:</b> The proposed development involves short term accommodation and does not involve a non-residential use or building work.
<b>Non-residential Uses – Impact on Road Network</b>		
PO <sub>2</sub> Non-residential uses are located on major roads and do not introduce non-residential traffic into local streets.	AO <sub>2.1</sub> Non-residential uses: (a) have frontage and vehicle access to a Regional Arterial, Sub-Arterial or Distributor Road; and (b) vehicle access to the development does not occur from a local street.	<b>N/A:</b> The proposed development involves short term accommodation and does not involve a non-residential use or building work.

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**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Residential Development</b>		
PO <sub>1</sub> The zone accommodates predominantly dwelling houses and other housing forms at a low density scale and intensity.	AO <sub>1.1</sub> Uses which are consistent with the intent of the zone include: (a) dwelling house; and (b) dual occupancy, except where located in the Clifford Park Stables and Park Residential Precincts	<b>Complies:</b> The proposed development involves reuse of a dwelling house at a similar low scale and intensity. The proposed development does not introduce uses at an inappropriate scale or density for the area.
PO <sub>2</sub> The density of accommodation activities in the Clifford Park Stables Precinct is not increased, other than where directly associated with the management of stables on the same site.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development is not located in the Clifford Park Stables Precinct.
PO <sub>3</sub> Short-term accommodation occurs where: (a) it is of a small scale that is consistent with the intensity of development in the surrounding residential area; (b) adjoining or located on the opposite side of a road to existing Short-term accommodation or other Accommodation activities of a similar scale and density ; (c) all car parking needs can be met on site; (d) have direct vehicle access to a distributor, sub-arterial and regional arterial level road that is not a State-controlled road; and (e) do not unduly detract from the amenity of nearby residences.	No acceptable outcome is nominated.	<b>Complies:</b> The proposed development involves reuse of an existing dwelling, is of a similar scale to the surrounding residential area and involves sufficient carparking spaces on-site. The proposed development has direct access to a distributor road and will not introduce an inappropriate amount of traffic to the existing road network. The proposed development is unlikely to impact on residential amenity. The proposed development has been designed to maintain a high level of residential amenity by restricting bookings to one (1) group at a time, strict booking rules including no parties, limiting the number of guests, and managing potential impacts through measures such as a Noise Complaints Management Procedure; refer to <b>Appendix D</b> .
PO <sub>4</sub> Higher density forms of Accommodation activities (i.e. other than caretaker's accommodation, community residence, dwelling house and dual occupancy) are	AO <sub>4.1</sub> The number of dwellings on the site does not exceed one per 500m <sup>2</sup> . AO <sub>4.2</sub> The site has a minimum frontage of 12m.	<b>Complies:</b> The proposed development does not involve increasing dwellings on-site. The site has an existing frontage greater than 12m.

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**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
designed to reflect the residential scale and density of the surrounding area.		
<b>Non-Residential Development</b>		
PO <sub>5</sub> Non-residential uses establish only where they: <ul style="list-style-type: none"> <li>(a) except for child care centres, provide for the day to day convenience needs or local service of the immediate local residential community;</li> <li>(b) do not undermine the viability of a nearby centre and are not of a scale that impacts on the role or function centres network;</li> <li>(c) do not contribute to strip development or expansion of an existing centre;</li> <li>(d) are a of a small scale, and have low intensity operation and employment;</li> <li>(e) are highly accessible to the immediate local community it serves and have direct vehicle access to a distributor, sub-arterial and regional arterial;</li> <li>(f) do no introduce non-local traffic into a local street;</li> <li>(g) are in buildings, including extensions and alterations to an existing building, that have a low rise bulk and scale compatible with a dwelling house and consistent with the character of adjoining residential buildings and the surrounding streetscape;</li> <li>(h) do not adversely impact the amenity, safety or privacy of nearby residences.</li> </ul>	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development involves short term accommodation and does not involve a non-residential use.

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**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Built Form</b>		
PO <sub>6</sub> Buildings are of a height which is consistent with the intended character of the zone and overlay, and which do not unduly reduce privacy or access to sunlight to habitable rooms, private open space and solar panels on adjoining land.	AO <sub>6.1</sub> Buildings have a maximum height of two (2) storeys or 8.5m above ground level.	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve new building work.
PO <sub>7</sub> Except for Dwelling Houses and Dual Occupancies site coverage: (a) maximizes setbacks; (b) maximizes landscaping; (c) ensures adequate useable outdoor areas; (d) ensures adequate space for vehicle movement and parking areas; (e) maximizes solar access for internal and external living spaces; (f) does not compromise solar access for adjoining premises; and (g) does not result in overshadowing of adjoining properties.	AO <sub>7.1</sub> For development up to two (2) storeys site cover does not exceed 50% of the site area.  For development over two (2) storeys no acceptable outcome is nominated.	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve new building work.
PO <sub>8</sub> Impervious site coverage: (a) ensures development maximizes on-site infiltration and minimizes the additional burden on drainage infrastructure; (b) reduces the visual impact of additional hardstand; (c) respects the existing or preferred neighbourhood character and responds to the features of the site; and (d) allows for the provision of an appropriate supply of landscaping and open space.	AO <sub>8.1</sub> Impervious areas of the site do not exceed 60% of the site area.	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve new building work.

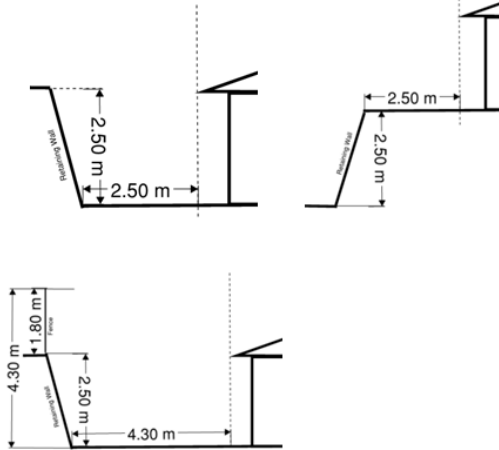
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**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>9</sub> The front building setback is consistent with the prevailing front setbacks in the street.</p>	<p>AO<sub>9.1</sub> Where the site has frontage to a collector or local road, buildings are set back from that frontage:</p> <ul style="list-style-type: none"> <li>(a) within 20% of the average front setback of adjoining buildings; or</li> <li>(b) where there are no adjoining buildings, 4m.</li> </ul> <p>AO<sub>9.2</sub> Where the site has frontage to a road other than a collector or local road:</p> <ul style="list-style-type: none"> <li>(a) residential buildings are set back from that frontage in accordance with the requirements of the Queensland Development Code.</li> <li>(b) non-residential buildings are set back from that frontage 6m.</li> </ul> <p>AO<sub>9.3</sub> Where involving non-residential use, buildings are set back a minimum 6m from the road frontage.</p>	<p><b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve new building work.</p> <p><b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve new building work.</p> <p><b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve a non-residential use.</p>
<p>PO<sub>10</sub> Side and rear building setbacks:</p> <ul style="list-style-type: none"> <li>(a) enhance the appearance and character of streets and buildings;</li> <li>(b) are appropriate to the scale of the development and the intended low density character of the zone or precinct in which the site is located;</li> <li>(c) provide for adequate daylight for habitable rooms and open space areas on and adjoining the site;</li> <li>(d) are sufficient to minimise overshadowing and overlooking of adjoining premises;</li> </ul>	<p>AO<sub>10.1</sub> Buildings are set back from a side boundary:</p> <ul style="list-style-type: none"> <li>(a) Ground Floor (up to 3.5m high) – 1.5m; and</li> <li>(b) First floor (up to 7.5m high) – 2m;</li> <li>(c) Above 7.5m – 3m; or</li> <li>(d) the distance of the height of the retaining wall on the side boundary or combination of the height of the retaining wall and a fence, whichever is greater (Figure 1). Where the retaining wall extends into the site, the setback is measured from the base or top of the retaining wall.</li> </ul> <p>AO<sub>10.2</sub> Buildings are set back:</p> <ul style="list-style-type: none"> <li>(a) a minimum of 3m from a rear boundary; or</li> </ul>	<p><b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve new building work.</p> <p><b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve new building work.</p>

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Low Density Residential Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(e) provide adequate separation and buffering between residential and non-residential premises; and</p> <p>(f) maximise opportunities for landscaping.</p>	<p>(b) the distance equivalent to of the cumulative height of the retaining wall and fence on the rear boundary (Figure 1). Where the retaining wall extends into the site, the setback is measured from the base or top of the retaining wall.</p>  <p>Figure 1</p> <p>For non-residential development:</p> <p>AO<sub>10.3</sub> Buildings are set back a minimum of 2.5m from a side boundary.</p> <p>AO<sub>10.4</sub> Buildings are set back from a rear boundary whichever is the greater -</p> <p>(a) a minimum of 3m; or.</p>	<p><b>N/A:</b> The proposed development does not involve a non-residential use.</p>

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**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	(b) The distance equivalent to the cumulative height of the retaining wall and fence on the rear boundary. Where the retaining wall extends into the site, the setback is measured from the base or top of the retaining wall.	
PO <sub>11</sub> The development is designed to a high aesthetic standard and to integrate with and enhance the locality, having regard to the following: (a) built form; (b) open space; (c) landscaping; (d) the public realm; (e) parking and access; (f) solar access; (g) views and outlook; (h) setbacks; (i) height; (j) scale and bulk; and (k) provision of services.	In partial compliance with the performance outcome:  AO <sub>11.1</sub> A minimum 1m wide landscaping strip is provided for the full length of the driveway and parking areas where parallel to a boundary common with a residential premises.  <i>Note: Such landscaping areas must not be compromised by the maneuvering areas required for the vehicle.</i>	<b>Performance Solution:</b> The proposed development involves short term accommodation within an existing dwelling. The proposed development is a scale and nature consistent and compatible with the surrounding residential area. The proposed development involves sufficient carparking on-site and existing service connections. The proposed development is not of a scale or nature to involve provision of additional landscaping or new building work.
PO <sub>12</sub> Non-residential use buildings, including extensions or additions, exhibit design elements that are characteristic of and compatible with surrounding houses in the streetscape including:- (a) buildings orient to and addresses the street frontage; (b) the main building entry faces the street at ground level; (c) building mass is broken up by articulation, fenestration, recesses and landscape elements to avoid large expanses of blank surfaces ;	No acceptable outcome is nominated	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve a non-residential use.

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Low Density Residential Zone Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(d) differentiate between private houses and the non-residential use through variation in materials, patterns, textures and/or colours</p> <p><b>Editor's note</b> – examples of façade treatment to avoid expanses of blank surface may include (but is not limited to):</p> <ul style="list-style-type: none"> <li>• windows are provided in any building façade facing the street or other public and communal space;</li> <li>• awnings, verandahs, pitched roofs and rectangular building plan forms are used in the building design.</li> </ul>		
<p>PO13 Where appropriate, development facilitates active transport and open space connections through the neighbourhood.</p>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The proposed development is not of a scale or nature to warrant new active transport or open space connections.</p>
<p>PO14 The site layout responds sensitively to adjoining land uses as well as on-site and surrounding topography, drainage patterns, utility services, access, built forms and vegetation such that:</p> <ol style="list-style-type: none"> <li>(a) any hazards or nuisance to people or property on the site or offsite are avoided;</li> <li>(b) any earthworks are minimised and design alternatives are prioritised over earthworks;</li> <li>(c) natural drainage lines are retained;</li> <li>(d) existing vegetation is retained or replaced;</li> <li>(e) damage or disruption to sewer, stormwater and water infrastructure is avoided; and</li> <li>(f) there is adequate buffering, screening a separation to adjoining development.</li> </ol>	<p>No acceptable outcome is nominated.</p>	<p><b>Complies:</b> The proposed development involves reuse of an existing dwelling and is of a similar scale to the surrounding residential area. The proposed development involves sufficient carparking on-site and existing service connections. The proposed development has been designed to maintain a high level of residential amenity by restricting bookings to one (1) group at a time, strict booking rules including no parties, limiting the number of guests, and managing potential impacts through measures such as a Noise Complaints Management Procedure; refer to <b>Appendix D</b>.</p>

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**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Amenity and Safety</b>		
PO <sub>15</sub> Development maintains a high level of residential amenity within the site and for surrounding areas, having regard to noise, odour, lighting, access to sunlight, privacy and outlook.	No acceptable outcome is nominated.	<b>Complies:</b> The proposed development has been designed to maintain a high level of residential amenity by restricting bookings to one (1) group at a time, strict booking rules including no parties, limiting the number of guests, and managing potential impacts through measures such as a Noise Complaints Management Procedure; refer to <b>Appendix D</b> .
PO <sub>16</sub> Site layout, building design and landscaping facilitates the security of people and property having regard to: (a) opportunities for casual surveillance of and sightlines to publicly accessible areas such as car parks, pathways, public toilets and communal areas; (b) exterior building design and orientation which promote safety; (c) adequate definition of uses and public and private ownership; (d) adequate lighting; (e) appropriate way-finding mechanisms (e.g. signage); (f) minimisation of entrapment locations; and (g) building entrances, loading and storage areas being well lit and lockable after hours.	AO <sub>16.1</sub> Setbacks are provided from all boundaries in accordance with acceptable outcome AO <sub>10.2</sub> ; AO <sub>16.2</sub> The development does not introduce lighting which is inconsistent with a residential area; AO <sub>16.3</sub> Sunlight access for the private open space or habitable rooms on adjoining properties or private open spaces on the subject site is not reduced to less than 3 hours between 9am and 3pm on June 21, or AO <sub>16.4</sub> Where existing overshadowing by building and fences is greater than this, sunlight is not further reduced by 20%.	<b>N/A:</b> The proposed development does not involve new building work.
PO <sub>17</sub> Development is designed to incorporate graffiti-prevention measures.	AO <sub>17.1</sub> Building design and layout incorporates the following features where practical: (a) designs with an absence of 'natural ladders';	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve new building work.

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**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(b) minimal unbroken vertical surface areas; and</li> <li>(c) graffiti-deterrent surface treatments.</li> </ul>	
PO <sub>18</sub> Development for non-residential uses provides landscaping that: <ul style="list-style-type: none"> <li>(a) is consistent with the dominant landscape character appearance of the streetscape in an established area and intended in the zone and character overlay in which the site is located;</li> <li>(b) provides an attractive interface between the use, the streetscape and adjoining residential uses</li> <li>(c) provides and maintains:               <ul style="list-style-type: none"> <li>(i) the privacy and amenity for adjoining residential uses</li> <li>(ii) sight lines and overlooking to public spaces and the street to enable casual surveillance</li> <li>(iii) a clearly defined pedestrian entry point for visitors and customers that is separated from the driveway;</li> <li>(iv) established trees (including street trees) and other significant existing vegetation.</li> </ul> </li> </ul>	AO <sub>18.1</sub> Where not involving reuse of a premises development provides a minimum 3m wide landscaped garden strip along the frontage of the site.	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve a non-residential use.
PO <sub>19</sub> Development for non-residential uses provide car parking and loading and servicing areas that: <ul style="list-style-type: none"> <li>(a) are located to minimise impact on any adjoining residential premises</li> </ul>	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve a non-residential use.

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**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(b) are located behind the building, and hardstand areas do not dominate the streetscape;</li> <li>(c) prioritise the movement and safety of pedestrians along the frontage of the site, and between the street frontage and the entrance to the building;</li> <li>(d) are integrated into the building design and include screening and buffering to reduce negative impact on adjoining residential uses</li> </ul>		
PO <sub>20</sub> Development for a sensitive use on land within 250m of land within the Medium Impact Industry Zone must not result in that use being exposed to industrial air, noise or odour emissions that impact on human health, amenity and wellbeing.	AO <sub>20.1</sub> The use is designed to ensure that: <ul style="list-style-type: none"> <li>(a) the indoor noise objectives set out in the <i>Environmental Projection (Noise) Policy 2008</i> are met; and</li> <li>(b) the air quality objectives in the <i>Environmental Protection (Air) Policy 2008</i>, are met.</li> </ul> <p><i>Note: Design measure that may assist in achieving the acceptable outcome may include:</i></p> <ul style="list-style-type: none"> <li>(a) landscaping;</li> <li>(b) setbacks;</li> <li>(c) the orientation of buildings away from the industrial area; and</li> <li>(d) barriers, mounds and fencing; and/or screening.</li> </ul>	<b>N/A:</b> The proposed development will not be in proximity to land in the Medium Impact Industry Zone.
PO <sub>21</sub> Where located on land that is affected by environmental emissions generated by an operational rail corridor, sensitive development mitigates noise generated by the railway to the	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development is not in proximity to an operational rail corridor.

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**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>extent that it adversely impacts on the development.</p>		
<b>Development involving Animal Keeping (where for Stables) in the Clifford Park Stables Precinct</b>		
<p>PO<sub>22</sub> The minimum site area for Animal Keeping (where for stables) is sufficient to:</p> <ul style="list-style-type: none"> <li>(a) meet the needs of the use;</li> <li>(b) accommodate a dwelling house on the same premises; and</li> <li>(c) maintain an overall scale and intensity of development consistent with the character of the local area.</li> </ul>	<p>AO<sub>22.1</sub> The site area for a Animal Keeping (where for stables) is a minimum of 1,200m<sup>2</sup>.</p>	<p><b>N/A:</b> The proposed development does not involve animal keeping and is not in the Clifford Park Stables Precinct.</p>
<p>PO<sub>23</sub> Animal Keeping (where for stables) does not adversely affect the health and safety of residents of the site or adjoining premises and do not unduly impact on the amenity of the local area.</p>	<p>AO<sub>23.1</sub> No horse is stabled closer than 15m to a residential building on an adjoining lot.</p> <p>AO<sub>23.2</sub> Stable buildings are constructed with impervious reinforced concrete floors, gravity drained to the effluent collection/treatment point.</p> <p>AO<sub>23.3</sub> Exterior walls of buildings are constructed of sound absorbent material being brick, concrete, masonry or other similar material.</p> <p>AO<sub>23.4</sub> A person who is responsible for the supervision of the stables is accommodated on the premises at all times.</p> <p>AO<sub>23.5</sub> A minimum 1.8m high solid screen fence is provided on the site along all common boundaries.</p> <p>AO<sub>23.6</sub> Wastes are collected and disposed of daily.</p> <p>AO<sub>23.7</sub> All food/waste holding areas and receptacles are contained and covered.</p> <p>AO<sub>23.8</sub> A health management plan approved by Council identifies potential health and amenity hazards associated with the stables such as</p>	<p><b>N/A:</b> The proposed development does not involve animal keeping and is not in the Clifford Park Stables Precinct.</p>

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**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	vermin and other pests, animal waste, other odour sources and the methods by which these hazards are to be addressed and managed.	
PO <sub>24</sub> Residential development maintains an overall low density, domestic scale and intensity to: (a) maintain suitable development sites for Animal Keeping (where for stables) to co-locate with residential uses; and (b) limit conflicts between Animal Keeping (where for stables) and neighbouring residential uses.	AO <sub>24</sub> Accommodation activities are limited to Dwelling Houses and Community Residences.	<b>N/A:</b> The proposed development does not involve animal keeping and is not in the Clifford Park Stables Precinct.
<b>Development in the Park Residential Precinct</b>		
PO <sub>25</sub> Development does not create lots smaller than 2,250m <sup>2</sup> within the Park Residential Precinct.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development is not located in the Park Residential precinct.
PO <sub>26</sub> Effluent generated by a development is capable of being treated and disposed of on site.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development is not located in the Park Residential precinct.

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**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Height of Buildings and Other Structures</b>		
<p>PO<sub>1</sub> The height of buildings or other structures does not affect the operational efficiency of the Toowoomba Airport or Oakey Army Aviation Centre or create a hazard to the safe navigation of aircraft using the airport.</p>	<p>AO<sub>1.1</sub> Buildings and other structures in areas adjacent to the Toowoomba Airport do not penetrate the Airport Obstacle Limitation Surfaces shown on the Airport Environs Overlay maps.</p> <p>AO<sub>1.2</sub> Buildings or other structures (including private forestry plantations) in areas adjacent to the Oakey Army Aviation Centre shown on the Airport Environs Overlay maps do not exceed the following heights:            (a) Area A – 0m;            (b) Area B – 7.5m;            (c) Area C – 15m;            (d) Area D – 45m; and            (e) Area F – 90m.</p> <p>AO<sub>1.3</sub> Any cranes or other equipment used during the construction do not exceed the heights set out in AO<sub>1.1</sub> or AO<sub>1.2</sub>.</p>	<p><b>N/A:</b> The proposed development is not in proximity to the Toowoomba Airport.</p> <p><b>N/A:</b> The proposed development is not in proximity to the Oakey Army Aviation Centre.</p> <p><b>N/A:</b> The proposed development does not involve new building work.</p>
<b>Acoustic Amenity</b>		
<p>PO<sub>2</sub> Development is appropriately located and designed to prevent adverse impacts from aircraft noise in accordance with Australian Standards AS2021 – Acoustics – Aircraft Noise Intrusion – Building Siting and Construction.</p>	<p>AO<sub>2.1</sub> Premises are not developed for:            (a) dwelling house, multiple dwelling, dual occupancy, retirement facility, tourist park, hospital, health care services, relocatable home park, community residence, child care centre, educational establishment, residential care facility, resort complex, rooming accommodation or rural worker's accommodation on land within the 25 or higher ANEF contour;            (b) short term accommodation, community use, hotel, place of worship, rooming</p>	<p><b>N/A:</b> The proposed development is not located in the ANEF Contour.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>accommodation or theatre, on land within the 30 or higher ANEF contour;</p> <p>(c) business activities or commercial uses on land within the 35 or higher ANEF contour.</p>	
<p>PO<sub>3</sub> Development for:</p> <p>(a) caretaker's accommodation, community residence, dwelling house, dwelling unit, multiple dwelling, dual occupancy, child care centre, retirement facility, tourist park, educational establishment, hospital, health care services, rooming accommodation, relocatable home park, residential care facility, resort complex or rural worker's accommodation on land situated between the 20 and 25 ANEF contours;</p> <p>(b) short term accommodation, club, community uses, hotel, place of worship or theatre, on land situated between the 20 and 30 ANEF contours; or</p> <p>(c) business activities or commercial uses on land situated between the 20 and 35 ANEF contours;</p> <p>incorporates effective noise attenuation measures which meet Australian Standard AS2021 - Acoustics - Aircraft Noise Intrusion – Building Siting and Construction and Australian Standard AS2107 – Acoustics – Recommended design sound levels and reverberation times for building interiors.</p>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The proposed development is not located in the ANEF Contour.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Lighting and Emission Hazards</b>		
PO <sub>4</sub> The development of premises does not cause an obstruction or other potential hazard to aircraft movement associated with the airports by way of: <ul style="list-style-type: none"> <li>(a) introducing reflective surfaces, very bright lighting, or lighting similar to aerodrome lighting, which can distract or confuse aircraft pilots;</li> <li>(b) interfering with navigation or communication facilities; or</li> <li>(c) emissions that may effect pilot visibility or aircraft operations.</li> </ul>	AO <sub>4.1</sub> Street lighting and outdoor lighting within the airport dangerous light area shown on the Airport Environs Overlay maps does not involve: <ul style="list-style-type: none"> <li>(a) lighting that shines light above the horizontal;</li> <li>(b) coloured or flashing lights;</li> <li>(c) sodium lights; or</li> <li>(d) flare plumes.</li> </ul> AO <sub>4.2</sub> Street lighting and outdoor lighting within the airport dangerous light area shown on the Airport Environs Overlay maps does not involve configurations of lights in straight parallel lines 500m to 1,000m long.  AO <sub>4.3</sub> Roofs of buildings within the airport dangerous light area shown on Airport Environs Overlay maps are of a non-reflective colour or material.	<b>N/A:</b> The proposed development is not located within the Airport Dangerous Light Area.  <b>N/A:</b> The proposed development is not located within the Airport Dangerous Light Area.  <b>N/A:</b> The proposed development is not located within the Airport Dangerous Light Area.
PO <sub>5</sub> Development and operational activities do not generate emissions within the airport obstacle limitation surface shown on the Airport Environs Overlay maps that may affect pilot visibility or aircraft operation by way of: <ul style="list-style-type: none"> <li>(a) gas plumes;</li> <li>(b) particulate emissions (e.g. dust or smoke); or</li> <li>(c) electromagnetic field radiations.</li> </ul>	In partial fulfilment of the performance outcome:  AO <sub>5.1</sub> Any cleared vegetation is mulched or removed from the site and not burnt on-site.  AO <sub>5.2</sub> Any gas plumes do not have peak vertical velocities of more than 4.3m/sec and/or oxygen content of less than 50% of ambient levels.	<b>N/A:</b> The proposed development does not involve clearing of vegetation.  <b>N/A:</b> The proposed development does not involve activities that will generate gas plumes.
<b>Development within the Public Safety Area</b>		
PO <sub>6</sub> Development does not introduce or intensify uses within the public safety area shown on the Airport Environs Overlay maps which are likely to increase risks to public safety.	AO <sub>6.1</sub> Premises within the Public Safety Area are not developed for:	<b>N/A:</b> The proposed development is not located in the Public Safety Area.

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(a) accommodation activities;</li> <li>(b) uses that attract large numbers of people (e.g. business, community or industrial activities involving large numbers of workers or customers such as shopping centres, child care centres, health care services, major sport, recreation and entertainment facility, place of worship or club);</li> <li>(c) institutional uses (e.g. educational establishment, hospital or detention facility);</li> <li>(d) uses involving the manufacture or depot storage of hazardous materials (e.g. fuel, lubricants and other flammable materials, chemical, explosives, and poisonous or noxious gases); and</li> <li>(e) utility installations being transport terminals.</li> </ul>	
<b>Potential Wildlife Hazards</b>		
<p>PO<sub>7</sub> The development of premises does not cause an obstruction or other potential hazard to aircraft movement associated with the airport by way of attracting wildlife to the area which could cause or contribute to bird-strike hazard.</p>	<p>AO<sub>7.1</sub> The following uses are not located within the 3km Wildlife Hazard Buffer Zone shown on the Airport Environs Overlay maps:</p> <ul style="list-style-type: none"> <li>(a) animal keeping (being a bird sanctuary or fauna reserve);</li> <li>(b) aquaculture;</li> <li>(c) cropping (being fruit cropping or turf farming);</li> <li>(d) intensive animal industries (being a piggery or feedlot);</li> <li>(e) special industries (being an abattoir or food processing plant); and</li> <li>(f) commercial fish processing.</li> </ul>	<p><b>N/A:</b> The proposed development is not located in the 3km Wildlife Hazard Buffer Zone.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO7.2 The following uses are located within the 3km Wildlife Hazard Buffer Zone shown on the Airport Environs Overlay maps only where all food/waste holding areas and receptacles are contained and covered:</p> <ul style="list-style-type: none"> <li>(a) animal keeping (being a riding school, the keeping, handling and racing of horses or stables);</li> <li>(b) community use (being show grounds);</li> <li>(c) outdoor sport and recreation (being a drive in theatre); and</li> <li>(d) food and drink outlet (being a drive-through facility).</li> </ul> <p>AO7.3 The following uses are located between the 3km and 8km Wildlife Hazard Buffer Zones shown on the Airport Environs Overlay maps only where all food/waste holding areas and receptacles are contained and covered:</p> <ul style="list-style-type: none"> <li>(a) animal keeping (being a bird sanctuary or fauna reserve);</li> <li>(b) aquaculture;</li> <li>(c) cropping (being fruit cropping or turf farming);</li> <li>(d) intensive animal industries (being a piggery or feedlot);</li> <li>(e) special industries (being an abattoir or food processing plant); and</li> <li>(f) commercial fish processing.</li> </ul> <p>AO7.4 Utility installations (being waste facilities that involve the disposal of putrescible waste) only establish within the 13km Wildlife Hazard Buffer zone shown on the Airport Environs Overlay Maps.</p>	<p><b>N/A:</b> The proposed development is not located in the 3km Wildlife Hazard Buffer Zone.</p> <p><b>N/A:</b> The proposed development is not located in the 3km or 8km Wildlife Hazard Buffer Zone.</p> <p><b>N/A:</b> The proposed development does not involve a utility installation.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	AO <sub>7.5</sub> Landscaping does not include species that at maturity would intersect the Airport Obstacle Limitation Surfaces shown on the Airport Environs Overlay maps.	<b>N/A:</b> The proposed development does not involve provision of landscaping.
<b>Transient Aviation Activities</b>		
PO <sub>8</sub> Development does not adversely impact on the operational airspace of the Toowoomba Airport or Oakey Army Aviation Centre.	AO <sub>8.1</sub> Development that includes activities involving transient intrusions such as parachuting, hot-air ballooning or hang gliding are not located within the operational airspace shown on the Airport Environs Overlay maps.	<b>N/A:</b> The proposed development does not involve activities with transient intrusions.

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Where a Dual Occupancy</b>		
<p>PO<sub>1</sub> Dual Occupancy development is dispersed within residential neighbourhoods, not clustering in groups of more than three adjoining or opposite sites, and does not become the dominant form of housing within a street by occupying more than 20 percent of properties within a street block.</p>	<p>AO<sub>1.1</sub> In Greenfield Areas Dual Occupancy development occurs on designated Dual Occupancy lots.</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>
<p>PO<sub>2</sub> The development is designed in a manner that responds to the sites opportunities and constraints, including any existing building on the lot, and to the broader urban and streetscape contexts.</p> <p>Note: Development should be designed and documented by a suitably qualified professional (i.e. architect or building designer). Details of professional qualifications should be noted on drawings submitted with any development application</p>	<p>AO<sub>2.1</sub> The design of the development considers and responds to:</p> <p>(a) Site specific qualities including:</p> <ul style="list-style-type: none"> <li>(i) site topography and slope (contours at a minimum of 1m intervals);</li> <li>(ii) Views</li> <li>(iii) solar aspect and access;</li> <li>(iv) prevailing breezes</li> <li>(v) existing buildings on the site;</li> <li>(vi) site access</li> <li>(vii) services</li> <li>(viii) identification of buildings to be demolished;</li> <li>(ix) existing vegetation (locations to be surveyed and the diameter of the trunk at 1m above ground level shown. On highly buttressed trees the extent of buttressed roots is to be shown);</li> <li>(x) proposed position of new buildings overlaid</li> </ul> <p>(b) Nearby features including:</p> <ul style="list-style-type: none"> <li>(i) opportunities for on street visitor car parking;</li> <li>(ii) buildings on adjoining land</li> </ul>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(iii) important views from neighbouring properties</li> <li>(iv) features and character of adjoining land and buildings including doors &amp; windows</li> <li>(v) street fixtures;</li> <li>(c) Local qualities and amenities including:                             <ul style="list-style-type: none"> <li>(i) available public transport;</li> <li>(ii) nearby amenities such as parks, shops</li> </ul> </li> </ul> <p>AO2.2 Site design retains all existing street trees.</p> <p>Note: See Figure 1 below for examples of site analysis and nearby feature plans.</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>

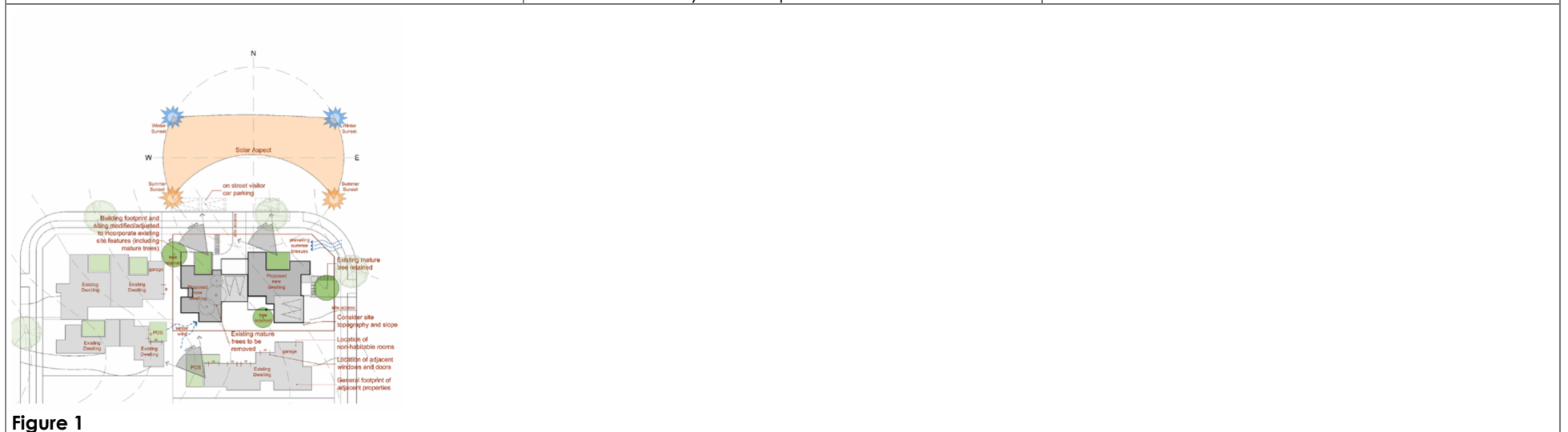


Figure 1

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>3</sub> Development responds to the slope of the land in the siting, design and form of buildings and structures to minimise requirements for cut and fill.</p>	<p>AO<sub>3.1</sub> Cut and fill beyond natural ground level does not exceed a vertical distance of 1m, unless contained within building walls and in accordance with the requirements outlined in SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.</p> <p>Note: See Figure 2.</p> <p>AO<sub>3.2</sub> Batters and earth-retaining structures are constructed in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p> <p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>

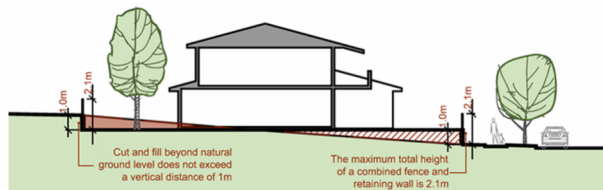


Figure 2

<p>PO<sub>4</sub> Development is designed to facilitate the retention of any existing street tree adjacent to the site.</p>	<p>AO<sub>4.1</sub> Development does not compromise or require the removal of any street tree.</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>
<p>PO<sub>5</sub> Development:</p> <ul style="list-style-type: none"> <li>(a) provides for the on-site infiltration of stormwater and minimises the additional burden on drainage infrastructure;</li> <li>(b) minimises the visual impact of hard surface areas; and</li> </ul>	<p>AO<sub>5.1</sub> The impervious site cover does not exceed Table 1:</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary																
<p>(c) respects the existing or preferred neighbourhood character and responds to the features of the site.</p>	<table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr> <th colspan="2">Table 1</th> </tr> <tr> <th>Average building height</th> <th>Maximum impervious site cover</th> </tr> </thead> <tbody> <tr> <td>If both of the proposed dwellings are 1 storey</td> <td>70%</td> </tr> <tr> <td>If one or both of the proposed dwellings is 2 storeys</td> <td>65%</td> </tr> </tbody> </table> <p>AO<sub>5.2</sub> No more than 50% of the front setbacks area is impervious.</p> <p>AO<sub>5.3</sub> Site cover does not exceed rates specified in Table 2.</p> <table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr> <th colspan="2">Table 2</th> </tr> <tr> <th>Dwelling Height</th> <th>Maximum site cover</th> </tr> </thead> <tbody> <tr> <td>If both of proposed dwellings are 1 storey</td> <td>50%</td> </tr> <tr> <td>If one of proposed dwellings is 2 storeys</td> <td>45%</td> </tr> </tbody> </table> <p>Note: See Figure 3.</p>	Table 1		Average building height	Maximum impervious site cover	If both of the proposed dwellings are 1 storey	70%	If one or both of the proposed dwellings is 2 storeys	65%	Table 2		Dwelling Height	Maximum site cover	If both of proposed dwellings are 1 storey	50%	If one of proposed dwellings is 2 storeys	45%	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>
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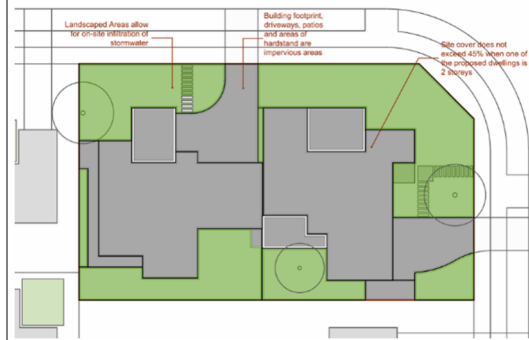


Figure 3

Toowoomba Regional Planning Scheme 2012 (v28.0)

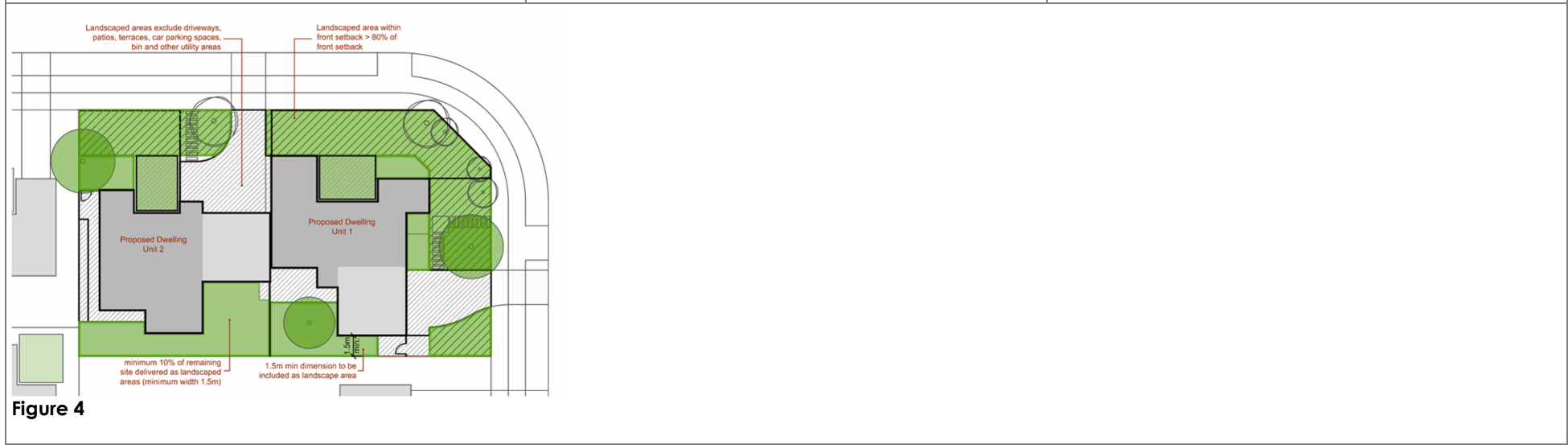
Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary																	
<p>PO<sub>6</sub> Areas of hardstand required for parking and pathways:</p> <ul style="list-style-type: none"> <li>(a) are minimised,</li> <li>(b) incorporate generous landscaping:               <ul style="list-style-type: none"> <li>(i) to promote a garden streetscape character, and</li> <li>(ii) to minimise visual impacts of views to hardstand areas for occupants and the public, particularly in the front garden.</li> </ul> </li> </ul>	<p>AO<sub>6.1</sub> Landscaped areas are provided in accordance with Table 3:</p> <table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr> <th colspan="3">Table 3</th> </tr> <tr> <th rowspan="2">Building height</th> <th colspan="2">Minimum landscaped area</th> </tr> <tr> <th>Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:</th> <th>Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:</th> </tr> </thead> <tbody> <tr> <td>1-2 storeys</td> <td>10% site area</td> <td>15% site area</td> </tr> <tr> <td>3 storeys</td> <td>12.5% site area</td> <td>17.5% site area</td> </tr> <tr> <td>4+ storeys</td> <td>15% site area</td> <td>20% site area</td> </tr> </tbody> </table> <p>Note: Landscaped areas included in the calculations consist of garden beds and lawn areas throughout the site with a width of no less than 1.5m;</p>	Table 3			Building height	Minimum landscaped area		Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:	Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:	1-2 storeys	10% site area	15% site area	3 storeys	12.5% site area	17.5% site area	4+ storeys	15% site area	20% site area	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	Excludes all paved and other impervious areas such as driveways, patios, terraces, car parking spaces and bin and other utility areas. See Figure 4.	



<p>PO7 The front building setback is consistent with the prevailing front setbacks of other residential buildings in the street.</p> <p>Note: The prevailing setback of the street is determined by the setbacks of the residential buildings on any lot along the road within 100m of the site.</p>	<p>AO7.1 Buildings are setback from the road frontage:</p> <ul style="list-style-type: none"> <li>(a) within 10% of the average front setback of existing residential buildings within 100m of the site; or</li> <li>(b) where there is no adjoining buildings the front setback is provided in accordance with Table 4.</li> </ul>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary										
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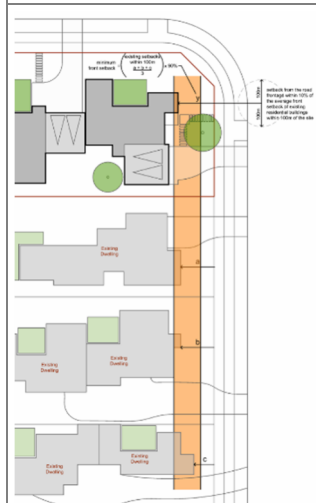


Figure 5



Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(b) is not further reduced where existing solar access is less than that specified in (a).</p> <p>Note: See Figure 6.</p>	



Figure 6

<p>PO<sub>9</sub> Development provides adequate and suitable private open space to meet the needs of occupants for privacy, relaxation and entertainment and to accommodate visitors.</p>	<p>AO<sub>9.1</sub> Private open space has a minimum area of 16m<sup>2</sup> and minimum dimensions of 4 metres by 4 metres.</p> <p>AO<sub>9.2</sub> Private open space is:</p> <ul style="list-style-type: none"> <li>(a) is oriented to receive at least three hours of sun between 9am and 5pm on 21 June.</li> <li>(b) directly accessible from a living area or kitchen;</li> <li>(c) fenced or landscaped to achieve privacy for occupants;</li> <li>(d) clear of bins, clothes lines, hot water systems, air conditioning units, above</li> </ul>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>ground water tanks and associated infrastructure and other forms of ancillary equipment and (e) not located within the front setback area.</p> <p>Note: See Figure 7.</p>	

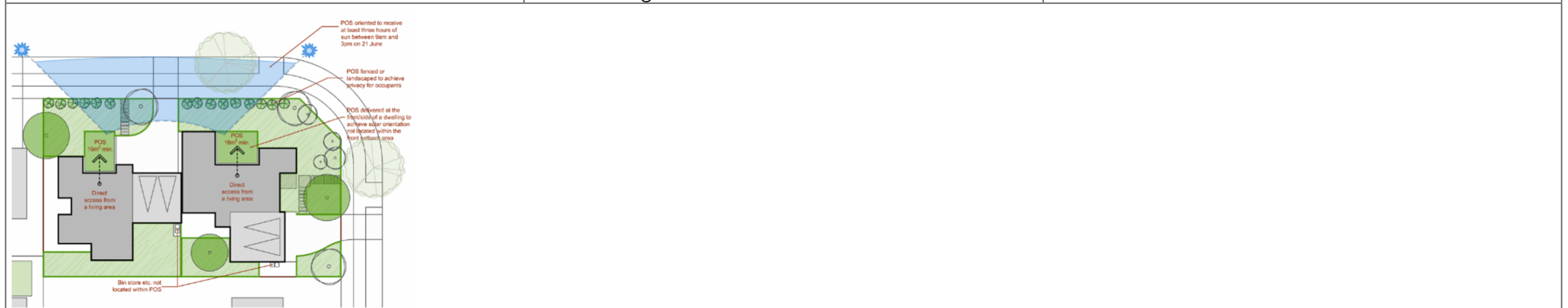


Figure 7

<p>PO<sub>10</sub> All car parking is designed to accommodate a standard car (B85 vehicle) and incorporates adequate manoeuvring area to parking spaces (including carports and garages).</p>	<p>AO<sub>10.1</sub> Design of the site layout must ensure a B85 vehicle (as defined in AS 2890.1:2004 Parking facilities - Off-street car parking) is able to enter an enclosed garage or visitor car park in a single forward manoeuvre and exit in a single reverse manoeuvre.</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>
<p>PO<sub>11</sub> Development provides resident and visitor car parking which is integrated into the site and building design and: (a) does not negatively impact the amenity of the site, adjoining sites or streetscape;</p>	<p>AO<sub>11.1</sub> Parking is only provided within the front setback area where it is: (a) located on the driveway directly in front of a carport or garage; and (b) not covered.</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(b) is not a prominent visual feature from the street or other public spaces; and</p> <p>(c) contributes to the establishment of a garden setting by avoiding large areas of visible hardstand.</p>	<p>Note: Where car parking is provided in accordance with AO11.1(a) the driveway should have a minimum length of 5 metres to avoid parked vehicles extending beyond the site boundary.</p> <p>AO11.2 Where car parking is provided below ground:</p> <p>(a) Access ramps are not located within 6m of the street boundary and do not exceed an opening width of 6m; and</p> <p>(b) Entry to the site for pedestrians is provided separate from vehicle entries by way of a safe, well lit and clearly identifiable pathway or gatehouse;</p> <p>AO11.3 All car parking is freely available for use by to the applicable occupants and visitors of the development.</p> <p>Note: See Figure 8.</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>

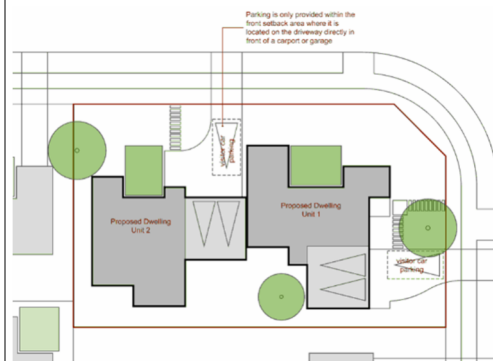


Figure 8

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>12</sub> Garages and carports (except those with frontage to a rear lane) do not dominate the street frontage.</p>	<p>AO<sub>12.1</sub> Garages and/or carports facing a street make up no more than 30% of the width of the site.            AO<sub>12.2</sub> Garages and/or carports facing a street are setback a minimum of 1 metre behind the main face of the dwelling.            Note: See Figures 9 and 10.</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>

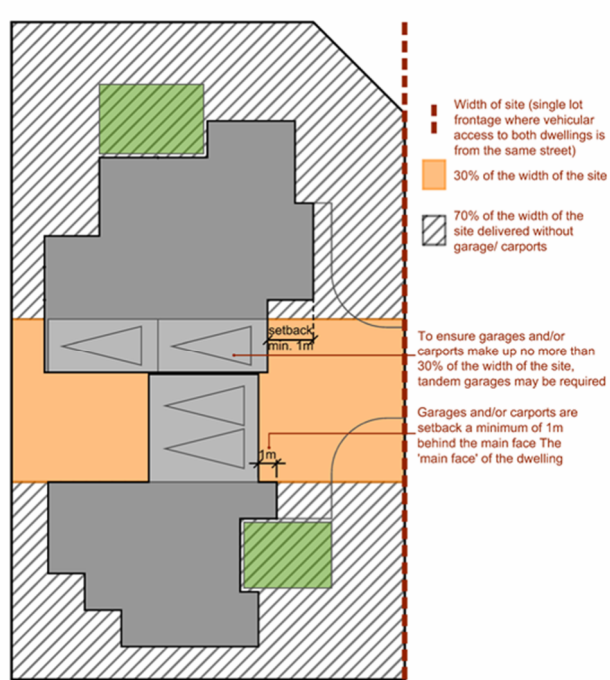


Figure 9

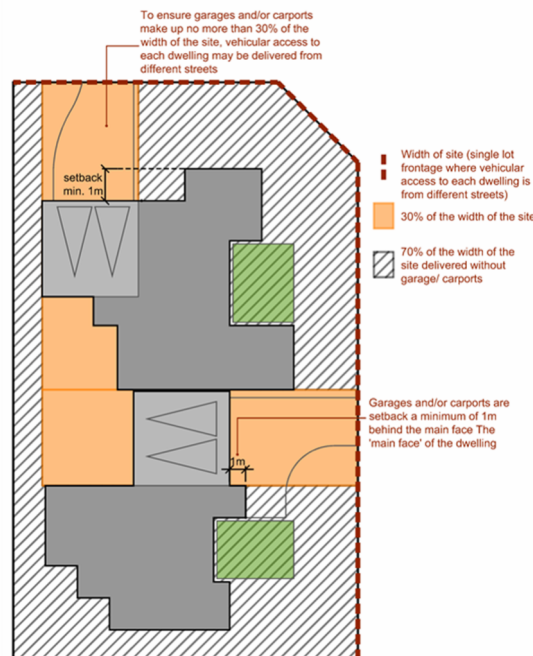


Figure 10

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO13 Safe and equitable access for both pedestrians and vehicles is provided, to and from buildings and the street whilst minimising the extent and number of driveways.</p>	<p>AO13.1 Where vehicular access to both dwellings is from the same street the driveway is shared and has a maximum crossover width of 5m.</p> <p>AO13.2 Where vehicular access to each dwelling is from different streets the driveway has a maximum crossover width of 3m.</p> <p>AO13.3 Where the site abuts a lane vehicle access is provided from the lane.</p> <p>AO13.4 Separate pedestrian access is provided to each dwelling, clearly distinguishable from the vehicle zone.</p> <p>Note: See Figure 11.</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>

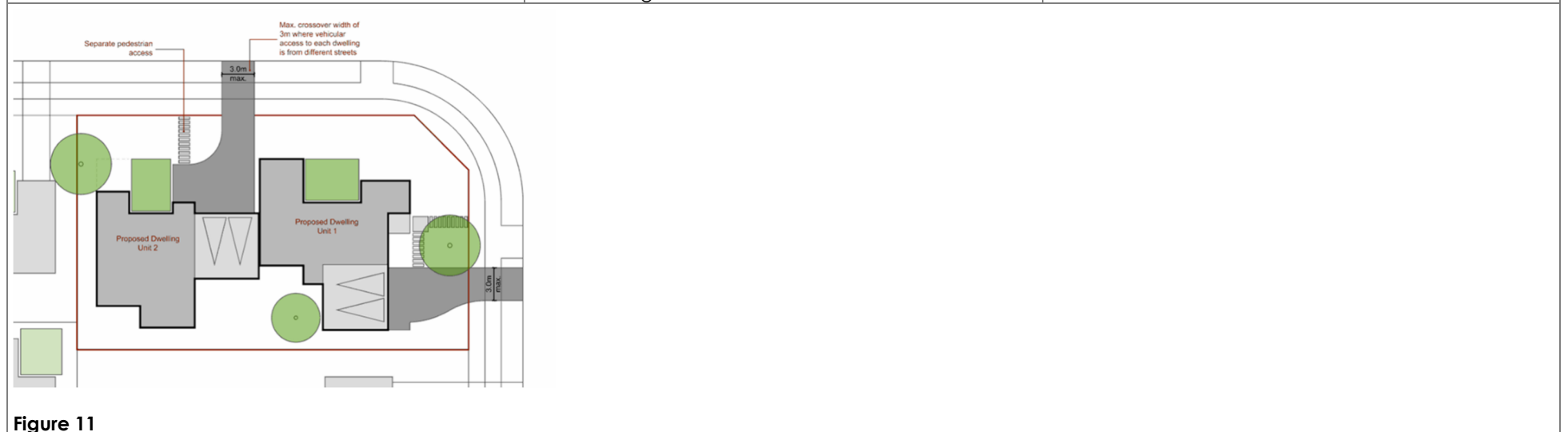


Figure 11

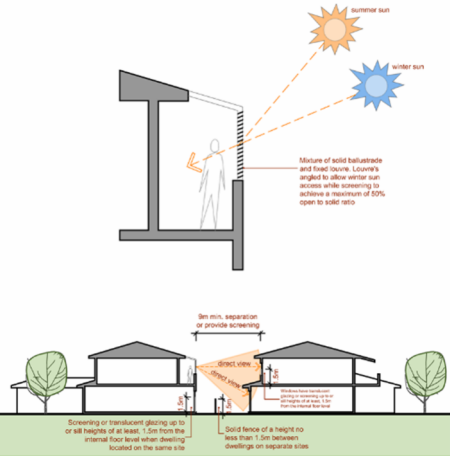
**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>14</sub> The privacy of residents of the development or adjoining premises is protected when they are inside a dwelling or in a private open space area.</p>	<p>AO<sub>14.1</sub> Habitable room windows of a dwelling are separated a minimum of 9m from a habitable room window or private open space of another dwelling on the same or an adjoining site.</p> <p>OR</p> <p>AO<sub>14.2</sub> Where there is a direct view within 9m into a habitable room or private open space of another dwelling on the same or an adjoining site</p> <p>(a) the elements with the view such as windows, balconies, or terraces of a dwelling must be screened in accordance with AO<sub>14.3</sub>; or</p> <p>(b) the Windows have translucent glazing up to or sill heights of at least 1.5m from the internal floor level; or</p> <p>(c) a solid fence or masonry wall with a minimum height of 1.5m is positioned between the elements with the view.</p> <p>AO<sub>14.3</sub> Where screening is used, it:</p> <p>(a) is a solid translucent screen or a louvre perforated panel, trellis or the like that has a maximum of 50% open to solid ratio; and</p> <p>(b) is a fixed, permanent element of a design that complements the style, detailing and materials of the development.</p> <p>Note: See Figure 12.</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>

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Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
 <p><b>Figure 12</b></p>	<p>PO<sub>15</sub> The development is designed so that occupants are not adversely affected by adjoining non-residential uses.</p> <p>AO<sub>15.1</sub> A 1.8m high solid screen fence and 2m wide vegetated buffer is provided along any section of the site boundary that is common with vehicle movement and/or parking areas associated with a non-residential use on an adjoining site.</p>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>
<p>PO<sub>16</sub> Buildings are designed to a high aesthetic standard providing:</p> <ul style="list-style-type: none"> <li>(a) prominent features towards the street frontage;</li> <li>(b) detailing and finishes appropriate to the scale, quality and character of the street and to help differentiate between dwellings</li> </ul>	<p>AO<sub>16.1</sub> Buildings:</p> <ul style="list-style-type: none"> <li>(a) are oriented to and address the street frontage;</li> <li>(b) have front entries that provide full weather protection and where within 10m of any street, have clearly defined front entries that are visible from the street;</li> </ul>	<p><b>N/A:</b> The proposed development does not involve a dual occupancy.</p>

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Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(c) opportunities for informal surveillance of the street and other public spaces;</p> <p>(d) clear and visible entries; and elements that complement or enhance the character of the local neighbourhood.</p>	<p>(c) where within 10m of any street; have one or more habitable rooms facing the street boundary that have windows or doors with a minimum area of clear glazing of 0.25m<sup>2</sup> for each linear metre of street frontage; and</p> <p>(d) are designed to deliver depth in the façade facing the street or public space with a variation not less than 2m deep for a minimum 50% of the building width at ground level.</p> <p>Note: Variation in building façade may be achieved through delivery of balconies, patios or building footprint articulation. The roofline over a balcony or patio may remain consistent with the dominant roof form.</p> <p>Note: See Figure 13.</p>	

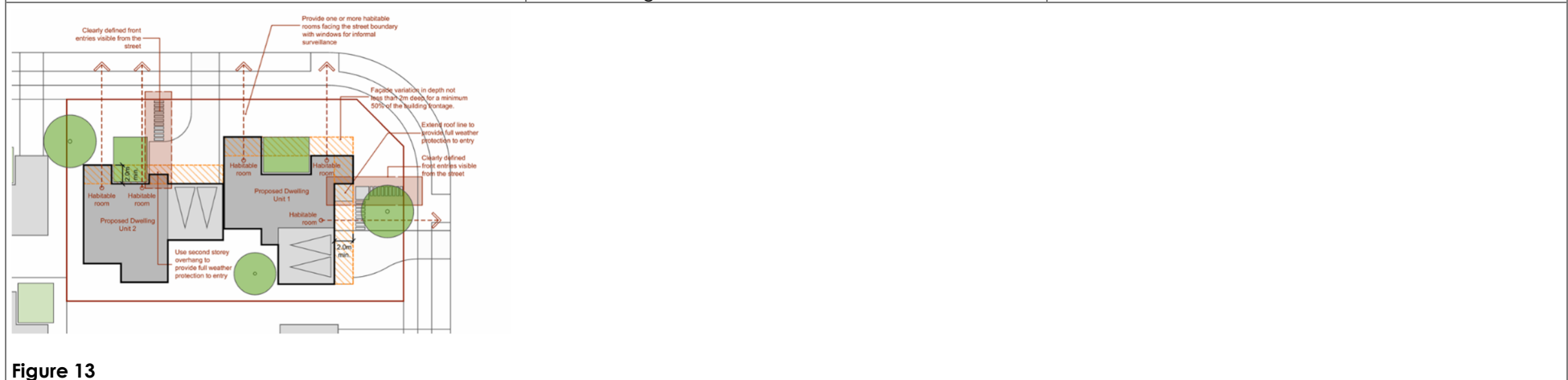


Figure 13



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Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>18</sub> Development provides landscaping that contributes to the landscape character, shade and the micro-climate of the neighbourhood and site through the retention or provision of at least one canopy tree.	AO <sub>18.1</sub> One (1) tree capable of achieving a height of at least six (6) metres and canopy spread of at least five (5) metres at maturity is provided.	<b>N/A:</b> The proposed development does not involve a dual occupancy.
PO <sub>19</sub> The treatment of fencing and retaining walls in all situations reinforces the high quality, amenity and safety of the development.	<p>AO<sub>19.1</sub> The maximum total height of a combined fence and retaining wall is 2.1m, as measured from the base of the retaining wall to the top of the fence.</p> <p>AO<sub>19.2</sub> Where a combined fence and retaining wall is located on the front boundary, the fence has a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>AO<sub>19.3</sub> Retaining walls and fences are constructed of high quality materials and contribute to residential amenity.</p> <p>AO<sub>19.4</sub> Any fence on the front boundary has a minimum 30% transparency when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>Note: Where retaining walls are set off the property boundary by a minimum of 600mm, they are not considered to be combined with the fencing. (See Figure 15.)</p>	<b>N/A:</b> The proposed development does not involve a dual occupancy.

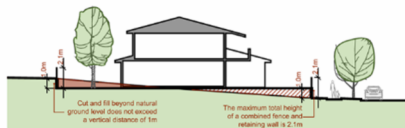


Figure 15

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**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>20</sub> Front fencing contributes to privacy, screening, security, and street character while maintaining a visual relationship between dwellings and public spaces.	AO <sub>20.1</sub> Fences located on a front property boundary or a common boundary with an adjacent park or within the front setback area: (a) have a maximum height of 1.2 metres; or (b) have a maximum height of 1.5 metres and a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).  Note: In instances where the fence is located in conjunction with a retaining wall refer PO <sub>19</sub> .	<b>N/A:</b> The proposed development does not involve a dual occupancy.
PO <sub>21</sub> Stormwater discharge from the site is to a lawful point of discharge as defined in the Queensland Urban Drainage Manual (QUDM), without the use of pumped or charged pipe systems, and not to private land other than to an easement for stormwater purposes befitting the site and allowing discharge to a lawful point of discharge in land over which Council has tenure or control.	AO <sub>21.1</sub> The site: (a) has a natural surface with an elevation that is higher than the abutting road and enables stormwater to drain gravitationally to the abutting road via subsurface pipes; or (b) has a natural surface with an elevation that is higher than Council's stormwater drainage network in the abutting road and enables stormwater to drain gravitationally to the stormwater drainage network in the abutting road via subsurface pipes; or (c) has lawful access to an inter allotment drainage network with available capacity to meet the requirements of the development.	<b>N/A:</b> The proposed development does not involve a dual occupancy.
<b>Where a Dwelling House on a Hatchet Lot</b>		
PO <sub>22</sub> The configuration and finish of the access strip/easement: (a) enhances the streetscape qualities of the area and any adjoining premises;	AO <sub>22.1</sub> Any area of hardstand, including the driveway, within the access strip / easement: (a) has a maximum width of 3m; and (b) is sealed with a rigid pavement.	<b>N/A:</b> The proposed development does not involve a dwelling house on a hatchet lot.

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>(b) enhances the residential amenity of the area; and</p> <p>(c) contributes to a high quality residential outcome for the site, by the incorporation of landscaping, provision for adequate setbacks and use of high quality materials.</p>	<p>AO<sub>22.2</sub> The access strip/easement has a landscaped area/s with a combined minimum width of 2m for its full length.</p> <p>AO<sub>22.3</sub> The driveway landscaping must continue for the full length of the driveway for a minimum width of 0.5m.</p> <p>AO<sub>22.4</sub> Any fence on the inside boundary of the access strip / easement has a maximum height of one (1) metre for the first six (6) metres from the front boundary.</p>	<p><b>N/A:</b> The proposed development does not involve a dwelling house on a hatchet lot.</p> <p><b>N/A:</b> The proposed development does not involve a dwelling house on a hatchet lot.</p> <p><b>N/A:</b> The proposed development does not involve a dwelling house on a hatchet lot.</p>
<p>PO<sub>23</sub> The dwelling does not result in a reduction of amenity, privacy, or loss of solar access for any adjoining property.</p>	<p>AO<sub>23.1</sub> The dwelling does not exceed a maximum height of 2 storeys or 8.5m above ground level.</p> <p>AO<sub>23.2</sub> The dwelling is setback a minimum of 2m from all boundaries.</p> <p>AO<sub>23.3</sub> North-facing windows to habitable rooms and/or the private outdoor space of an adjoining premises does not have sunlight reduced to less than 3 hours of solar gain between 9 a.m. and 3 p.m. on 21 June.</p>	<p><b>N/A:</b> The proposed development does not involve a dwelling house on a hatchet lot.</p>
<p>PO<sub>24</sub> Stormwater discharge from the site is to a lawful point of discharge as defined in the Queensland Urban Drainage Manual (QUDM), without the use of pumped or charged pipe systems, and not to private land other than to an easement for stormwater purposes befitting the site and allowing discharge to a lawful point of discharge in land over which Council has tenure or control.</p> <p>Note: Land over which Council has tenure or control does not include Council's open space network.</p>	<p>AO<sub>24.1</sub> The site:</p> <p>(a) has a natural surface with an elevation that is higher than the abutting road and enables stormwater to drain gravitationally to the abutting road via subsurface pipes; or</p> <p>(b) has a natural surface with an elevation that is higher than Council's stormwater drainage network in the abutting road and enables stormwater to drain gravitationally to the stormwater drainage network in the abutting road via subsurface pipes; or</p> <p>(c) has lawful access to an inter allotment drainage network with available capacity</p>	<p><b>N/A:</b> The proposed development does not involve a dwelling house on a hatchet lot.</p>

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**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

<b>Performance outcomes</b>	<b>Acceptable outcomes</b>	<b>Compliance summary</b>
	to meet the requirements of the development.	

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**Medium Density Residential Code – Assessment benchmarks for assessable development**

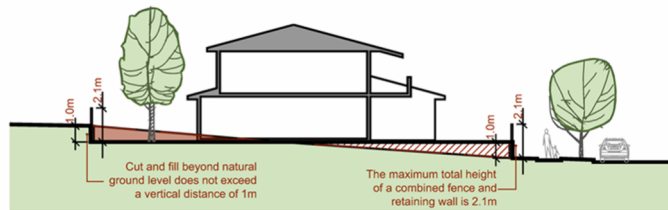
Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>1</sub> The development is designed in a manner that responds to the sites opportunities and constraints and to the broader urban and streetscape contexts.</p> <p>Note: Development should be designed and documented by a suitably qualified professional (i.e. architect or building designer). Details of professional qualifications should be noted on drawings submitted with any development application</p>	<p>AO<sub>1.1</sub> The design of the development considers and responds to:</p> <p>(a) Site specific qualities including:</p> <ul style="list-style-type: none"> <li>(i) site topography and slope (contours at a minimum of 1m intervals);</li> <li>(ii) Views</li> <li>(iii) solar aspect and access;</li> <li>(iv) prevailing breezes</li> <li>(v) existing buildings on the site;</li> <li>(vi) site access</li> <li>(vii) services</li> <li>(viii) identification of buildings to be demolished;</li> <li>(ix) existing vegetation (locations to be surveyed and the diameter of the trunk at 1m above ground level shown. On highly buttressed trees the extent of buttressed roots is to be shown);</li> <li>(x) proposed position of new buildings overlaid</li> </ul> <p>(b) Nearby features including:</p> <ul style="list-style-type: none"> <li>(i) opportunities for on street visitor car parking;</li> <li>(ii) buildings on adjoining land</li> <li>(iii) important views from neighbouring properties</li> <li>(iv) features and character of adjoining land and buildings including doors &amp; windows</li> <li>(v) street fixtures;</li> </ul> <p>(c) Local qualities and amenities including:</p> <ul style="list-style-type: none"> <li>(i) available public transport; and</li> </ul>	<p><b>Complies:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve new building work. The proposed development is of a scale and nature compatible and consistent with the surrounding residential area. The proposed development involves on-site carparking sufficient for the nature of the use and existing service connections. The proposed development is located in proximity to parks, schools and other services.</p>



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**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure. The extent of cut and fill must be shown on the proposed site plan and elevations.</p> <p>AO<sub>2.2</sub> Batters and earth-retaining structures are constructed in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.</p> <p>Note: See Figure 3.</p>	<p><b>N/A:</b> The proposed development does not involve batters and earth-retaining structures.</p>



**Figure 3**

<p>PO<sub>3</sub> Development is designed to facilitate the retention of any existing street tree adjacent to the site.</p>	<p>AO<sub>3.1</sub> Development does not compromise or require the removal of any street tree.</p>	<p><b>Complies:</b> The proposed development does not involve the removal of any street tree.</p>										
<p>PO<sub>4</sub> Development:</p> <p>(a) provides for the on-site infiltration of stormwater and minimises the additional burden on drainage infrastructure;</p> <p>(b) minimises the visual impact of hard surface areas; and</p>	<p>AO<sub>4.1</sub> The impervious site cover does not exceed Table 1:</p> <table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr> <th colspan="2">Table 1</th> </tr> <tr> <th>Average building height</th> <th>Maximum impervious site cover</th> </tr> </thead> <tbody> <tr> <td>If more than 50% of the proposed dwellings are 1 storey</td> <td>70%</td> </tr> <tr> <td>If more than 50% of the proposed dwellings are 2 storeys</td> <td>65%</td> </tr> <tr> <td>If more than 50% of the proposed dwellings are 3 or more storeys</td> <td>60%</td> </tr> </tbody> </table>	Table 1		Average building height	Maximum impervious site cover	If more than 50% of the proposed dwellings are 1 storey	70%	If more than 50% of the proposed dwellings are 2 storeys	65%	If more than 50% of the proposed dwellings are 3 or more storeys	60%	<p><b>N/A:</b> The proposed development involves short term accommodation in an existing dwelling and does not involve new building work.</p>
Table 1												
Average building height	Maximum impervious site cover											
If more than 50% of the proposed dwellings are 1 storey	70%											
If more than 50% of the proposed dwellings are 2 storeys	65%											
If more than 50% of the proposed dwellings are 3 or more storeys	60%											

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary										
respects the existing or preferred neighbourhood character and responds to the features of the site.	<p>AO<sub>4.2</sub> No more than 50% of the front setbacks area is impervious.</p> <p>AO<sub>4.3</sub> Site cover does not exceed rates specified in Table 2.</p> <table border="1" data-bbox="1003 608 1223 810"> <caption>Table 2</caption> <thead> <tr> <th>Dwelling Height</th> <th>Maximum site cover</th> </tr> </thead> <tbody> <tr> <td>If more than 50% of proposed dwellings are 1 storey</td> <td>50%</td> </tr> <tr> <td>If more than 50% of proposed dwellings are 2 storeys</td> <td>45%</td> </tr> <tr> <td>If more than 50% of proposed dwellings are 3 storeys</td> <td>40%</td> </tr> <tr> <td>If more than 50% of proposed dwellings are 4 storeys or more</td> <td>35%</td> </tr> </tbody> </table> <p>Note: See Figure 4.</p>	Dwelling Height	Maximum site cover	If more than 50% of proposed dwellings are 1 storey	50%	If more than 50% of proposed dwellings are 2 storeys	45%	If more than 50% of proposed dwellings are 3 storeys	40%	If more than 50% of proposed dwellings are 4 storeys or more	35%	<b>N/A:</b> The proposed development involves short term accommodation in an existing dwelling and does not involve new building work.
Dwelling Height	Maximum site cover											
If more than 50% of proposed dwellings are 1 storey	50%											
If more than 50% of proposed dwellings are 2 storeys	45%											
If more than 50% of proposed dwellings are 3 storeys	40%											
If more than 50% of proposed dwellings are 4 storeys or more	35%											

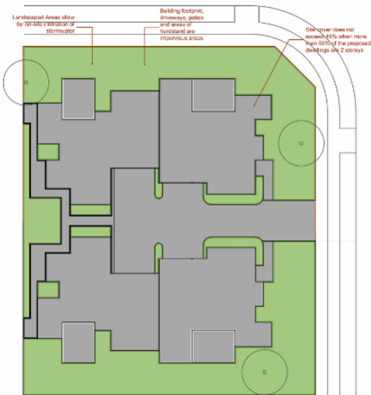


Figure 4

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary																	
<p>PO<sub>5</sub> Areas of hardstand required for parking and pathways:</p> <ul style="list-style-type: none"> <li>(a) are minimised,</li> <li>(b) Incorporate generous landscaping:               <ul style="list-style-type: none"> <li>(i) to promote a garden streetscape character, and</li> <li>(ii) (to minimise visual impacts of views to hardstand areas for occupants and the public, particularly in the front garden.</li> </ul> </li> </ul>	<p>AO<sub>5.1</sub> Landscaped areas are provided in accordance with Table 3:</p> <table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr> <th colspan="3">Table 3</th> </tr> <tr> <th rowspan="2">Building height</th> <th colspan="2">Minimum landscaped area</th> </tr> <tr> <th>Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:</th> <th>Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:</th> </tr> </thead> <tbody> <tr> <td>1-2 storeys</td> <td>10% site area</td> <td>15% site area</td> </tr> <tr> <td>3 storeys</td> <td>12.5% site area</td> <td>17.5% site area</td> </tr> <tr> <td>4+ storeys</td> <td>15% site area</td> <td>20% site area</td> </tr> </tbody> </table> <p>Note: Landscaped areas included in the calculations consist of garden beds and lawn areas</p>	Table 3			Building height	Minimum landscaped area		Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:	Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:	1-2 storeys	10% site area	15% site area	3 storeys	12.5% site area	17.5% site area	4+ storeys	15% site area	20% site area	<p><b>N/A:</b> The proposed development does not involve the provision of landscaping.</p>
Table 3																			
Building height	Minimum landscaped area																		
	Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:	Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:																	
1-2 storeys	10% site area	15% site area																	
3 storeys	12.5% site area	17.5% site area																	
4+ storeys	15% site area	20% site area																	

Toowoomba Regional Planning Scheme 2012 (v28.0)

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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>throughout the site with a width of no less than 1.5m;</p> <p>Excludes all paved and other impervious areas such as driveways, patios, terraces, car parking spaces and bin and other utility areas.</p> <p>See Figure 5.</p>	

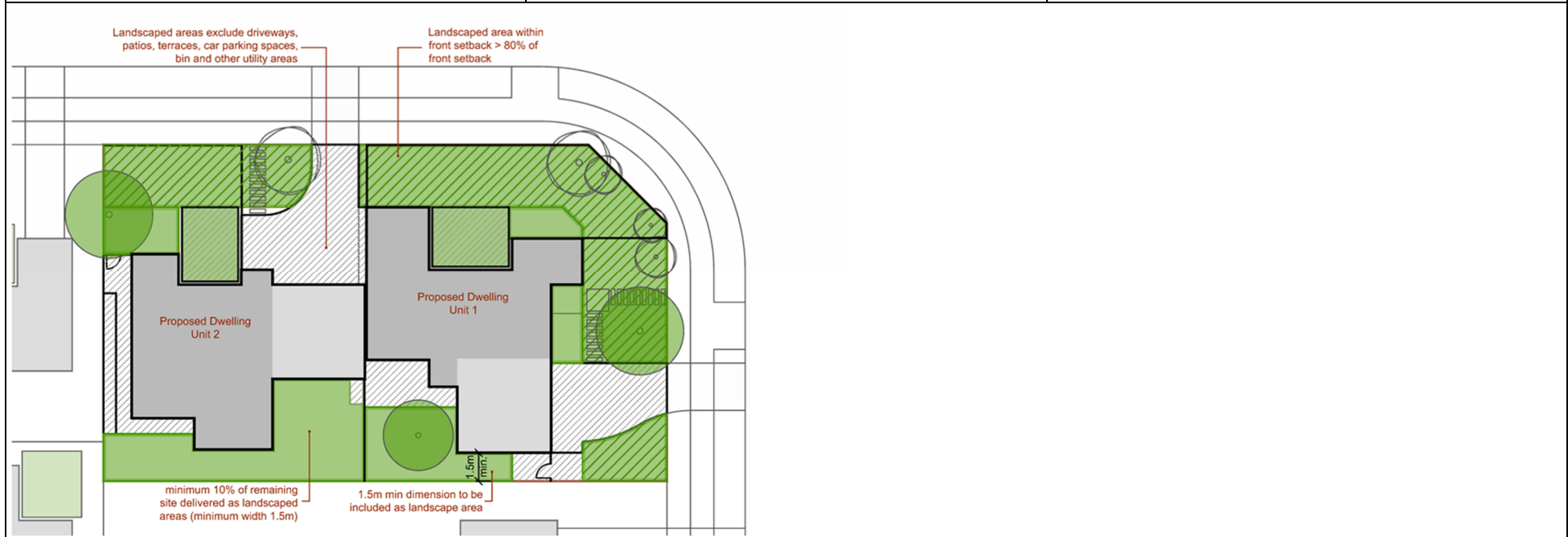


Figure 5

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary										
<p>PO<sub>6</sub> The front building setback is consistent with the prevailing front setbacks of other residential buildings in the street.</p> <p>Note: The prevailing setback of the street is determined by the setbacks of the residential buildings on any lot along the road within 100m of the site.</p>	<p>AO<sub>6.1</sub> Buildings are setback from the road frontage:</p> <p>(a) within 10% of the average front setback of existing residential buildings within 100m of the site; or</p> <p>(b) where there is no adjoining buildings the front setback is provided in accordance with Table 4.</p> <table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr> <th colspan="2" style="text-align: center;">Table 4</th> </tr> <tr> <th style="text-align: left;">Building element</th> <th style="text-align: left;">Minimum setback</th> </tr> </thead> <tbody> <tr> <td>Solid building wall (other than a garage)</td> <td style="text-align: center;">4.0m</td> </tr> <tr> <td>Solid building wall (where for a garage)</td> <td style="text-align: center;">5.5m</td> </tr> <tr> <td>Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)</td> <td style="text-align: center;">3.5m (or 3m where secondary road frontage)</td> </tr> </tbody> </table> <p>Note: See Figure 6.</p>	Table 4		Building element	Minimum setback	Solid building wall (other than a garage)	4.0m	Solid building wall (where for a garage)	5.5m	Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)	3.5m (or 3m where secondary road frontage)	<p><b>N/A:</b> The proposed development does not involve new building work.</p>
Table 4												
Building element	Minimum setback											
Solid building wall (other than a garage)	4.0m											
Solid building wall (where for a garage)	5.5m											
Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)	3.5m (or 3m where secondary road frontage)											

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Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

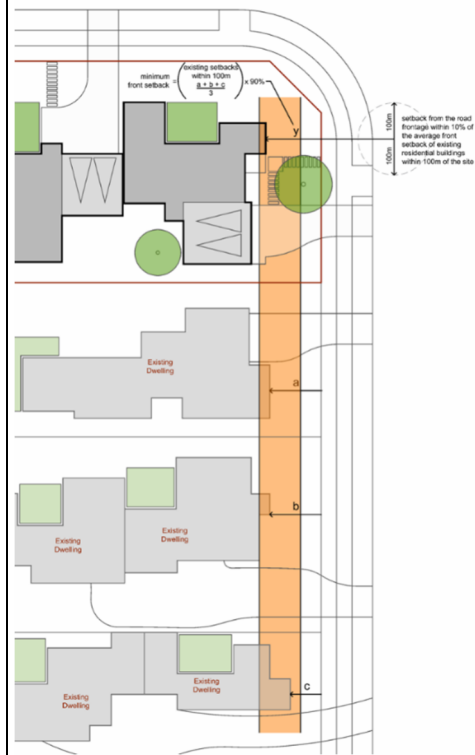


Figure 6





Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary																																				
	<p style="text-align: center;">Table 5</p> <table border="1"> <thead> <tr> <th>No. of Dwellings</th> <th>Min. Communal Open Space per Dwelling</th> <th>Min. Ground Floor Private Open Space per Dwelling</th> <th>Min. Private Open Space above Ground Floor per Dwelling</th> </tr> </thead> <tbody> <tr> <td>1-10</td> <td>0m<sup>2</sup></td> <td>16m<sup>2</sup></td> <td>8m<sup>2</sup></td> </tr> <tr> <td>11-15</td> <td>20m<sup>2</sup></td> <td>16m<sup>2</sup></td> <td>8m<sup>2</sup></td> </tr> <tr> <td>16+</td> <td>25m<sup>2</sup></td> <td>16m<sup>2</sup></td> <td>8m<sup>2</sup></td> </tr> </tbody> </table> <p>AO<sub>8.2</sub> Communal and private open space has a minimum dimension in accordance with Table 6.</p> <p style="text-align: center;">Table 6</p> <table border="1"> <thead> <tr> <th>Type</th> <th>Area</th> <th>Minimum Dimension</th> </tr> </thead> <tbody> <tr> <td colspan="3" style="text-align: center;">Private Open Space</td> </tr> <tr> <td>Ground Floor</td> <td>16m<sup>2</sup></td> <td>4m*</td> </tr> <tr> <td>First Floor</td> <td>8m<sup>2</sup></td> <td>2.7m*</td> </tr> <tr> <td colspan="3" style="text-align: center;">Communal Open Space</td> </tr> <tr> <td rowspan="2" style="text-align: center;">All</td> <td style="text-align: center;">&lt;100m<sup>2</sup></td> <td style="text-align: center;">5m</td> </tr> <tr> <td style="text-align: center;">100m<sup>2</sup>+</td> <td style="text-align: center;">Length to breadth ratio of 2.5:1</td> </tr> </tbody> </table> <p>*Min length required for both length and width.</p> <p>AO<sub>8.3</sub> Private open space is:</p> <ul style="list-style-type: none"> <li>(a) is oriented to receive at least three hours of sun between 9am and 3pm on 21 June.</li> <li>(b) directly accessible from a living area or kitchen;</li> <li>(c) fenced or landscaped to achieve privacy for occupants;</li> </ul>	No. of Dwellings	Min. Communal Open Space per Dwelling	Min. Ground Floor Private Open Space per Dwelling	Min. Private Open Space above Ground Floor per Dwelling	1-10	0m <sup>2</sup>	16m <sup>2</sup>	8m <sup>2</sup>	11-15	20m <sup>2</sup>	16m <sup>2</sup>	8m <sup>2</sup>	16+	25m <sup>2</sup>	16m <sup>2</sup>	8m <sup>2</sup>	Type	Area	Minimum Dimension	Private Open Space			Ground Floor	16m <sup>2</sup>	4m*	First Floor	8m <sup>2</sup>	2.7m*	Communal Open Space			All	<100m <sup>2</sup>	5m	100m <sup>2</sup> +	Length to breadth ratio of 2.5:1	<p><b>Complies:</b> The proposed development involves existing outdoor living spaces which comply with the minimum dimensions and area.</p> <p><b>Complies:</b> The proposed development does not involve new dwellings or private open space. The proposed development involves short term accommodation within an existing dwelling that involves outdoor living spaces, directly accessible from living areas within the dwelling.</p>
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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(d) clear of bins, clothes lines, hot water systems, air conditioning units, above ground water tanks and associated infrastructure and other forms of ancillary equipment; and</p> <p>(e) not located within the front setback area.</p> <p>Note: See Figure 8.</p>	

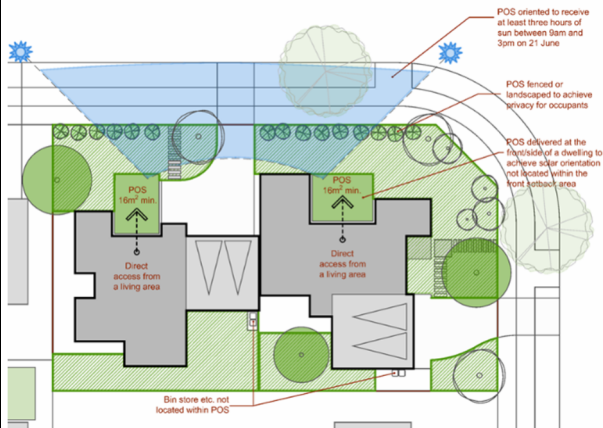


Figure 8

<p>PO<sub>9</sub> All car parking is designed to accommodate a standard car (B85 vehicle for occupant car parking and B99 vehicle for visitor car parking) and:</p> <p>(a) is clearly identifiable and of adequate dimensions;</p>	<p>AO<sub>9.1</sub> Design of the site layout must ensure a B85 vehicle (as defined in AS 2890.1:2004 Parking facilities - Off-street car parking) is able to enter an enclosed garage or visitor car park in a single forward manoeuvre and exit in a single reverse manoeuvre prior to departing the site in a forward direction.</p>	<p><b>N/A:</b> The proposed development has been designed to accommodate two (2) parking spaces for guests within the existing carport. The proposed development does not involve visitor parking.</p>
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Performance outcomes

- (b) incorporates adequate manoeuvring area to parking spaces (including carports and garages); and
- (c) incorporates a clear, safe and effective circulation system.

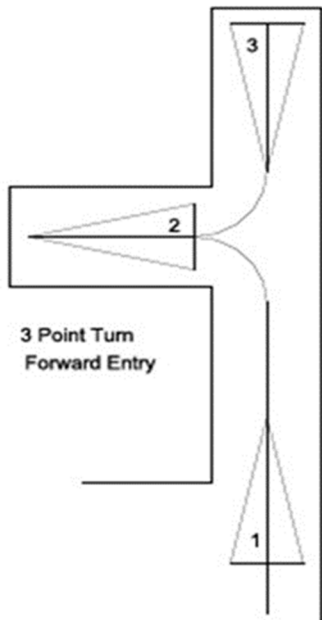


Figure 9

Acceptable outcomes

AO<sub>9.2</sub> Design of the site layout must ensure a B99 vehicle (as defined in AS 2890.1:2004 Parking facilities - Off-street car parking) is able to enter a visitor car park in a single forward manoeuvre and exit in a single reverse manoeuvre prior to departing the site in a forward direction.

Note: See Figure 9.

AO<sub>9.3</sub> Each parking space has minimum dimensions as per Table 7:

Table 7	
Parking Space	Minimum Dimensions
Single uncovered parking space (with at least 400mm clearance between either side of the parking space and the nearest wall.)	5.4 metres long by 2.6 metres wide
Single uncovered parking space (with less than 400mm clearance between either side of the parking space and the nearest wall.)	5.4 metres long by 3 metres wide
Single carport	5.4 metres long by 3 metres wide
Double carport	5.4 metres long by 5.8 metres wide
Single garage	6.4 metres long by 3 metres wide internally
Double garage	6.4 metres by 6.4 metres wide internally

Compliance summary

**N/A:** The proposed development does not involve visitor car parks.

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<p>PO<sub>10</sub> Development provides resident and visitor car parking which is integrated into the site and building design and:</p> <ul style="list-style-type: none"> <li>(a) Does not negatively impact the amenity of the site, adjoining sites or streetscape;</li> <li>(b) Is not a prominent visual feature from the street or other public spaces; and</li> <li>(c) Contributes to the establishment of a garden setting by avoiding large areas of visible hardstand.</li> </ul>	<p>AO<sub>10.1</sub> Parking is only provided within the front setback area where it is:</p> <ul style="list-style-type: none"> <li>(a) uncovered and located on a driveway directly in front of a carport or garage; or</li> <li>(b) the front setback is a least 6m, and</li> <li>(c) the car park surface is permeable;</li> <li>(d) a minimum 2m wide densely planted landscape buffer is provided between the car park and the front boundary for the full length of the car park;</li> <li>(e) the location of the car park does not compromise the ability of the dwelling to provide casual surveillance of the public realm; and</li> <li>(f) at least 60% of the front setback area is landscaped area.</li> </ul> <p>AO<sub>10.2</sub> Where car parking is provided below ground:</p> <ul style="list-style-type: none"> <li>(a) Access ramps are not located within 6m of the street boundary and do not exceed an opening width of 6m; and</li> <li>(b) Entry to the site for pedestrians is provided separate from vehicle entries by way of a safe, well lit and clearly identifiable pathway or gatehouse;</li> </ul> <p>AO<sub>10.3</sub> All car parking:</p> <ul style="list-style-type: none"> <li>(a) is clearly signposted; and</li> <li>(b) is freely available for use by to the applicable occupants and visitors of the development.</li> </ul> <p>Note: See Figures 10 and 11.</p>	<p><b>Performance Solution:</b> The proposed development involves guest parking within existing car accommodation and an existing sealed driveway. The proposed development does not involve new building work or changes to the existing on-site infrastructure. The proposed development involves parking arrangements consistent and compatible with the surrounding residential area.</p> <p><b>N/A:</b> The proposed development does not involve below ground car parking.</p> <p><b>Complies:</b> The proposed development involves on-site carparking that will be freely available for use by occupants and visitors of the development.</p>

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Performance outcomes

Acceptable outcomes

Compliance summary

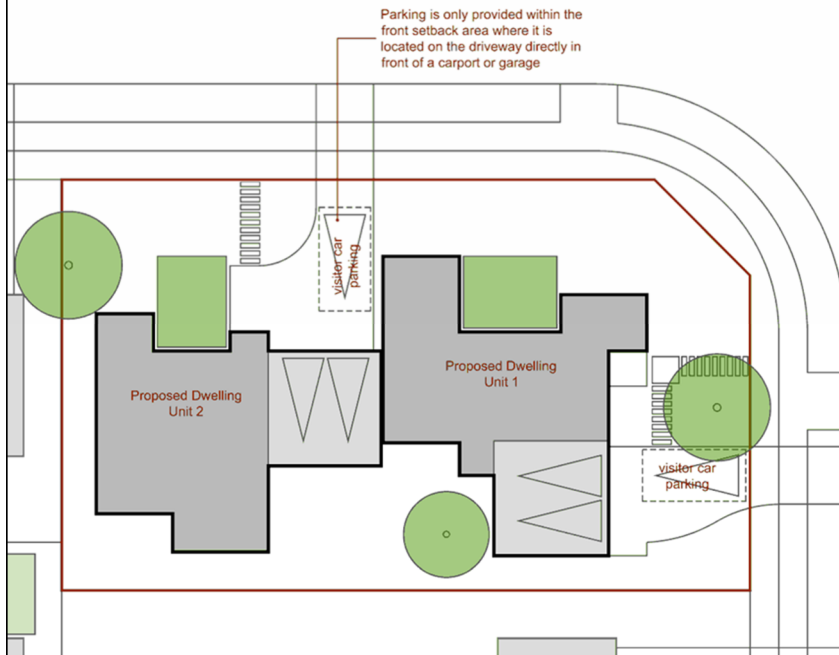


Figure 10

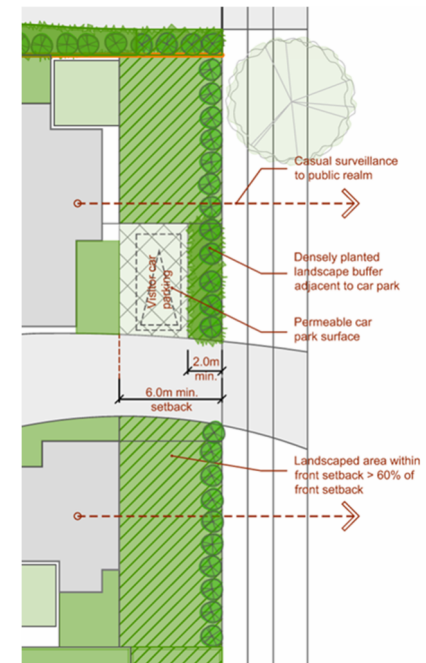


Figure 11

PO<sub>11</sub> Safe and equitable access for both pedestrians and vehicles is provided, to and from buildings and the street whilst minimising the extent and number of driveways.

In partial fulfilment of the performance outcome:

AO<sub>11.1</sub> Vehicular access from the street is provided via a single shared driveway with a maximum crossover width of 5m.

AO<sub>11.2</sub> Where the site abuts a lane vehicle access is provided from the lane.

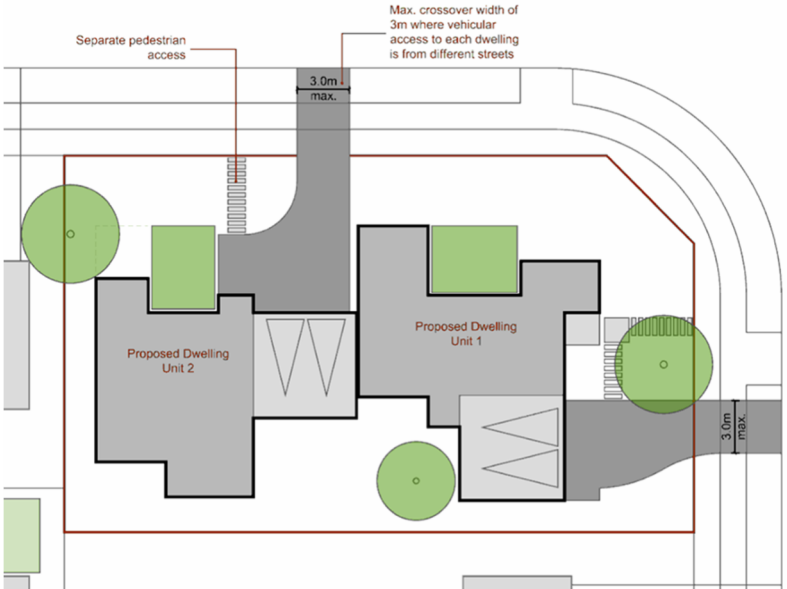
Note: See Figure 12.

**Complies:** The proposed development involves existing vehicular access via a single shared driveway. The proposed development does not involve changes to the existing vehicular access arrangements.

**N/A:** The proposed development does not have access to a lane.

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Performance outcomes	Acceptable outcomes	Compliance summary
 <p>Figure 12</p>		
<p>PO<sub>12</sub> The privacy of residents of the development or adjoining premises is protected when they are inside a dwelling or in a private open space area.</p>	<p>AO<sub>12.1</sub> Habitable room windows of a dwelling are separated a minimum of 9m from a habitable room window or private open space of another dwelling on the same or an adjoining site.</p> <p>OR</p> <p>AO<sub>12.2</sub> Where there is a direct view within 9m into a habitable room or private open space of</p>	<p><b>N/A:</b> The proposed development involves reuse of an existing dwelling for short term accommodation and does not involve new dwellings.</p>

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	<p>another dwelling on the same or an adjoining site:</p> <ul style="list-style-type: none"> <li>(a) The elements with the view such as windows, balconies, or terraces of a dwelling must be screened in accordance with AO13.3; or</li> <li>(b) The Windows have translucent glazing up to or sill heights of at least, 1.5m from the internal floor level; or</li> <li>(c) A solid fence or masonry wall with a minimum height of 1.5m is positioned between the elements with the view.</li> </ul> <p>AND</p> <p>AO<sub>12.3</sub> Where screening is used, it:</p> <ul style="list-style-type: none"> <li>(a) is a solid translucent screen or a louvre perforated panel, trellis or the like that has a maximum of 50% open to solid ratio; and</li> <li>(b) is a fixed, permanent element of a design that complements the style, detailing and materials of the development.</li> </ul> <p>Note: See Figure 13.</p>	

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<p>Mixture of solid balustrade and fixed louvre. Louvre's angled to allow winter sun access while screening to achieve a maximum of 50% open to solid ratio</p> <p>9m min. separation or provide screening</p> <p>direct view</p> <p>direct view</p> <p>Screening or translucent glazing up to or sill heights of at least, 1.5m from the internal floor level when dwelling located on the same site</p> <p>Solid fence of a height no less than 1.5m between dwellings on separate sites</p>		
<p><b>Figure 13</b></p> <p>PO<sub>13</sub> The development is designed so that occupants are not adversely affected by adjoining non-residential uses.</p>	<p>In partial fulfilment of the performance outcome</p> <p>AO<sub>13.1</sub> A 1.8m high solid screen fence and 2m wide vegetated buffer is provided along any section of the site boundary that is common with vehicle movement and/or parking areas associated with a non-residential use on an adjoining site.</p>	<p><b>N/A:</b> The proposed development does not adjoin a site with a non-residential use.</p>

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<p>PO<sub>14</sub> Noise from the development does not unreasonably impact on existing or likely future dwellings on nearby land.</p>	<p>AO<sub>14.1</sub> For developments of six (6) dwellings or more, vehicle movement areas are located a minimum of 3m from any adjoining dwellings.</p> <p>AO<sub>14.2</sub> Swimming pools, BBQs, dining areas and other recreational facilities located within communal open areas are located a minimum of 3m from adjoining dwellings.</p> <p>OR</p> <p>AO<sub>14.3</sub> Provide acoustic screening in accordance with an acoustic engineer's design where vehicle movement or recreational areas are located closer than 3m to any adjoining dwellings.</p> <p>Note: A minimum 1.5m vegetation buffer will be required with any acoustic fence to provide amenity. (See Figures 14 and 15.)</p>	<p><b>N/A:</b> The proposed development does not involve six or more dwellings.</p> <p><b>N/A:</b> The proposed development does not involve communal open space areas.</p>

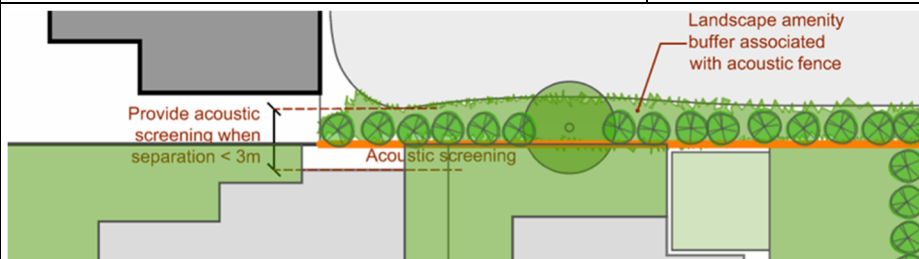


Figure 14

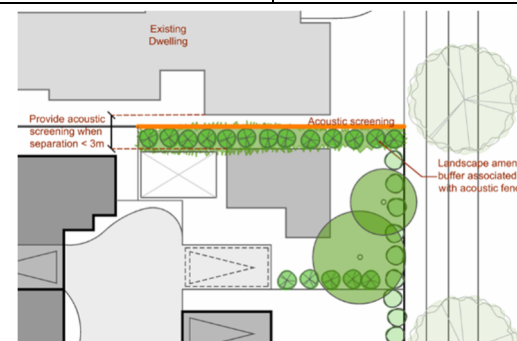


Figure 15

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO15 Where communal open space is provided, it:</p> <ul style="list-style-type: none"> <li>(a) is positioned on site to enjoy a good solar orientation;</li> <li>(b) includes landscaping and screening for privacy to provide amenity;</li> <li>(c) is easily accessible and conveniently located for the use of all occupants;</li> <li>(d) is useable and safe;</li> <li>(e) is designed and located to minimise internal and external impacts on the amenity of residents and neighbouring premises; and</li> <li>(f) contributes positively to the streetscape where located in a frontage area.</li> </ul>	<p>AO15.1 Communal open space is oriented to receive at least three hours of sun between 9am and 5pm on 21 June.</p> <p>AO15.2 Communal open space provided at the ground level, includes a balance of soft and hard landscape features with:</p> <ul style="list-style-type: none"> <li>(a) a minimum 50% open to the sky, and</li> <li>(b) a minimum 25% shaded by trees within 5 years.</li> </ul> <p>AO 15.3 The location and design of communal open space ensures that it is:</p> <ul style="list-style-type: none"> <li>(a) able to be accessed by all residents from a common area within the site;</li> <li>(b) subject to casual surveillance; and</li> <li>(c) separated from private areas of the site.</li> </ul> <p>AO15.4 Communal open spaces may include indoor recreation facilities.</p> <p>Note: Communal open spaces do not contain:</p> <ul style="list-style-type: none"> <li>(a) vehicle driveways, parking, manoeuvring or other associated hardstand areas; or</li> <li>(b) structures such as rainwater tanks, bin storage areas, air conditioning plant, clothes hoists, etc. (See Figure 16.)</li> </ul>	<p><b>N/A:</b> The proposed development does not involve communal open space.</p>

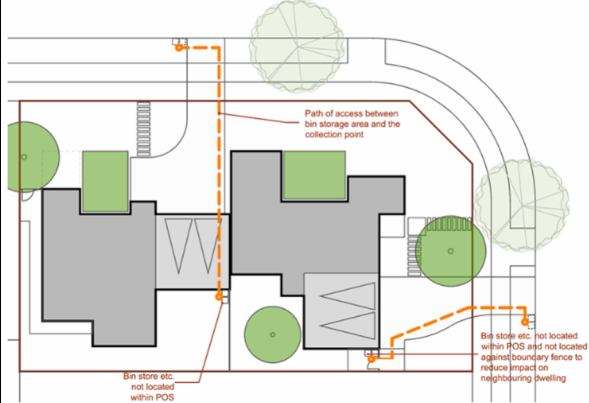
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<p>Figure 16</p>	<p>PO<sub>16</sub> Bin storage and access to the collection point is provided in a manner which is convenient, hygienic and will not result in an amenity impact on any use onsite or adjoining the premises.</p>	<p>AO<sub>16.1</sub> Bin storage is:</p> <ul style="list-style-type: none"> <li>(a) not located within the minimum private open space area required for the development;</li> <li>(b) not located within 2m of any fresh air intake of any dwelling on the premises or adjoining premises; and</li> <li>(c) if in an area which is accessible by more than one dwelling, is clearly identified as the storage area for the relevant unit/dwelling.</li> </ul> <p>AO<sub>16.2</sub> A path of access is provided between any bin storage area and the collection point which:</p> <ul style="list-style-type: none"> <li>(a) is a minimum of 1.2m wide;</li> <li>(b) is clear of any obstruction (i.e. retaining wall, landscaping, hot water tank);</li> <li>(c) does not traverse through any habitable room (other than a garage);</li> </ul>
		<p><b>Complies:</b> The proposed development will not involve bin storage within private open space area or within 2m of adjoining dwellings.</p> <p><b>Complies:</b> The proposed development will retain existing kerbside collection arrangements, which involve a path of access from bin storage areas to the kerbside collection point which does not traverse through habitable rooms and will be clear of obstructions.</p>

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	<p>(d) provides an at grade path of access (i.e. no steps); and (e) is no longer than 30m. Note: See Figure 17.</p>	
 <p><b>Figure 17</b></p>		
<p>PO<sub>17</sub> Long, unvaried driveways to property boundaries are avoided and do not detract from the local streetscape.</p>	<p>AO<sub>17.1</sub> Driveways are setback from side boundaries a minimum of 1.5m for the first 6m and 750mm thereafter incorporating a continuous densely planted landscape strip between the edge of the driveway and side boundary.</p> <p>AO<sub>17.2</sub> Driveways that exceed 40 metres in length must incorporate a change of alignment of a minimum of 1m. Note: See Figure 18.</p>	<p><b>Complies:</b> The proposed development retains the existing driveway which does not adjoin a side boundary and is appropriately separated from adjoining dwellings.</p> <p><b>N/A:</b> The proposed development does not involve a driveway greater than 40 metres in length.</p>

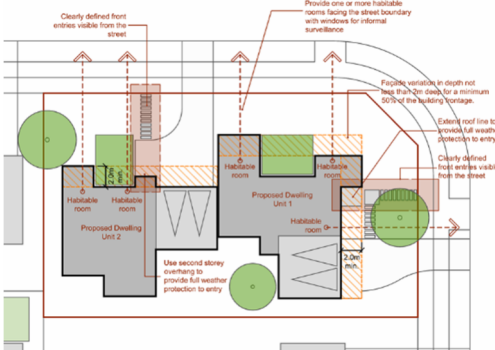
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<p><b>Figure 18</b></p> <p>PO<sub>18</sub> Buildings are designed to a high aesthetic standard providing:</p> <ul style="list-style-type: none"> <li>(a) prominent features towards the street frontage;</li> <li>(b) detailing and finishes appropriate to the scale, quality and character of the street and to help differentiate between dwellings;</li> <li>(c) opportunities for informal surveillance of the street and other public spaces;</li> <li>(d) clear and visible entries; and</li> <li>(e) elements that complement or enhance the character of the local neighbourhood.</li> </ul>	<p>In partial fulfilment of the performance outcome:</p> <p>AO<sub>18.1</sub> Buildings:</p> <ul style="list-style-type: none"> <li>(a) where at the front of the site, are oriented to and address the street;</li> <li>(b) have front entries that provide full weather protection and where within 10m of any street, have clearly defined front entries that are visible from the street;</li> <li>(c) where within 10m of any street; have one or more habitable rooms facing the street boundary that have windows or doors with a minimum area of clear glazing of 0.25m<sup>2</sup> for each linear metre of street frontage; and</li> <li>(d) are designed to deliver depth in the façade facing the street or public space with a variation not less than 2m deep for a minimum 50% of the building width at ground level.</li> </ul> <p>Note: See Figure 19.</p>	<p><b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve new building work.</p>

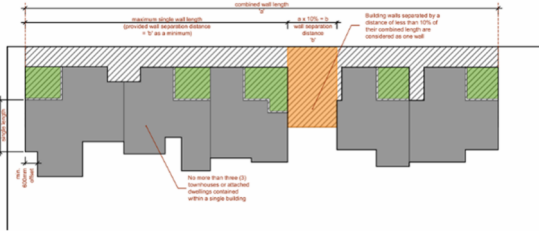
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Performance outcomes	Acceptable outcomes	Compliance summary
 <p><b>Figure 19</b></p>	<p>In partial fulfilment of the performance outcome</p> <p>AO<sub>19.1</sub> The maximum single wall length of a building is 32m with offsets of 600mm minimum provided every 7.5m;</p> <p>Note: Building walls separated by a distance of less than 10% of their combined length are considered as one wall. (See Figure 20.)</p> <p>AO<sub>19.2</sub> Building bulk is minimised through articulation and other measures, including the provision of at least one (1) element from at least three (3) of the following element groups:</p> <ul style="list-style-type: none"> <li>(a) verandahs, porches, pergolas or balconies;</li> <li>(b) roof overhangs;</li> <li>(c) window hoods/screens;</li> <li>(d) awning and shade structures; or</li> <li>(e) 600mm vertical gutter offset to create roof articulation.</li> </ul>	<p><b>N/A:</b> The proposed development does not involve new building work.</p> <p><b>N/A:</b> The proposed development does not involve new building work.</p>
<p>PO<sub>19</sub> Development incorporates typical domestic detailing to achieve a residential scale living environment with aesthetic standards and amenity for occupants similar to those of single dwellings in the neighbourhood.</p>		

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	<p>AO<sub>19.3</sub> Townhouses or attached dwellings have:</p> <ul style="list-style-type: none"> <li>(a) no more than three (3) dwellings contained within a single building;</li> <li>(b) a minimum of 4.5m separation between buildings.</li> </ul>	<p><b>N/A:</b> The proposed development does not involve townhouses or attached dwellings.</p>
 <p><b>Figure 20</b></p>		
<p>PO<sub>20</sub> Facades include a variety of colours, finishes and textures based on those found in houses within the surrounding streetscape and appropriate to the desired character of the area.</p>	<p>In partial fulfilment of the performance outcome:</p> <p>AO<sub>20.1</sub> Building facades incorporate:</p> <ul style="list-style-type: none"> <li>(a) external elements and treatments that are of a high quality finish, durable and need minimal maintenance;</li> <li>(b) at least three (3) distinct textures to finishes or surfaces not including roof, rainwater goods, glazing, joinery, doors, garage doors or soffits; and</li> <li>(c) colours and textures that are similar with the colours and textures of adjoining buildings.</li> </ul>	<p><b>N/A:</b> The proposed development does not involve new building work.</p>
<p>PO<sub>21</sub> Development provides landscaping that contributes to the landscape character, shade and the micro-climate of the neighbourhood and site through the retention of existing significant trees on the site.</p>	<p>In partial fulfilment of the performance outcome:</p> <p>AO<sub>21.1</sub> Landscape design plans for the subject site are prepared by a Registered Landscape Architect and lodged with the application for development approval.</p>	<p><b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling. The proposed development is of a scale and nature that does not warrant provision of formal landscaping plans.</p>



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	<p>Note: New street trees should be provided in accordance with Toowoomba Regional Council Street Tree Master Plan.</p>	
<p>PO<sub>22</sub> The treatment of fencing and retaining walls in all situations reinforces the high quality, amenity and safety of the development.</p> <p>Note: Details demonstrating compliance with the performance outcome should be included in the landscape design drawings submitted with any development application.</p>	<p>AO<sub>22.1</sub> The maximum total height of a combined fence and retaining wall is 2.1m, as measured from the finished surface level.</p> <p>AO<sub>22.2</sub> Where a combined fence and retaining wall is located on the front boundary, the fence has a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>AO<sub>22.3</sub> Retaining walls and fences are constructed of high quality materials and contribute to residential amenity.</p> <p>AO<sub>22.4</sub> Any fence on the front boundary has a minimum 30% transparency when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>Note: Where retaining walls are set off the property boundary by a minimum of 600mm, they are not considered to be combined with the fencing. (See Figure 21.)</p>	<p><b>N/A:</b> The proposed development involves existing fencing and retaining walls and does not involve provision of new fencing and retaining walls.</p> <p><b>N/A:</b> The proposed development involves existing fencing and retaining walls and does not involve provision of new fencing and retaining walls.</p> <p><b>N/A:</b> The proposed development involves existing fencing and retaining walls and does not involve provision of new fencing and retaining walls.</p> <p><b>N/A:</b> The proposed development involves existing fencing and retaining walls and does not involve provision of new fencing and retaining walls.</p>

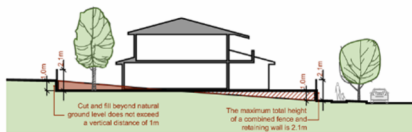


Figure 21

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>23</sub> Front fencing contributes to privacy, screening, security, and street character while maintaining a visual relationship between dwellings and public spaces.</p> <p>Note: Details demonstrating compliance with the performance outcome should be included in the landscape design drawings submitted with any development application.</p>	<p>AO<sub>23.1</sub> Fences located on a front property boundary or a common boundary with an adjacent park or within the front setback area:</p> <ul style="list-style-type: none"> <li>(a) have a maximum height of 1.2 metres; or</li> <li>(b) have a maximum height of 1.5 metres and a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).</li> </ul> <p>Note: In instances where the fence is located in conjunction with a retaining wall refer PO<sub>23</sub>.</p>	<p><b>N/A:</b> The proposed development involves existing fencing and retaining walls and does not involve provision of new fencing and retaining walls.</p>
<p>PO<sub>24</sub> Stormwater discharge from the site is to a lawful point of discharge as defined in the Queensland Urban Drainage Manual (QUDM), without the use of pumped or charged pipe systems, and not to private land other than to an easement for stormwater purposes befitting the site and allowing discharge to a lawful point of discharge in land over which Council has tenure or control.</p> <p>Note: Land over which Council has tenure or control does not include Council's open space network.</p>	<p>AO<sub>24.1</sub> The site:</p> <ul style="list-style-type: none"> <li>(a) has a natural surface with an elevation that is higher than the abutting road and enables stormwater to drain gravitationally to the abutting road via subsurface pipes; or</li> <li>(b) has a natural surface with an elevation that is higher than Council's stormwater drainage network in the abutting road and enables stormwater to drain gravitationally to the stormwater drainage network in the abutting road via subsurface pipes; or</li> <li>(c) has lawful access to an inter allotment drainage network with available capacity to meet the requirements of the development.</li> </ul>	<p><b>N/A:</b> The proposed development does not involve new building work.</p>
<p>PO<sub>25</sub> Where a Relocatable Home Park or Tourist Park the development site has an appropriate area and configuration to accommodate home or van sites, as well as adequate landscape and</p>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The proposed development does not involve a relocatable home park or tourist park.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
recreation areas, ancillary amenities and facilities, car parking and service areas.		

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Outdoor Lighting</b>		
<p>PO<sub>1</sub> Development does not unacceptably reduce the amenity and environmental quality of environs, especially of any nearby residential premises or public spaces as a result of light spill.</p>	<p>AO<sub>1.1</sub> No outdoor lighting is proposed as part of the development.</p> <p>OR</p> <p>AO<sub>1.2</sub> Technical parameters, design, installation, operation and maintenance of outdoor lighting comply with the requirements of Australian Standard 'AS4282-1997 control of the obtrusive effects of outdoor lighting'.</p> <p>AO<sub>1.3</sub> For sporting fields and sporting courts the technical parameters, design, installation, operation and maintenance comply with the requirements of Australian Standard AS4282-1997 – Control of the obtrusive Effects of Outdoor Lighting and a compliance statement by a lighting designer has been provided in accordance with the Australian Standard (Section 4).</p> <p>AO<sub>1.4</sub> Where light spillage outside of the property boundary is likely to result in levels above those mentioned in AO<sub>1.3</sub> the applicant has provided a lighting proposal and impact assessment (environmental and amenity) as part of the application which has demonstrated that the lighting will not create nuisance issues for surrounding sensitive receptors.</p> <p>AO<sub>1.5</sub> For private sporting courts the lighting system:</p> <p>(a) is baffled or shielded to ensure that a light source is not directly visible from a Habitable Room window of an adjoining Dwelling; and</p> <p>(b) the luminaire does not exceed a height of</p>	<p><b>Complies:</b> The proposed development involves only existing low-level outdoor security lighting.</p> <p><b>N/A:</b> The proposed development does not involve sporting fields or sporting courts.</p> <p><b>N/A:</b> The proposed development does not involve sporting fields or sporting courts.</p> <p><b>N/A:</b> The proposed development does not involve sporting courts.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p align="center">8m above the court surface.</p> <p>AO<sub>1.6</sub> The alignment of streets, driveways and servicing areas avoid vehicle headlight impacts on adjacent residential dwellings.</p>	<p><b>N/A:</b> The proposed development involves an existing driveway access.</p>
<p>PO<sub>2</sub> Outdoor lighting (excluding street lighting, normal residential lighting and low level security lighting) situated in excess of 4m above ground level does not jeopardise the safety or well-being of any pedestrian, cyclist or motorist. Light emissions do not reduce the ability of transport system users to see essential details of the route ahead, including signalling systems and signage.</p>	<p>AO<sub>2.1</sub> Outdoor lighting situated in excess of 4m above ground level is provided in accordance with Australian Standard AS1158.1.1:2005 – <i>Road Lighting – Vehicular Traffic (Category V) Lighting – Performance and Installation Design Requirements</i>.</p>	<p><b>N/A:</b> The proposed development does not involve outdoor lighting in excess of 4m above ground level.</p>
<p>PO<sub>3</sub> Outdoor Lighting does not cause unreasonable disturbance or cause detrimental impacts to any significant natural environment.</p>	<p>AO<sub>3.1</sub> The vertical illumination emanating from the outdoor lighting does not exceed one (1) lx on land within the Environmental Significance Overlay.</p>	<p><b>N/A:</b> The proposed development is not located in the Environmental Significance overlay.</p>
<p>PO<sub>4</sub> Proposed sensitive land uses adjoining existing lawful non-residential uses with significant lighting for community purposes, security or safety reasons are designed to proactively address possible obtrusive light nuisance.</p>	<p>AO<sub>4.1</sub> Proposed sensitive land uses adjoining existing lawful non-residential uses with significant lighting for community purposes, security or safety reasons are designed in a manner to mitigate any light nuisance impacts from the existing lawful use by establishing:</p> <ul style="list-style-type: none"> <li>(a) shielding or louvers on windows facing the light source;</li> <li>(b) orientating buildings and bedrooms so that external lighting does not impact on residents during night time hours; and</li> <li>(c) utilising earth embankments, landscaping or other physical measures to shield existing light sources.</li> </ul>	<p><b>N/A:</b> The proposed development is not adjoining land with lawful non-residential uses.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Odour</b>		
<p>PO<sub>5</sub> Development does not unreasonably affect the amenity and environmental quality of environs, especially of any nearby residential premises or public spaces due to odour impacts.</p>	<p>AO<sub>5.1</sub> The development does not involve activities that create odorous air emissions.</p> <p>OR</p> <p>AO<sub>5.2</sub> The development does not result in air emissions that exceed any of the acceptable levels specified within the <i>Environmental Protection (Air) Policy 2008</i>.</p> <p>AO<sub>5.3</sub> The development will not result in the release of noxious or offensive odours beyond the boundary of the site that cause environmental nuisance at any odour sensitive place, i.e. sufficient buffering is available within the development site itself to dissipate odour issues.</p> <p><i>Note: An Odour Assessment Report provided with the application may be necessary to demonstrate compliance with AO5.3.</i></p>	<p><b>N/A:</b> The proposed development does not involve activities that will create odorous air emissions.</p>
<p>PO<sub>6</sub> Lot reconfigurations for residential or other environmentally sensitive land uses do not encroach upon existing or approved uses that may detrimentally impact upon the amenity of those proposed uses in terms of odour nuisance.</p>	<p>AO<sub>6.1</sub> Lots for residential or other environmentally sensitive land uses are not located within the distances from specific uses outlined in Table 9.4.2:2 at the end of this code.</p> <p>AO<sub>6.2</sub> Where lots for residential or other environmentally sensitive land uses are located within the distances from specific existing uses outlined in Table 9.4.2:2, an Odour Assessment Report has been provided to demonstrate that the development will achieve the following thresholds therefore minimising odour nuisance.</p>	<p><b>N/A:</b> The proposed development does not involve a lot reconfiguration.</p> <p><b>N/A:</b> The proposed development does not involve a lot reconfiguration.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary					
	<table border="1" style="width: 100%;"> <thead> <tr> <th style="text-align: left;">Existing Use/Activity</th> <th style="text-align: left;">Odour Level at Sensitive Receiving Environment.</th> </tr> </thead> <tbody> <tr> <td rowspan="2" style="text-align: center;">All Activities</td> <td>2OU/m<sup>3</sup> 3 minute average, 99.5th percentile.</td> </tr> <tr> <td>4OU/m<sup>3</sup> 3 minute average, 99.9th percentile.</td> </tr> </tbody> </table>	Existing Use/Activity	Odour Level at Sensitive Receiving Environment.	All Activities	2OU/m <sup>3</sup> 3 minute average, 99.5th percentile.	4OU/m <sup>3</sup> 3 minute average, 99.9th percentile.	
Existing Use/Activity	Odour Level at Sensitive Receiving Environment.						
All Activities	2OU/m <sup>3</sup> 3 minute average, 99.5th percentile.						
	4OU/m <sup>3</sup> 3 minute average, 99.9th percentile.						
<p>PO<sub>7</sub> Putrescibles waste generated as a result of the development does not cause odour nuisance issues for adjoining land uses.</p>	<p>AO<sub>7.1</sub> The development ensures that all putrescibles waste will be stored in a manner that prevents odour nuisance and fly breeding and will be disposed of at intervals not exceeding seven (7) days.</p>	<p><b>Complies:</b> The proposed development will retain the existing kerbside waste collection arrangements. The proposed development will involve weekly relocation of wheelie bins for collection to the kerbside by the property manager. The proposed development will involve booking policies to ensure that guests handle putrescible waste appropriately to avoid odour or vermin issues.</p>					
<b>Noise</b>							
<p>PO<sub>8</sub> The generation of noise from the premises does not cause Environmental Harm or Nuisance to adjoining properties or other noise sensitive land uses.</p> <p>(a) Development:</p> <ul style="list-style-type: none"> <li>(i) is located in an appropriate zone;</li> <li>(ii) proposes best practice design and construction materials (in relation to noise attenuation); and</li> <li>(iii) proposes operational practices that will minimise noise nuisance for adjoining sensitive land uses.</li> </ul>	<p>AO<sub>8.1</sub> The development will achieve the following noise levels (when measured at the nearest sensitive receiver):</p> <ul style="list-style-type: none"> <li>(a) Background (L<sub>90</sub>) + 5dB(A) for variable noise between the hours of 7:00 am to 10:00 pm (measured at the facade of the sensitive land use);</li> <li>(b) Background (L<sub>90</sub>) + 3dB(A) for variable noise between the hours of 10:00 pm and 7:00 am (measured within bedrooms assuming open windows);</li> <li>(c) Background (L<sub>90</sub>) for continuous noise sources (measured at the facade of the sensitive land use between 7:00 am and</li> </ul>	<p><b>Complies:</b> The proposed development does not involve activities that are likely to involve excessive noise. The proposed development has been designed to maintain a high level of residential amenity and manage potential noise impacts by restricting bookings to one (1) group at a time, strict booking rules including no parties, limiting the number of guests, and managing potential impacts through measures such as a Noise Complaints Management Procedure; refer to <b>Appendix D</b>.</p>					

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>10:00 pm and within bedrooms assuming open windows from 10:00 pm – 7:00 am); and            (d) maximum limit L<sub>max</sub> 45dB(A) inside dwellings; and            The development will achieve the Acoustic Quality Objectives listed within the <i>Environmental Protection (Noise) Policy 2008</i>.  <b>HOWEVER</b>            AO<sub>8.2</sub> Where a development is unable to meet noise levels specified in AO<sub>8.1</sub> an acoustic assessment has been undertaken by a suitably qualified and skilled person which demonstrates that the development will not result in environmental nuisance at any existing or likely future residential premises (within a 10 year planning horizon).</p>	
<p>PO<sub>9</sub> Development (other than licensed premises operating under a Liquor Licensing Approval) proposing the use of amplified sound equipment is designed, constructed and operated in a manner that is sensitive to the impacts of high and low frequency noise on adjoining sensitive land uses.</p>	<p>AO<sub>9.1</sub> Where development (other than licensed premises operating under a Liquor Licensing Approval) proposes the use of amplified sound equipment, existing background octave band centre frequencies have been assessed and the development proposes the following maximum sound pressure criterion:</p>	<p><b>N/A:</b> The proposed development does not involve the use of amplified sound equipment.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary																											
	<table border="1"> <thead> <tr> <th colspan="9">Frequency - Hz - "A" Weighted</th> </tr> <tr> <th></th> <th>31</th> <th>63</th> <th>125</th> <th>250</th> <th>500</th> <th>1K</th> <th>2K</th> <th>4K</th> </tr> </thead> <tbody> <tr> <th>Background Level SPL dB(A)</th> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> </tr> </tbody> </table>	Frequency - Hz - "A" Weighted										31	63	125	250	500	1K	2K	4K	Background Level SPL dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	
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PO <sub>10</sub> Proposed sensitive land uses in close proximity to existing lawful land uses involving significant noise emissions such as entertainment venues, child care centres, industrial zones or other commercial premises are designed and constructed in a manner that achieves acoustic amenity for the users of the development.	<p>AO<sub>10.1</sub> The development is designed to achieve the internal noise criterion (Acoustic Quality Objectives) for the particular use as specified within the <i>Environmental Protection (Noise) Policy 2008</i>.</p> <p>AO<sub>10.2</sub> Where the proposed sensitive land use is not listed in the Environmental Protection (Noise) Policy 2008, the development is designed and constructed to meet the internal sound level design criterion contained in Australian Standard AS2107:2000 <i>Acoustics – Recommended design sound levels and reverberation times for building interiors</i>.</p> <p>AO<sub>10.3</sub> Where the sensitive land use is within or adjoining entertainment venues utilising amplified music the applicant has undertaken an acoustic assessment of existing background octave levels and designed the sensitive land use component to mitigate the impacts of low frequency noise (particularly between 31.5Hz and 125Hz).</p>	<p><b>N/A:</b> The proposed development is not in proximity to existing lawful land uses involving significant noise emissions.</p> <p><b>N/A:</b> The proposed development is not in proximity to existing lawful land uses involving significant noise emissions.</p> <p><b>N/A:</b> The proposed development is not in proximity to an entertainment venue.</p>																											

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>11</sub> Proposed sensitive land uses adjoining Council controlled arterial roads (other than designated 'Transport Noise Corridors') are designed and constructed in a manner that provides acoustic amenity for users/residents of the development.	<p>Where development involves a sensitive land use adjoining a Council controlled arterial road (other than designated 'Transport Noise Corridors':</p> <p>AO<sub>11.1</sub> The development is designed and constructed in a manner that achieves the internal noise Acoustic Quality Objectives listed within the <i>Environmental Protection (Noise) Policy 2008</i>.</p> <p>AO<sub>11.2</sub> The siting of buildings and selection of construction materials complies with the specifications of Australian Standard AS3671-1989 <i>Acoustics – Road traffic noise intrusion – Building siting and construction</i>.</p>	<b>N/A:</b> The proposed development is not located on a Council controlled arterial road.
PO <sub>12</sub> Proposed sensitive land uses adjoining 'Transport Noise Corridors' as designated by State or Local Government are designed and constructed in a manner that provides acoustic amenity for users/residents of the development.	AO <sub>12.1</sub> Proposed sensitive land uses adjoining 'Transport Noise Corridors' as designated by State or Local Government comply with the <i>Queensland Development Code Mandatory Part (MP) 4.4 'Buildings in a Transport Noise Corridor'</i> for all habitable rooms adjoining the corridor.	<b>N/A:</b> The proposed development is not located on a designated Transport Noise Corridor.
PO <sub>13</sub> Air conditioning units, refrigeration units and any other form of mechanical ventilation or extraction systems do not adversely impact on the acoustic amenity of surrounding sensitive land uses.	<p>AO<sub>13.1</sub> Plant of this nature is not elevated, is acoustically shielded (if necessary) and will not be audible at adjoining sensitive receivers.</p> <p>AO<sub>13.2</sub> Roof-top mounted plant and equipment is located away from surrounding sensitive land uses and is acoustically shielded to achieve a nil increase in background noise levels (L<sub>90</sub>) at the nearest sensitive receiver.</p>	<p><b>N/A:</b> The proposed development does not involve new plant or equipment or existing elevated plant or equipment.</p> <p><b>N/A:</b> The proposed development does not involve new plant or equipment or existing elevated plant or equipment.</p>
PO <sub>14</sub> The construction phase of the development does not cause adverse acoustic impacts on surrounding sensitive receivers.	AO <sub>14.1</sub> Building work (including excavation and filling) is only conducted between the hours of 6:30 am and 6:30 pm Monday to Saturday (excluding public holidays).	<b>N/A:</b> The proposed development does not involve new building work.

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	AO <sub>14.2</sub> Where building work is proposed outside of the acceptable timeframe of 6:30 am to 6:30 pm (Monday – Saturday) the applicant has supplied a 'construction management plan' which adequately addresses noise mitigation measures.	<b>N/A:</b> The proposed development does not involve new building work.
PO <sub>15</sub> Private sporting courts do not create acoustic amenity issues for surrounding sensitive receivers.	AO <sub>15.1</sub> Private sporting courts are not used between 10:00 pm and 7:00 am. AO <sub>15.2</sub> Mechanical equipment such as ball throwing machines which create audible noise at the nearest sensitive receiver is not used between 7:00 pm and 7:00 am.	<b>N/A:</b> The proposed development does not involve private sporting courts.
PO <sub>16</sub> Vibration from the development does not affect the amenity of surrounding sensitive land uses or cause environmental harm or nuisance.	AO <sub>16.1</sub> The development does not result in vibration impacts outside of the development site. AO <sub>16.2</sub> Where vibration may impact on surrounding sensitive land uses, the proponent has provided a vibration impact assessment or alternatively included vibration within an environmental impact report for the site which demonstrates that the level of vibration will not cause adverse amenity impacts or cause environmental harm or nuisance at any sensitive land use surrounding the development.	<b>N/A:</b> The proposed development does not involve activities that will create vibration impacts.
<b>Dust</b>		

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>17</sub> The construction phase of the development prevents or mitigates (to an acceptable level) the release of dust particles which have potential to cause environmental nuisance to adjoining sensitive receivers (including sensitive receivers along haulage routes during excavation and filling operations).</p>	<p>AO<sub>17.1</sub> Off-site release of dust particles will be strictly managed to ensure that dust emissions do not travel beyond the property boundary and environmental nuisance does not occur.</p> <p>AO<sub>17.2</sub> Areas of exposed fill, excavation and unsealed accesses on the site are watered regularly (particularly during periods of high or constant wind) to reduce dust generation.</p> <p>AO<sub>17.3</sub> Areas of fill and excavation are graded, compacted and planted and/or mulched immediately after the dumping operation is complete.</p> <p>AO<sub>17.4</sub> Stockpiles of aggregate, sand or other materials brought onto the site are sprayed with water (or treated with an alternative method) to minimise dust nuisance. The frequency of water spraying is increased during hot, dry periods or where wind conditions are such that a dust nuisance is likely to occur. Stockpiles are located away from adjoining sensitive land uses.</p> <p><i>Note: Where excavation and filling exceeds 1,000 cubic metres the development has submitted a 'construction management plan' which adequately addresses dust mitigation measures. Measures must include strategies such as progressive rehabilitation and complaints processes.</i></p>	<p><b>N/A:</b> The proposed development does not involve new building work or activities that will generate dust.</p> <p><b>N/A:</b> The proposed development does not involve new building work or activities that will generate dust.</p> <p><b>N/A:</b> The proposed development does not involve new building work or activities that will generate dust.</p> <p><b>N/A:</b> The proposed development does not involve new building work or activities that will generate dust.</p>
<p>PO<sub>18</sub> Haulage activities associated with excavation and filling are managed to prevent environmental nuisance issues.</p>	<p>AO<sub>18.1</sub> Haulage routes are selected on the basis of using the most suitable road surface to prevent dust generation and minimising the number of</p>	<p><b>N/A:</b> The proposed development does not involve excavating or filling.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>dwellings or other sensitive land uses affected by potential dust nuisance.</p>	
<p>PO<sub>19</sub> Water used for dust suppression activities does not itself create environmental harm.</p>	<p>AO<sub>19.1</sub> Water approved as a method for controlling dust emissions must not be used in a manner that enables contaminated water to enter any stormwater system or natural drainage corridor outside of the site boundaries.</p>	<p><b>N/A:</b> The proposed development does not involve excavating or filling.</p>
<p>PO<sub>20</sub> The ongoing operation of the development site does not create dust nuisance for adjoining landholders.</p>	<p>AO<sub>20.1</sub> Areas within the site that are frequently used for vehicular purposes are imperviously sealed.</p> <p>AO<sub>20.2</sub> Industry-specific activities undertaken on site that create dust are performed in an enclosed shed or other structure with suitable dust extraction and filtration systems.</p> <p>AO<sub>20.3</sub> Grain facilities are equipped with semi enclosed grain receival hoppers fitted with dust extraction and filtration systems. All conveyor belts and bulk grain processing equipment are enclosed to prevent dust emission. Bunker storage without dust extraction is only permitted whereby the release of dust will not impact on surrounding sensitive receivers.</p> <p>AO<sub>20.4</sub> All development likely to generate any significant amount of dust must have an adequate water supply available at all times in order to undertake proactive dust reduction measures e.g. watering of access roads.</p> <p>AO<sub>20.5</sub> Development that is likely to create ongoing significant dust issues has submitted a 'site based management plan' which adequately addresses dust mitigation measures.</p>	<p><b>Complies:</b> The proposed development involves an existing sealed driveway access.</p> <p><b>N/A:</b> The proposed development does not involve industry-specific activities.</p> <p><b>N/A:</b> The proposed development does not involve grain facilities.</p> <p><b>N/A:</b> The proposed development does not involve activities that are likely to generate a significant amount of dust.</p> <p><b>N/A:</b> The proposed development does not involve activities that are likely to generate a significant amount of dust.</p>
<p>PO<sub>21</sub> Proposed sensitive land uses are adequately separated from existing lawful land uses likely to</p>	<p>AO<sub>21.1</sub> Sensitive land uses achieve the separation distances from the nominated uses specified in</p>	<p><b>Complies:</b> The proposed development is not in proximity to existing lawful land uses likely to generate</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
generate dust emissions such as landfill sites, quarries, cropping land, motor sport facilities and other similar dust generating activities.	Table 9.4.2:2.	dust emissions.
PO <sub>22</sub> Development does not result in dustfall quantities that are likely to impact on the health of surrounding sensitive receivers.	AO <sub>22.1</sub> Dustfall averaged over an annual period of time does not exceed 133mg/m <sup>2</sup> /day when measured at the nearest sensitive receiver.	<b>N/A:</b> The proposed development does not involve activities that are likely to generate a significant amount of dust.
<b>General Emissions</b>		
PO <sub>23</sub> Air emissions resulting from development do not cause environmental harm (including environmental nuisance).	AO <sub>23.1</sub> The development does not result in air emissions that exceed any of the acceptable levels specified within the <i>Environmental Protection (Air) Policy 2008</i> . AO <sub>23.2</sub> Where a type of air emission is not listed within the <i>Environmental Protection (Air) Policy 2008</i> the proponent can demonstrate that the level of emission is in compliance with Australian ambient air quality standards; or If Australian standards do not exist, an ambient air quality standard from another country or organisation may be used with appropriate justification. AO <sub>23.3</sub> Where a development is proposing to generate and release air emissions in excess of current air quality emission standards the proponent will provide an 'air quality impact assessment' which adequately addresses the impact of the release and provides justification as to why the industry cannot mitigate the levels further.	<b>Complies:</b> The proposed development does not involve activities that will result in air emissions.  <b>N/A:</b> The proposed development does not involve activities that will result in air emissions.  <b>N/A:</b> The proposed development does not involve activities that will result in air emissions.
PO <sub>24</sub> Child Care Centres are well located to avoid any harmful impacts from air pollution.	AO <sub>24.1</sub> Maximum concentrations of air pollutants do not exceed those recommended by the National Health and Medical Research Council.	<b>N/A:</b> The proposed development does not involve a child care centre.

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**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>25</sub> Proposed sensitive land uses are adequately separated from existing lawful land uses that produce air emissions.	AO <sub>25.1</sub> Sensitive land uses in relation to air emissions are not located within the separation distances specified in Table 9.4.2:2.	<b>Complies:</b> The proposed development is not in proximity to existing lawful land uses that produce air emissions.
PO <sub>26</sub> Electromagnetic radiation levels from telecommunications and other facilities do not pose health risks to the community.	AO <sub>26.1</sub> Emission levels from equipment and infrastructure comply with the relevant industry standards as demonstrated through an approved written statement or certification provided by the carrier to council i.e. Electromagnetic Energy report.	<b>N/A:</b> The proposed development does not involve electromagnetic radiation emissions.
PO <sub>27</sub> Air emission vents or stacks are sited appropriately to ensure that surrounding land uses are not exposed to concentrated levels of air contaminants.	AO <sub>27.1</sub> Car park exhaust stacks are located away from adjoining sensitive receivers. AO <sub>27.2</sub> Emissions are discharged vertically and have an exit velocity of at least 10m/second. AO <sub>27.3</sub> Spray booth exhaust stacks are at least 8m in height or 4m higher than the adjoining ridgeline of a neighbouring building (if the building is within 40m of the emission point), whichever is the greater. AO <sub>27.4</sub> Tank venting for hydrocarbon fuel storage and LP Gas is located in accordance with AS1940-2004 <i>The Storage and handling of flammable and combustible liquids (for hydrocarbons)</i> and AS1596:2008 <i>The Storage and Handling of LP Gas</i> .	<b>N/A:</b> The proposed development does not involve air emission vents or stacks. <b>N/A:</b> The proposed development does not involve air emission vents or stacks. <b>N/A:</b> The proposed development does not involve air emission vents or stacks. <b>N/A:</b> The proposed development does not involve hydrocarbon fuel storage.
<b>Waste Management</b>		
PO <sub>28</sub> The development (excluding high rise buildings in excess of three (3) stories) is designed to ensure that waste storage and collection can be undertaken in a manner that complies with Council's <i>Technical Guidelines for New</i>	AO <sub>28.1</sub> For commercial premises and industry activities (other than those premises utilising Council's wheelie bin waste collection program): (a) general waste and recycling containers are located within the curtilage of the property in an area that enables the waste	<b>N/A:</b> The proposed development does not involve commercial or industrial activities.

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**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p><i>Developments Waste Storage and Collection Requirements.</i></p>	<p>collection truck to pick up the containers while entering and leaving the premises in a forward gear;</p> <p>(b) a container storage area is dedicated that is large enough to cater for the expected volume of general waste and recycling;</p> <p>(c) storage areas are screened either behind a building or using screening materials or landscaping to a minimum height of 1.5m;</p> <p>(d) where bulk bins (or alternative combined waste and recycling containers exceeding 2 cubic metres) are proposed the bin storage area is roofed and bunded, contains an impervious surface, is in close proximity to a hose cock and is graded and drained to either a wastewater system connection (requiring a trade waste approval) where sewer is available or in sewered areas, storage areas are drained to an area of significant landscaping, waste water treatment device or water quality improvement system e.g. Bioretention;</p> <p>(e) where bulk bins (or alternative combined waste and recycling containers exceeding 2 cubic metres) are proposed the bin storage area is designed to enable bins to be washed out within the storage area and drained to a sewer system (requiring trade waste approval) within sewered areas or area of significant landscaping, water treatment device or water quality improvement system e.g. Bioretention in non-sewered areas; and</p>	

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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(f) bin storage areas do not pose amenity issues for surrounding sensitive receivers, including odour during storage periods or noise issues resulting from collection programs.</p>	
	<p>AO<sub>28.2</sub> For a Multiple Dwelling of three (3) – six (6) units the development satisfies one of the following criteria:</p> <p>(a) a minimum road frontage is available within the immediate road reserve adjoining the development in order to place the required number of waste and recycling containers out for collection (2 x 240L wheelie bins per tenement) when calculated at 1m/bin e.g. a development requiring eight (8) bins must have at least 8m of useable road reserve (in terms of bin collection, excluding a 1m clearance around power poles and any area below a street trees canopy where bins cannot be collected);</p> <p>OR</p> <p>(b) the complex includes a communal bin storage area, whereby the body corporate will implement internal procedures requiring residents to progressively fill bins and only place full bins out for collection; and</p> <p>(c) each tenement has an approved bin storage area that will not create amenity issues for surrounding sensitive land uses; and</p>	<p><b>N/A:</b> The proposed development does not involve multiple dwellings.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(d) bin storage areas are screened behind buildings for amenity purposes; and</p> <p>(e) storage areas are not within dwellings (including garages) and it is not necessary to take the bins through dwellings (including garages) for collection purposes; and</p> <p>(f) a hose cock is located in close proximity to the storage location to enable bins to be cleaned; and</p> <p>(g) where a rear storage area is not possible bins are stored in a minimum 1.5m high screened area in the front of the dwelling(s);</p> <p>OR</p> <p>(h) screened communal storage areas (to a minimum height of 1.5m) are proposed which contain an impervious floor, hose cock and grading/drainage towards a grassed area or other porous surface.</p>	
	<p>AO<sub>28.3</sub> For a Multiple Dwelling above six (6) units the development satisfies one of the following criteria:</p> <p>(a) The development incorporates 'internal collection' of either bulk bins or wheelie bins (in accordance with the waste management guideline that accompanies the environmental standard);</p> <p>OR</p> <p>(b) Communal bin storage areas contain a roof, bunding and bin 'washing' provisions in the form of either a sewer connection</p>	<p><b>N/A:</b> The proposed development does not involve multiple dwellings.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(requiring trade waste approval) or where no sewer is available a connection to a waste water treatment device, drain to an area of significant landscaping or drain to a water quality improvement device e.g. Bioretention system is acceptable;</p> <p>OR</p> <p>(c) Where 'internal collection' is proposed the internal design complies with the waste management guideline that accompanies this environmental standard and a certification from a registered RPEQ has been provided to demonstrate that manoeuvrability is acceptable for an appropriately sized refuse vehicle.</p>	
	<p>AO<sub>28.4</sub> Commercial premises utilising Council's wheelie bin waste collection service to dispose of commercial waste:</p> <ul style="list-style-type: none"> <li>(a) utilise a maximum of four (4) wheelie bins i.e. less than 1 cubic metre;</li> <li>(b) store bins within the curtilage of the property in a designated area in close proximity to a hose cock, whereby any adjoining sensitive land uses will not experience amenity issues i.e. odour;</li> <li>(c) store bins on an impervious surface;</li> <li>(d) place bins on the road reserve for a maximum period of 24 hours during collection programs; and</li> <li>(e) store bins in an area that is screened from public view either in a building, behind a building or within a purpose built screened</li> </ul>	<p><b>N/A:</b> The proposed development does not involve a commercial premises.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	storage area within a 1.5m minimum height.	
PO <sub>29</sub> Development for a Community Residence, Residential Care Facility or Retirement Facility utilising communal bin storage areas ensure that residents have reasonable access to waste containers, where the development is for aged care purposes.	AO <sub>29.1</sub> For development involving a Community Residence, Residential Care Facility or Retirement Facility, bin storage areas are located within reasonable proximity to all units, in accordance with Council's Environmental Guideline.	<b>N/A:</b> The proposed development does not involve a community residence, residential care facility or retirement facility.
PO <sub>30</sub> High rise (in excess of three (3) storeys) residential developments and joint commercial and residential developments are designed to enable best practice waste management principles to be applied.	AO <sub>30.1</sub> The applicant has provided a waste management plan that as a minimum has addressed the following issues: (a) likely waste quantity and waste type to be generated on site on a weekly basis; (b) likely recycling quantities to be generated on a weekly basis; (c) waste container and recycling container (type and volume) requirements for the residential component (based on 240L of general waste and 240L of recycling per unit) and the commercial component (if applicable); (d) waste storage area locations; (e) dual waste chutes for general waste and recycling; (f) bin room specifications and hygiene practices for waste handling areas, chutes, waste containers and other applicable equipment; (g) collection arrangements and manoeuvring diagrams (including overhead clearances); (h) waste minimisation practices; (i) use of compactors;	<b>N/A:</b> The proposed development does not involve a high rise residential development or joint commercial and residential development.

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Performance outcomes	Acceptable outcomes	Compliance summary
	(j) an impact assessment of waste management practices on any surrounding sensitive land uses; (k) air extraction fans, refrigeration or associated devices for refuse storage areas to prevent odour, particularly where putrescibles are stored; and (l) clinical and related waste storage and collection issues (if applicable).	
PO <sub>31</sub> Demolition and building activities actively involve waste minimisation and waste avoidance principles including the promotion of recycling and re-use.	AO <sub>31.1</sub> The development will be carried out in accordance with the waste management hierarchy outlined in the <i>Technical Guideline for New Developments Waste Storage and Collection Requirements</i> and the applicant has nominated the quantity and type of materials that will be disposed of to landfill.	<b>N/A:</b> The proposed development does not involve demolition or new building work.
PO <sub>32</sub> Development that involves the generation of 'clinical and related waste' as per the definition of the <i>Technical Guideline for New Developments Waste Storage and Collection Requirements</i> is designed to adequately cater for legislative storage and collection requirements.	Where involving development that involves the generation of 'clinical and related waste' as per the definition of the <i>Technical Guideline for New Developments Waste Storage and Collection Requirements</i> :  AO <sub>32.1</sub> The storage of 'clinical and related waste' is in accordance with the <i>Technical Guideline for New Developments Waste Storage and Collection Requirements</i> with storage locations being demonstrated on submitted site/floor plans.  AO <sub>32.2</sub> The development has proposed a method of disposing of 'clinical and related waste' and has demonstrated that an applicable waste collection vehicle is able to manoeuvre on site,	<b>N/A:</b> The proposed development does not involve activities that will generate clinical and related waste.

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**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	while entering and leaving the premises in a forward gear.	
PO <sub>33</sub> Residential development involving 'internal collection' of either bulk bins or wheelie bins is designed to a standard that enables heavy vehicle access and manoeuvring whilst providing safety to residents and the protection of infrastructure.	AO <sub>33.1</sub> The development is designed and certified by a RPEQ and complies with the requirements outlined in Council's <i>Technical Guidelines for Waste Storage and Collection Requirements for New Developments</i> , including: <ul style="list-style-type: none"> <li>(a) appropriate manoeuvring is adequate with vehicles being able to enter and exit the property in a forward gear;</li> <li>(b) overhead clearance is adequate for the applicable refuse vehicle;</li> <li>(c) road surface is appropriate for a HRV;</li> <li>(d) side clearance is appropriate for wheelie bin collection;</li> <li>(e) collection areas are appropriate for either bulk bins or wheelie bins;</li> <li>(f) minimum road width of 5.5m; and</li> <li>(g) internal road networks enable the refuse vehicle to traverse the site without resident safety being jeopardised.</li> </ul>	<b>N/A:</b> The proposed development does not involve internal collection of bulk bins or wheelie bins.
PO <sub>34</sub> Development involving refuse storage and collection external to Council's waste contract utilise waste containers and hygiene practices that prevent odour issues and remove harbourage opportunities for vermin and mosquitoes.	AO <sub>34.1</sub> The applicant will utilise the following control measures: <ul style="list-style-type: none"> <li>(a) putrescibles waste will be removed from the property at intervals not exceeding seven (7) days (putrescibles will be refrigerated where possible and appropriate);</li> <li>(b) tight fitting lid assemblies will be utilised on all waste containers to prevent the pooling of rainwater, thus minimising mosquito breeding opportunities; and</li> <li>(c) bins will be secured to ensure that vermin</li> </ul>	<b>N/A:</b> The proposed development does not involve storage and collection external to Council's waste contract.

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Environmental Standards Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>and pest animals do not have access to a potential food source; and            (d) bins will be cleaned on an 'as needed' basis if odour is identified as an issue.</p>	

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**Integrated Water Cycle Management Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes or compliance outcomes if made applicable to compliance assessable development	Compliance summary
<b>Stormwater Management</b>		
PO <sub>1</sub> Development does not adversely impact on the quality of receiving waters by avoiding or minimising pollutants entering and being transported with stormwater.	AO <sub>1,1</sub> Stormwater quality treatment measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> . AO <sub>1,2</sub> Pollutant load reductions are achieved in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve any changes to existing stormwater arrangements.
PO <sub>2</sub> Adverse impacts of construction activities on stormwater quality are avoided or minimised using best practice environmental management for erosion and sediment control.	AO <sub>2,1</sub> Sediment and erosion control measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development does not involve new building work or construction activities.
PO <sub>3</sub> Stormwater management incorporates water sensitive urban design techniques and avoids adverse impacts from water quantity, flow rates and duration and frequency in receiving waters, having regard to: (a) channel, bed and bank stability; (b) aquatic and riparian ecosystems; and (c) hydrological functions.	AO <sub>3,1</sub> Stormwater flow control measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve any changes to existing stormwater arrangements.
<b>Waste Water Management</b>		
PO <sub>4</sub> Development does not discharge waste water to a waterway or external to the site unless demonstrated to be best practice environmental management for that site and has appropriate regard for:	AO <sub>4,1</sub> Waste water management measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies:</b> The proposed development involves short term accommodation within an existing dwelling. There are no changes proposed to the existing reticulated wastewater connection.

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**Integrated Water Cycle Management Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes or compliance outcomes if made applicable to compliance assessable development	Compliance summary
<ul style="list-style-type: none"> <li>(a) cumulative effects;</li> <li>(b) the applicable water quality objectives for the receiving waters;</li> <li>(c) adverse impact on ecosystem health of receiving waters; and</li> <li>(d) in waters mapped as being of high ecological value, the adverse impacts of such releases and their offset.</li> </ul>		
<b>Artificial Waterways and Water Bodies</b>		
PO <sub>5</sub> The waterway or water body is designed to integrate multiple functions, including: <ul style="list-style-type: none"> <li>(a) aesthetics, landscaping, and recreation;</li> <li>(b) flood management;</li> <li>(c) stormwater management;</li> <li>(d) water conservation and reuse;</li> <li>(e) community health; and</li> <li>(f) pest management.</li> </ul>	AO <sub>5.1</sub> Artificial waterways or water bodies are designed in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development does not involve artificial waterways or water bodies.
PO <sub>6</sub> The waterway is located and designed to be responsive to natural drainage features.	AO <sub>6.1</sub> Artificial waterways or water bodies are designed in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development does not involve artificial waterways or water bodies.
PO <sub>7</sub> The waterway or body is designed to minimise whole of life cycle costs.	AO <sub>7.1</sub> Artificial waterways or water bodies are designed in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development does not involve artificial waterways or water bodies.

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**Integrated Water Cycle Management Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes or compliance outcomes if made applicable to compliance assessable development	Compliance summary
<b>Flooding and Drainage</b>		
PO <sub>8</sub> Flooding and drainage characteristics upstream or downstream of the site are not worsened.	AO <sub>8.1</sub> Development is undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling. The proposed development does not involve any changes to existing stormwater arrangements that would impact local flooding or drainage characteristics.
PO <sub>9</sub> The drainage network has sufficient capacity to safely convey stormwater run-off from the site.	AO <sub>9.1</sub> Development is undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling. The proposed development does not involve any changes to existing stormwater arrangements.
PO <sub>10</sub> Stormwater resulting from roofed areas is collected and discharged in a manner that does not adversely affect the stability of buildings or the use of adjacent land.	AO <sub>10.1</sub> Roof water is collected and discharged in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling. The proposed development does not involve any changes to existing stormwater arrangements.
<b>Water Cycle Management</b>		
PO <sub>11</sub> The design and management of the development integrates water cycle elements so that: (a) water is used efficiently and potable water demand is reduced; (b) wastewater production is minimised; (c) stormwater peak discharges and runoff volumes are not worsened; (d) natural drainage lines and hydrological regimes are maintained as far as possible; (e) large, uninterrupted impervious surfaces are minimised; (f) reuse of stormwater and grey-water is	AO <sub>11.1</sub> Integrated water management practices and infrastructure are implemented in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>N/A:</b> The proposed development does not involve any changes to existing water, wastewater or stormwater arrangements.

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**Integrated Water Cycle Management Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes or compliance outcomes if made applicable to compliance assessable development	Compliance summary
<p>encouraged where public health and safety will not be compromised; and (g) water is used efficiently.</p>		

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**Landscaping Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>1</sub> Landscape design is developed by a suitably qualified landscape professional and demonstrates an integrated approach to planning/development issues and documents both hard and soft works proposed for the development.	AO <sub>1.1</sub> Landscape documentation is prepared by the landscape professional identified in Table 9.4.4:2.	<b>N/A:</b> The proposed development is not of a scale or nature to warrant provision of a landscaping plan.
PO <sub>2</sub> Landscape construction is undertaken by a suitably qualified landscape professional.	AO <sub>2.1</sub> Landscape construction is carried out by a member of the Queensland Association of Landscape Industries.	<b>N/A:</b> The proposed development does not involve provision of landscaping.
PO <sub>3</sub> Landscape design reflects the local context and incorporates cohesive and desirable aspects of the prevailing landscape character. (Desirable aspects are those considered necessary to maintain and enhance the character, setting and/or ambience, and ecological values of the location.)	AO <sub>3.1</sub> Where a street or locality has an identifiable character derived from existing vegetation, similar or identical plant species are used. AO <sub>3.2</sub> Existing desirable landscape elements and treatments are incorporated into landscaping to integrate the development into the existing character of the area. AO <sub>3.3</sub> Existing site trees are integrated into the development. AO <sub>3.4</sub> Species selection is reflective of cool temperate species.	<b>N/A:</b> The proposed development does not involve provision of landscaping.
PO <sub>4</sub> Where the development involves the creation of a new road street tree planting is undertaken having consideration of: (a) the hierarchy and function of the street; (b) selection of appropriate species; (c) avoidance of conflict between the street tree and utilities and services within the road reserve; (d) soil conditions; (e) existing street trees;	Where the development involves the creation of a new road: AO <sub>4.1</sub> Street planting is carried out in accordance with the requirements of <i>SC6.2 PSP No. 2 Engineering Services Infrastructure Roads and Drainage</i> . AO <sub>4.2</sub> Species and materials are used that minimise the use of potable water. AO <sub>4.3</sub> Street tree planting is in accordance with PSP No.8 – Street Trees.	<b>N/A:</b> The proposed development does not involve creation of a new road.

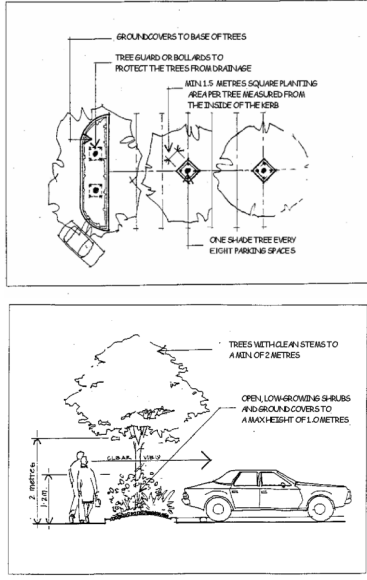
**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Landscaping Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
(f) solar access; and (g) driveway access.		
PO <sub>5</sub> Fencing design and acoustic barriers: (a) are compatible with the existing streetscape and proposed development type; and (b) provide visual interest and address the street.	AO <sub>5.1</sub> Front fences longer than 15m and greater than 1,400mm in height are visually fragmented with recesses at least 1.2m deep and 1.2m wide at 15m intervals, planted with at least one tree and groundcovers. AO <sub>5.2</sub> All planting and recesses along a fence are located within the property boundary and planting recesses are accessible from within the site. AO <sub>5.3</sub> Where acoustic fencing is required by the planning scheme it is designed by an acoustic engineer and incorporates a minimum 3m vegetated buffer on either side of the fence with vegetation having a mature height equal to or above the height of the acoustic fencing.	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve provision of new fencing.
PO <sub>6</sub> Location, design and provision of planting in carparks and internal roadways achieve a high degree of shade, amenity and safety.	AO <sub>6.1</sub> Landscaping visually fragments and shades carparking areas with regular tree planting in individual planting bays evenly distributed throughout the car parking area at the rate of one planting bay per eight (8) carparking spaces. AO <sub>6.2</sub> Individual planting bays have a minimum dimension of 1,500 x 1,500mm with permeable surface treatments and are flush with the finished surface levels of the car park. AO <sub>6.3</sub> No raised kerbing is provided around planting bays. Wheelstops or bollards are used to delineate planting bays where necessary and finished carpark surface levels fall toward planting areas.	<b>N/A:</b> The proposed development does not involve provision of landscaping.  <b>N/A:</b> The proposed development does not involve provision of landscaping.  <b>N/A:</b> The proposed development does not involve provision of landscaping.

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Landscaping Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>6.4</sub> Planting bays incorporate ground covers less than 1,000mm height that allow unobstructed surveillance.</p>	<p><b>N/A:</b> The proposed development does not involve provision of landscaping.</p>
		
<p>PO<sub>7</sub> Location and habit of tree planting must not interfere with the function and accessibility of any adjacent utility services.</p>	<p>AO<sub>7.1</sub> Species mature height and siting must not interfere with or compromise overhead and underground utility assets including stormwater inlet pits.</p> <p>AO<sub>7.2</sub> Tree planting must be a minimum of 2m from any mains water easements and offset 4m from any sewer main or inspection chamber.</p>	<p><b>N/A:</b> The proposed development does not involve provision of new tree planting.</p> <p><b>N/A:</b> The proposed development does not involve provision of new tree planting.</p>

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**Landscaping Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>8</sub> Maintenance access points must be considered and accommodated for in the site planning and design process.	AO <sub>8.1</sub> Access by appropriate maintenance or utility vehicles must be demonstrated with ground surface treatments that are stable and usable in all weather. AO <sub>8.2</sub> Functional maintenance vehicle circulation and access gates to be provided.	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve new maintenance access points.
PO <sub>9</sub> On-site stormwater harvesting is to be maximised for irrigating landscaping in development with reuse measures and amelioration of stormwater impacts provided.	AO <sub>9.1</sub> Landscape design takes advantage of the flow of water along overland flow paths. AO <sub>9.2</sub> Landscaping is used to help maximise opportunities for on-site stormwater infiltration by: (a) minimising impervious surfaces and incorporating semi-permeable paving products; (b) falling hard surfaces towards pervious surfaces such as turf or mulched areas; (c) maximise opportunities for turf and planting areas; (d) align planting areas parallel to contours to slow the flow of surface water; and (e) ensure planting palette comprises canopy tree species. AO <sub>9.3</sub> Provision for drainage is incorporated through treatments such as subsurface drains, swales, ponds and infiltration cells. AO <sub>9.4</sub> Sediment and erosion control measures are provided. AO <sub>9.5</sub> Planter boxes on podiums and building forecourts are plumbed to stormwater.	<b>N/A:</b> The proposed development does not involve provision of new landscaping. <b>N/A:</b> The proposed development does not involve provision of new landscaping. <b>N/A:</b> The proposed development does not involve provision of new landscaping. <b>N/A:</b> The proposed development does not involve provision of new landscaping. <b>N/A:</b> The proposed development does not involve provision of new landscaping.
PO <sub>10</sub> Landscape design is integrated with any existing urban design theme within the surrounding area and coordinates paving, planting, street	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development is not of a scale or nature to warrant provision of landscaping.

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**Landscaping Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
furniture, lighting, signage and other elements to reflect that theme and assist in the creation of a sense of place.		
PO <sub>11</sub> Design of pedestrian paths and places reinforces the desired character of the area and/or place and includes features to enhance their use that are of universal design to ensure non-discriminatory access and use.	AO <sub>11.1</sub> Design complies with AS1428 parts 1, 2, 3, and 4 – <i>Design for Access and Mobility</i>	<b>N/A:</b> The proposed development does not involve provision of new pedestrian paths.
PO <sub>12</sub> Risks to personal safety and the potential for crime, vandalism and fear are reduced through landscape design that has been informed by <i>Crime Prevention Through Environmental Design</i> (CPTED) principles in relation to: (a) Surveillance. (b) Access control. (c) Territorial reinforcement. (d) Space management.	<p>Landscape design incorporates the following design measures:</p> <p>AO<sub>12.1</sub> The attractiveness of crime targets is minimised by providing opportunities for effective surveillance through: clear sight lines from private to public space, reducing concealment or entrapment opportunities, public facilities (toilets, shelters etc) located to promote use, dual access points, avoiding blind corners, and lighting where appropriate.</p> <p>AO<sub>12.2</sub> Barriers are used to attract, channel or restrict the movement of people by: clear spatial definition and legibility, optimising opportunity for public interaction, visually permeable screens and fencing, appropriate use of mechanical measures that correspond to actual risk.</p> <p>AO<sub>12.3</sub> Reinforcing definition of territory and ownership of private, semi-public and public spaces through: clear design cues for use and activities, transitions and boundaries between public and private, design that encourages public</p>	<b>Complies:</b> The proposed development has considered CPTED principles where possible.

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**Landscaping Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>interaction and ownership, legible universal signage.</p> <p>AO<sub>12.4</sub> Space Management: ensuring that public spaces are appropriately utilised and maintained by the use of vandal- and graffiti-resistant materials, easily accessed and maintained fixtures.</p>	

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**Transport, Access and Parking Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Driveway Crossovers</b>		
<p>PO<sub>1</sub> Vehicular access arrangements, including driveway crossovers:</p> <p>(a) are appropriate for:</p> <p style="margin-left: 20px;">(i) the capacity of the parking area;</p> <p style="margin-left: 20px;">(ii) the volume, frequency and type of vehicle useage; and</p> <p style="margin-left: 20px;">(iii) the function and configuration of the access road;</p> <p>(b) minimise any potentially adverse impact on:</p> <p style="margin-left: 20px;">(i) the safety and efficiency of the road and pedestrian/cycle paths;</p> <p style="margin-left: 20px;">(ii) the safety and efficiency of the road and footpath users;</p> <p style="margin-left: 20px;">(iii) the integrity of any infrastructure within the road reserve; and</p> <p style="margin-left: 20px;">(iv) the safety of access to adjacent properties.</p> <p>(c) protect the amenity of premises in the vicinity by:</p> <p style="margin-left: 20px;">(i) maintaining the predominant vehicular access pattern in the street, including consistent width, grade and location;</p> <p style="margin-left: 20px;">(ii) preserving the residential amenity of the streetscape, including noise and visual impact, and consideration of existing landscaping by considering:</p> <p style="margin-left: 40px;">(A) use of materials which integrate with the streetscape (e.g.</p>	<p>AO<sub>1.1</sub> Vehicular access and driveway crossovers are not:</p> <p>(a) an additional site/property access;</p> <p>(b) to a State-controlled Road or a road with bluestone kerbing;</p> <p>(c) within 25 m of a signalised road intersection;</p> <p>(d) within 20m of an unsignalised road intersection in a Commercial or Industrial Area;</p> <p>(e) within 10m of an unsignalised road intersection in a Community, Residential, Rural or Other Area;</p> <p>(f) within 1m of any infrastructure, including street signage, power poles, street lights, manholes, stormwater gully pits, or other Council/public utility asset;</p> <p>(g) within the Tree Protection Zone, as defined by Australian Standard 4970- 2009;</p> <p>(h) for a lot with a frontage of 10m or less;</p> <p>(i) greater than 4m in width when for a lot with a frontage / width of more than 10m but less than 20m; and</p> <p>(j) greater than 6m in width when for a lot with a frontage / width of greater than 20m.</p> <p><i>Note: An additional site access is considered to be more than one site access.</i></p> <p>AO<sub>1.2</sub> Except where in a Rural Zone, vehicular access and driveway crossovers:</p>	<p><b>Performance Solution:</b> The proposed development involves an existing vehicle crossover in proximity to infrastructure such as an electricity pole. The proposed development does not involve any changes to the existing crossover, as the existing crossover is appropriate for the scale and nature of the use.</p> <p><b>N/A:</b> The proposed development does not involve any changes to the existing vehicle crossover.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>existing crossovers and driveways, etc);</p> <p>(B) minimising the width and grade of the access;</p> <p>(C) minimising impacts on the appearance of the streetscape by retaining existing vegetation, including approved landscaping; and</p> <p>(D) locating the access to minimise the impact of vehicle noise on neighbouring/adjoining properties.</p>	<p>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</p> <p>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the roadway;</p> <p>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, bus stop, or other structure within the road reserve</p> <p>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</p> <p>(e) do not have access restricted by an access restriction strip or link reserve;</p> <p>(f) do not access an unformed or unkerbed road;</p> <p>(g) are constructed from reinforced concrete;</p> <p>(h) are perpendicular to the road edge; and</p> <p>(i) are provided in accordance with the <i>Australian Standard AS 2890.1 – Off Street Car Parking and Australian Standard AS 2890.2 (where relevant) and the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure.</i></p>	<p>AO1.3 Where in a Rural Zone, vehicular access and driveway crossovers:</p> <p><b>N/A:</b> The proposed development is not located in a Rural Zone.</p>

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**Transport, Access and Parking Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</li> <li>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the roadway;</li> <li>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, loading bay, bus stop, or other structure within the road reserve;</li> <li>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</li> <li>(e) do not have access restricted by an access restriction strip or link reserve;</li> <li>(f) are sealed where accessing a sealed formed road;</li> <li>(g) are perpendicular to the road edge; and</li> <li>(h) are provided in accordance with the relevant standard drawing in <i>SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure</i>.</li> </ul>	
<b>Car Parking Provision</b>		
PO <sub>2</sub> Provision is made for on-site vehicle parking to meet the demand likely to be generated by the development and to avoid on-street parking where that would adversely impact on the	AO <sub>2.1</sub> Where in the Principal Centre Zone or Mixed Use Zone Car parking is provided at the rate of: <ul style="list-style-type: none"> <li>(a) Non-Residential Use one (1) parking space per 50m<sup>2</sup> of GFA; and</li> </ul>	<b>N/A:</b> The proposed development is not located in the Principal Centre or Mixed Use Zone.

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Transport, Access and Parking Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>safety or capacity of the road network or undue impact on local amenity.</p> <p><i>Note: Where the development does not meet the acceptable outcomes, or where no acceptable outcome is specified, a parking demand analysis report prepared by a suitably qualified person may assist in demonstrating compliance with the performance outcome.</i></p>	<p>(b) Residential Use - one (1) parking space per dwelling.</p> <p>AO<sub>2.2</sub> Where not in the Principal Centre Zone or Mixed Use Zone Car parking is provided at the rates set out in Table 9.4.6:3 to this Code.</p> <p><i>Note: Where a parking rate for a use is unspecified in Table 9.4.6:3 – no acceptable outcome is provided.</i></p> <p><i>Note: If the number of car parking spaces calculated in accordance with AO<sub>2.1</sub> and AO<sub>2.2</sub> is not a whole number, the number of parking spaces to be provided is rounded-up to next highest whole number.</i></p> <p><i>Note: Where application is made for establishment of two or more uses on the same premises, the parking demand is calculated by totalling the requirements for each use.</i></p>	<p><b>Complies:</b> Table 9.4.6:3 specifies a rate of parking for short term accommodation of one (1) space per unit plus 50% of the requirement for each ancillary use. The proposed development does not include ancillary land uses. The proposed development involves reuse of an existing dwelling that is intended to be rented as a whole and has a double carport at the front intended for guest parking. Accordingly, the proposed development provides appropriate parking.</p>

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Transport Network</b>		
PO <sub>1</sub> The development is located on roads that are appropriate for the nature of traffic generated, having regard to the safety and efficiency of the transport network, and the functions and characteristics identified in the transport network hierarchy contained in SC 6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	No acceptable outcome is nominated.	<b>Complies:</b> The proposed development involves short term accommodation within an existing dwelling which is of a residential scale. The proposed development is located on both a distributor and local road and accordingly, is located on roads that will be sufficient for the nature of the traffic that will be generated.
PO <sub>2</sub> Development does not compromise the orderly provision or upgrading of the transport network.	No acceptable outcome is nominated.	<b>Complies:</b> The proposed development does not involve new building work or other activities that would compromise the orderly provision or upgrading of the transport network.
PO <sub>3</sub> Onsite transport network infrastructure (including roads, parking, access and public transport, pedestrian and cyclist facilities) appropriately integrates with surrounding networks and facilitates the orderly development of adjoining land.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve any changes to existing on-site transport network infrastructure.
PO <sub>4</sub> Development is designed to encourage travel by public transport, walking and cycling. This may include integrated access between adjoining sites and/or the provision of mid-block connections which are safe, functional and legible for potential users.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development does not involve integrated access between sites or mid-block connections.
PO <sub>5</sub> Car parking areas, pathways and other elements of transport network infrastructure are designed to enhance public safety by discouraging crime and anti-social behaviour, having regard to:	AO <sub>5.1</sub> Car parking areas, pathways and other elements of transport network infrastructure are designed in accordance with <i>Crime Prevention Through Environmental Design (CPTED) Guidelines</i> .	<b>Complies:</b> The proposed development has considered CPTED principles where possible.

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(a) provision of opportunities for casual surveillance;</li> <li>(b) provision of lighting;</li> <li>(c) the use of fencing to define public and private spaces, whilst allowing for appropriate sightlines;</li> <li>(d) minimising potential concealment points and assault locations;</li> <li>(e) minimising opportunities for graffiti and other vandalism; and</li> <li>(f) restricting unlawful access to buildings and between buildings.</li> </ul>		
PO <sub>6</sub> Directional signage is provided within a development site to assist legibility and way-finding, including for pedestrians and cyclists.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development is not of a scale or nature to warrant provision of directional signage.
<b>Access</b>		
PO <sub>7</sub> Vehicle access arrangements and queuing areas are appropriate for: <ul style="list-style-type: none"> <li>(a) the capacity of the parking area;</li> <li>(b) the volume, frequency and type of vehicle usage; and</li> <li>(c) the function and characteristics of the access road and adjoining road network.</li> </ul>	AO <sub>7.1</sub> Access driveways and queuing areas are located and designed in accordance with the provisions of <i>Australian Standard AS 2890.1 Part 1: Off Street Carparking</i> .	<b>Performance Solution:</b> The proposed development involves short term accommodation within an existing dwelling with existing vehicle access arrangements. There are no changes proposed to the existing arrangements. The existing arrangements are sufficient for the nature and scale of the use.
PO <sub>8</sub> Access arrangements minimise any adverse impact on: <ul style="list-style-type: none"> <li>(a) the integrity of any infrastructure within the road reserve;</li> <li>(b) the safety and convenience of pedestrians and cyclists;</li> </ul>	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development does not involve new vehicle access arrangements or changes to the existing vehicle access arrangements.

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(c) the safety and convenience of access to adjacent properties;</li> <li>(d) the amenity of premises in the vicinity; and</li> <li>(e) street trees in the road reserve.</li> </ul>		
PO <sub>9</sub> Where the nature of the proposed development creates a demand due to the frequency and volume of vehicle movements for the set-down and pick-up of passengers, provision is made for set-down and pick-up facilities by bus, taxis or private vehicle.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development does not involve provision of set-down or pick-up facilities.
PO <sub>10</sub> Where set-down and pick-up facilities for bus, taxis or private vehicles are provided as part of development they are: <ul style="list-style-type: none"> <li>(a) safe for pedestrians, cyclists and vehicles;</li> <li>(b) conveniently connected to the main component of the development by pedestrian pathway; and</li> <li>(c) designed to provide for pedestrian priority and clear sightlines.</li> </ul>	AO <sub>10.1</sub> Bus pick-up/set-down areas: <ul style="list-style-type: none"> <li>(a) allow a bus, based on the Long Rigid Bus (12m) in Austroads/Standards Australia HB72 – Design Vehicles and Turning Path Templates, to turn and manoeuvre in and out of the area in an easy and safe manner;</li> <li>(b) afford maximum safety for passengers boarding or alighting buses;</li> <li>(c) avoid standing or queuing buses from obstructing access to car parking spaces or circulation within the Site; and</li> <li>(d) avoid on-street queuing or boarding/alighting of buses that would reduce traffic flow or safety on the road network. One clear traffic lane in each direction should be maintained.</li> </ul> AO <sub>10.2</sub> Car and taxi pick-up/set-down areas: <ul style="list-style-type: none"> <li>(a) allow a car to manoeuvre in and out of the area in an easy and safe manner;</li> </ul>	<b>N/A:</b> The proposed development does not involve provision of set-down or pick-up facilities.

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Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(b) afford maximum safety for passengers boarding or alighting cars;</li> <li>(c) avoid standing or queuing cars from obstructing access to car parking spaces or circulation within the site; and</li> <li>(d) avoid on-street queuing or boarding/alighting of cars that would reduce traffic flow or safety on the road network. One clear traffic lane in each direction should be maintained.</li> </ul>	
<b>Pedestrian and Cycle Facilities</b>		
PO <sub>11</sub> Provision is made for the safe and convenient movement of pedestrians on site and external to the site, having regard to desire lines, legibility, weather protection and the needs of people with disabilities.	AO <sub>11.1</sub> Pedestrian pathways and crossings are provided in accordance with SC6.2 PSP No.2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .  AO <sub>11.2</sub> Access for cyclists and pedestrians is clearly distinguished from vehicle access.  AO <sub>11.3</sub> Pedestrian paths of a minimum width of 1.5m are provided through each car parking row and connect to the main entrance(s) to the building(s).	<b>N/A:</b> The proposed development does not involve provision of pedestrian pathways and crossings.  <b>Complies:</b> The proposed development involves existing access for cyclists and pedestrians which is clearly distinguished from vehicle access.  <b>N/A:</b> The proposed development does not involve provision of pedestrian pathways and crossings.
PO <sub>12</sub> Provision is made for safe and convenient cycle movement to the site and within the site having regard to desire lines, users' needs and legibility.	AO <sub>12.1</sub> Shared paths and on-road cycle lane facilities are provided in accordance with SC6.2 PSP No.2 – <i>Engineering Standards Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development does not involve provision of shared paths and on-road cycle lane facilities.
<b>Parking and Circulation</b>		
PO <sub>13</sub> Car parking areas are designed to be: (a) clearly defined, marked and signed;	AO <sub>13.1</sub> The entry to the car park is clearly signposted.	<b>N/A:</b> The proposed development does not involve provision of carparking entry signage.

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(b) convenient, safe and accessible; and            (c) safe for vehicles, pedestrians and cyclists and minimise vehicle/pedestrian conflicts by providing clear access lines for pedestrians movement within car park areas.</p>	<p>AO<sub>13.2</sub> Parking spaces are freely available for use by the development's occupants and visitors during the business hours of the use.</p> <p>AO<sub>13.3</sub> Visitor or customer parking spaces are located in the most accessible position to the main entrance of the building and signed as such.</p> <p>AO<sub>13.4</sub> Unless otherwise specified in another code relevant to the development, 60% of the parking spaces for non-residential development are clearly visible from the street.</p> <p>AO<sub>13.5</sub> Public Safety:</p> <ul style="list-style-type: none"> <li>(a) The car park is located where it can be monitored by passers-by or the occupiers of the development.</li> <li>(b) Where the car park is open to the public at night, lighting is provided throughout the car park and along pedestrian access paths in compliance with Australian Standard AS 1158.3.1 – Road Lighting – Pedestrian Area (Category P) Lighting – Performance and Installation Design Requirements.</li> <li>(c) Except in the case of residential development:               <ul style="list-style-type: none"> <li>(i) where the car park is not required at night, entry to the car park is physically restricted; and</li> <li>(ii) where the car park is enclosed, the walls are finished in a light coloured material that reflects light.</li> </ul> </li> <li>(d) Landscaping throughout the car park is provided in a manner, as indicated in the</li> </ul>	<p><b>Complies:</b> The proposed development involves parking spaces that will be freely available for use by guests.</p> <p><b>N/A:</b> The proposed development does not involve visitor or customer parking spaces.</p> <p><b>N/A:</b> The proposed development does not involve non-residential development.</p> <p><b>N/A:</b> The proposed development does not involve a public carpark.</p>

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	Landscaping Code that allows surveillance and minimises the risk of crime.	
PO <sub>14</sub> Car parking areas are designed to provide spaces which meet the needs of people with disabilities.	AO <sub>14.1</sub> Parking spaces for people with disabilities are provided at the rates specified in Appendix C of Australian Standard <i>AS2890.1 Part 1: Off Street Carparking</i> . AO <sub>14.2</sub> Car parking spaces for people with disabilities are located as near as possible to the entrance or entrances of the facility or use they serve. AO <sub>14.3</sub> Parking spaces for people with disabilities are designed in accordance with the provisions of Australian Standard <i>AS2890.1 Part 1: Off Street Carparking</i> . AO <sub>14.4</sub> Pathways and ramps between parking areas and the entrances to buildings are designed in accordance with the provisions of Australian Standard <i>AS1428.1: Design for Access and Mobility</i> . AO <sub>14.5</sub> Parking spaces for people with disabilities are identified by a sign incorporating the International Symbol specified in Australian Standard <i>AS1428.1: Design for Access and Mobility</i> . AO <sub>14.6</sub> The sign is readily visible from a vehicle at the entrance to the carpark, or guide signs are provided to indicate the direction of the disabled parking spaces.	<b>N/A:</b> The proposed development does not involve specific provision of parking spaces for people with disabilities.
PO <sub>15</sub> Car parking areas for non-residential development on a site in, or adjoining, a residential zone, are designed to minimise any	For non-residential development on a site in, or adjoining, a residential zone:	<b>N/A:</b> The proposed development does not involve non-residential development.

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>adverse impact on the amenity of premises in the vicinity.</p>	<p>AO<sub>15.1</sub> Car parking and driveway areas are setback a minimum distance of 3m from a side boundary that is common with a residential use in a residential zone.</p> <p>AO<sub>15.2</sub> Landscape planting is used between the car park and driveway areas and the side boundary to soften the visual impacts of car park areas and to provide shade.</p> <p>AO<sub>15.3</sub> An acoustic fence of 1.8m height is provided along the property boundary that is common with a residential use in a residential zone.</p>	
<p>PO<sub>16</sub> Car parking and associated access arrangements are located and designed to avoid dominating the road frontage of the site or otherwise detracting from streetscape character.</p>	<p>AO<sub>16.1</sub> Car parking is provided either at the rear of the development or beneath buildings.</p>	<p><b>Performance Solution:</b> The proposed development involves short term accommodation within an existing dwelling with existing vehicle access arrangements. The proposed development partially involves car accommodation beneath the dwelling. The proposed development does not involve changes to the existing car accommodation arrangements and will not involve any changes to the existing streetscape character.</p>
<p>PO<sub>17</sub> Above ground or multi-level car parking areas are designed, articulated and finished to make a positive contribution to the local streetscape character.</p>	<p>AO<sub>17.1</sub> Above ground or multi-level parking areas are designed, articulated and finished to a quality equal to or better than adjoining buildings.</p>	<p><b>N/A:</b> The proposed development does not involve above ground or multi-level car parking areas.</p>
<p>PO<sub>18</sub> Landscaping is provided to soften the visual impact of car parking areas and to provide shading and protection from glare.</p>	<p>AO<sub>18.1</sub> Aesthetics, glare, heat absorption and re-radiation.</p> <p>(a) Landscaping is provided throughout the car park in the manner and at the rate indicated in the Landscaping Code; and</p> <p>(b) Unless otherwise specified in a zone, precinct or use code, where the car park</p>	<p><b>N/A:</b> The proposed development does not involve provision of landscaping.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	adjoins a street frontage, or a boundary with a Residential or other sensitive land use, a landscaped strip of minimum 3 m width is provided along the frontage/boundary.	
PO <sub>19</sub> Any parking, access and any other vehicle access/manoeuvring areas incorporate design measures to avoid dust nuisance to surrounding properties.	AO <sub>19.1</sub> Car parking, access and any other vehicle access/manoeuvring areas vehicle manoeuvring areas are imperviously sealed.	<b>Complies:</b> The proposed development involves an existing sealed vehicle access and parking area.
PO <sub>20</sub> Noise impacts from vehicle movement areas on any adjoining residential or other sensitive land use are mitigated.	AO <sub>20.1</sub> A solid, good quality brick, timber or masonry fence of a minimum 1.8m height is constructed between any vehicle movement areas and a boundary to an adjoining residential or other sensitive land use.	<b>N/A:</b> The proposed development involves existing vehicle access arrangements and fencing. The proposed development is of a similar scale and nature to the surrounding area and is not likely to create excessive noise impacts from vehicle movements.
PO <sub>21</sub> Any part of the parking area designated as a vehicle cleaning or repair area is designed and constructed to avoid adverse impact on water quality or Council's wastewater or stormwater infrastructure.	AO <sub>21.1</sub> The development is capable of meeting the requirements of Council's <i>Trade Waste Policy</i> and the Trade Waste Environmental Management Plan.	<b>N/A:</b> The proposed development does not involve generation or disposal of trade waste.
<b>Servicing</b>		
PO <sub>22</sub> Provision is made for the on-site loading, unloading, manoeuvring and access by service vehicles that: (a) is adequate to meet the demands generated by the development; (b) is able to accommodate the design service vehicle requirements; and (c) does not unduly impede vehicular, cyclist and pedestrian safety and convenience within the site.	AO <sub>22.1</sub> The service bays provided and access to them, can accommodate, at any one time, the types and numbers of service vehicles detailed in Table 9.4.6:3.	<b>Complies:</b> Table 9.4.6:3 specifies a service vehicle provision rate of one (1) SRV. The proposed development involves existing car accommodation which can accommodate an SRV. Notwithstanding, the proposed development will involve a cleaning/laundry service that only involves a passenger vehicle attending the site between bookings. Accordingly, provision has been made for sufficient on-site loading, unloading, manoeuvring and access.

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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>22.2</sub> Service bays provided wholly or partly within a building are physically separated from the rest of the buildings floor space in manner that makes it impractical to use them as storage or work areas.</p> <p>AO<sub>22.3</sub> The design and provision of access driveways, manoeuvring areas and loading and unloading facilities for service vehicles complies with Australian Standard AS 2890.2 – 1989 – <i>Off Street Parking – Commercial Vehicle Facilities</i>.</p> <p>AO<sub>22.4</sub> Vehicles being loaded or unloaded with goods stand completely on-site and do not impede access to more than 6 parking spaces or 50% of the on-site parking spaces (whichever is the lesser) while doing so.</p> <p>AO<sub>22.5</sub> Service vehicles can enter and leave the site in a forward gear.</p>	<p><b>N/A:</b> The proposed development does not involve service bays wholly or partly within a building.</p> <p><b>N/A:</b> The proposed development does not involve commercial vehicle facilities.</p> <p><b>Complies:</b> The proposed development involves sufficient space on-site for the expected type of service vehicle to load or unload.</p> <p><b>Performance Solution:</b> The proposed development involves short term accommodation within an existing dwelling. The proposed development is of a scale similar to the surrounding residential area and will not involve service vehicles larger than a passenger vehicle. The proposed development will utilise existing vehicle access arrangements which are appropriate for the nature of service vehicles expected.</p>
PO <sub>23</sub> Refuse collection vehicles are able to access on-site refuse collection facilities.	AO <sub>23.1</sub> Where an on-site refuse area is provided, access and manoeuvring areas are designed and provided to enable access by refuse collection vehicle based on the Design Service Vehicle in Austroads/Standards Australia HB72 – Design Vehicles and Turning Path Templates.	<b>N/A:</b> The proposed development does not involve on-site refuse collection.

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>24</sub> Servicing arrangements minimise any adverse impact the amenity of premises in the vicinity.</p>	<p>No acceptable outcome is nominated.</p>	<p><b>Complies:</b> The proposed development involves short term accommodation within an existing dwelling. The proposed development is of a scale similar to the surrounding residential area and will not involve service vehicles larger than a passenger vehicle. The proposed development will utilise existing vehicle access arrangements which are appropriate for the nature of service vehicles expected.</p>
<p>PO<sub>25</sub> Servicing arrangements are located and designed to avoid dominating the road frontage of the site or otherwise detracting from streetscape character.</p>	<p>AO<sub>25.1</sub> Areas used for servicing are not located at the front of developments, or are otherwise screened to minimise visual intrusion in the streetscape.</p>	<p><b>Performance Solution:</b> The proposed development involves short term accommodation within an existing dwelling. The proposed development is of a scale similar to the surrounding residential area and will not involve service vehicles larger than a passenger vehicle. The proposed development will utilise existing vehicle access arrangements which are appropriate for the nature of service vehicles expected.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
<b>Utilities</b>		
<p>PO<sub>1</sub> A water supply is provided that is adequate for the current and future needs of the intended use.</p>	<p>AO<sub>1.1</sub> Where within a water supply area, the development is connected to Council's reticulated water supply system in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>OR</p> <p>AO<sub>1.2</sub> Where not in a water supply area, the development is provided with an on site water supply in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>AO<sub>1.3</sub> Water supply systems and connections are designed and constructed in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p>	<p><b>Complies:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve any changes to the existing reticulated water connection.</p>
<p>PO<sub>2</sub> Wastewater treatment and disposal is provided that is appropriate for the level of demand generated, protects public health and avoids environmental harm.</p>	<p>AO<sub>2.1</sub> Where within a wastewater area, the development is connected to the Council's reticulated wastewater system in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>OR</p> <p>AO<sub>2.2</sub> Where not within a wastewater area, on-site wastewater treatment and disposal is provided which complies with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>AO<sub>2.3</sub> Waste water systems and connections are designed and constructed in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p>	<p><b>Complies:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve any changes to the existing reticulated wastewater connection.</p>

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PO <sub>3</sub> The development is equipped with an adequate energy supply approved by and installed in accordance with the standards of the relevant energy regulatory authority.	AO <sub>3.1</sub> Premises are connected to an electricity supply approved by the relevant energy regulatory authority.	<b>Complies:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve any changes to the existing electricity connection.
PO <sub>4</sub> Premises are connected to a telecommunications service approved by the relevant telecommunication regulatory authority.	AO <sub>4.1</sub> The development is connected to telecommunications infrastructure in accordance with the standards of the relevant regulatory authority.	<b>Complies:</b> The proposed development involves short term accommodation within an existing dwelling and does not involve any changes to the existing telecommunications connection or infrastructure.
PO <sub>5</sub> Provision is made for future telecommunications services (e.g. fibre optic cable).	AO <sub>5.1</sub> Conduits are provided in accordance with <i>SC6.2 PSP No.2 Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development does not involve provision of conduits.
PO <sub>6</sub> Development near utility services does not: (a) adversely affect the function of the service; or (b) place an additional load on the service; and (c) protects the infrastructure form physical damage; and (d) allows ongoing necessary access for maintenance purposes.	AO <sub>6.1</sub> Setbacks and loadings comply with <i>the Queensland Development Code QDC MP1.4</i> .	<b>N/A:</b> The proposed development does not involve development near utility services.
PO <sub>7</sub> Infrastructure is integrated with and efficiently extends existing networks.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development does not involve provision of new infrastructure.
PO <sub>8</sub> Water meter/s are installed and located for easy access by the relevant authority.	AO <sub>8.1</sub> Water meter/s are installed in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i> .	<b>N/A:</b> The proposed development does not involve provision of a new water meter.
<b>Movement Networks</b>		
PO <sub>9</sub> Premises are provided with the following works along the full extent of the road frontage and to a standard that is appropriate to the function of the road and the character of the locality:	AO <sub>9.1</sub> Design and construction of external road works are undertaken in accordance with <i>SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development does not involve external road works.

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<ul style="list-style-type: none"> <li>(a) appropriate roadway treatment;</li> <li>(b) appropriate pavement edging (including kerb and channel);</li> <li>(c) safe vehicular access;</li> <li>(d) safe footpaths and bikeways;</li> <li>(e) street scaping or street tree planting;</li> <li>(f) stormwater drainage; and</li> <li>(g) street lighting systems.</li> </ul>	AO <sub>9.2</sub> Footpaths and bikeways are provided in accordance with the <i>Austrroads Guide to Road Design – Part 6A: Pedestrian and Cyclist Paths (Austrroads 2009m)</i> .	<b>N/A:</b> The proposed development does not involve provision of footpaths and bikeways.
PO <sub>10</sub> Provision is made in the road reserve for street scaping, pedestrians and cyclists in a manner consistent with: <ul style="list-style-type: none"> <li>(a) the current and projected level of usage;</li> <li>(b) the desired streetscape character; and</li> <li>(c) activities which are anticipated to occur within the verge.</li> </ul>	AO <sub>10.1</sub> Street scaping works, footpaths and cycle paths are provided in accordance with <i>PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure</i> . AO <sub>10.2</sub> Footpaths and bikeways are provided in accordance with the <i>Austrroads Guide to Road Design – Part 6A: Pedestrian and Cyclist Paths (Austrroads 2009m)</i> .	<b>N/A:</b> The proposed development does not involve street scaping works, footpaths or cycle paths.
PO <sub>11</sub> Parking areas are constructed in a manner that is sufficiently durable for the intended function, maintains all-weather access and ensures the safe passage of vehicles, pedestrians and cyclists.	AO <sub>11.1</sub> Parking area design and construction is undertaken in accordance with the Transport, Access and Parking Code.	<b>N/A:</b> The proposed development does not involve provision of new parking areas.
PO <sub>12</sub> Movement networks can be easily and efficiently maintained.	AO <sub>12.1</sub> Infrastructure is provided in accordance with <i>SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>N/A:</b> The proposed development does not involve provision of new movement networks.
<b>Vehicular Access – Non-residential</b>		
PO <sub>13</sub> Non-residential vehicular access arrangements to a public roadway, including driveway crossovers:	AO <sub>13.1</sub> Non-residential vehicular access and driveway crossovers to a public road are not: <ul style="list-style-type: none"> <li>(a) an additional site/property access;</li> <li>(b) to a State-controlled Road or a road with bluestone kerbing;</li> </ul>	<b>N/A:</b> The proposed development does not involve a non-residential vehicle crossover.

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Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(a) are safe and do not compromise the efficiency, function, convenience of use or capacity of the road network;</li> <li>(b) are located and designed to:               <ul style="list-style-type: none"> <li>(i) avoid damage to utility services, pathways, krebs, road pavement and seal and other council/public infrastructure;</li> <li>(ii) prevent conflict between vehicles and cyclists and pedestrians; and</li> <li>(iii) avoid damage to existing street trees and retain space for the future planting of street trees within the verge.</li> </ul> </li> <li>(c) minimise the number of vehicular access per site to avoid the loss of on-street carparking spaces, and are appropriately separated from other vehicular accesses and side property boundaries to prevent interference with:               <ul style="list-style-type: none"> <li>(i) the safety, capacity and operations of the existing or planned road network;</li> <li>(ii) any existing vehicular accesses;</li> <li>(iii) adjoining properties; and</li> <li>(iv) cycleways or pedestrian footpaths and their users.</li> </ul> </li> <li>(d) protect the amenity of premises in the vicinity and surrounding streetscape by:               <ul style="list-style-type: none"> <li>(i) use of materials which integrate with the streetscape (e.g. existing crossovers and driveways, etc);</li> <li>(ii) minimising the width and grade of the access; and</li> <li>(iii) minimising impacts on the appearance of the streetscape by retaining existing</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>(c) within 25m of a signalised road intersection;</li> <li>(d) within 20m of an unsignalised road intersection in a Commercial or Industrial Area;</li> <li>(e) within 10m of an unsignalised road intersection in a Community, Residential, Rural or Other Area;</li> <li>(f) for a lot with a frontage of 10m or less;</li> <li>(g) within 1m of any infrastructure, including street signage, power poles, street lights, manholes, stormwater gully pits, or other Council/public utility asset; and</li> <li>(h) within the Tree Protection Zone, as defined by <i>Australian Standard 4970-2009</i>.</li> </ul> <p><i>Note: An additional site access is considered to be more than one site access.</i></p> <p>AO<sub>13.2</sub> Non-residential vehicular access and driveway crossovers:</p> <ul style="list-style-type: none"> <li>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</li> <li>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the road reserve;</li> <li>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, loading</li> </ul>	<p><b>N/A:</b> The proposed development does not involve a non-residential vehicle crossover.</p>

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vegetation, including approved landscaping.	bay, bus stop, or other structure within the road reserve; (d) do not require any change to existing footpath/verge profiles, including table drains (where relevant); (e) do not have access restricted by an access restriction strip or link reserve; (f) do not access an unformed or unkerbed road; (g) are constructed from reinforced concrete; (h) are perpendicular to the road edge; (i) retain space for the planting of street trees at a minimum rate of one (1) per lot frontage; and (j) are provided in accordance with the Australian Standard AS 2890.1 – Off Street Car Parking and Australian Standard AS 2890.2 (where relevant), the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure and Australian Standard AS 4970-2009 – Protection of Trees on Development Sites.	
<b>Vehicular Access – Residential</b>		
PO <sub>14</sub> Residential vehicular access arrangements, including driveway crossovers: (a) are appropriate for: (i) the capacity of the parking area; (ii) the volume, frequency and type of vehicle usage; and	AO <sub>14.1</sub> Residential vehicular access and driveway crossovers are not: (a) an additional site/property access; (b) to a State-controlled Road or a road with bluestone kerbing; (c) within 25 m of a signalised road intersection;	<b>Performance Solution:</b> The proposed development involves an existing vehicle crossover in proximity to infrastructure such as an electricity pole. The proposed development does not involve any changes to the existing crossover, as the existing crossover is appropriate for the scale and nature of the use.

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<ul style="list-style-type: none"> <li>(iii) the function and configuration of the access road.</li> <li>(b) minimise any potentially adverse impact on:               <ul style="list-style-type: none"> <li>(i) the safety and efficiency of the road and pedestrian/cycle paths;</li> <li>(ii) the safety and efficiency of the road and footpath users;</li> <li>(iii) the integrity of any infrastructure within the road reserve; and</li> <li>(iv) the safety of access to adjacent properties.</li> </ul> </li> <li>(c) protect the amenity of premises in the vicinity by:               <ul style="list-style-type: none"> <li>(i) maintaining the predominant vehicular access pattern in the street, including consistent width, grade and location;</li> <li>(ii) preserving the residential amenity of the streetscape, including noise and visual impact, and consideration of existing landscaping by considering:                   <ul style="list-style-type: none"> <li>(A) use of materials which integrate with the streetscape (e.g. existing crossovers and driveways, etc);</li> <li>(B) minimising the width and grade of the access;</li> <li>(C) minimising impacts on the appearance of the streetscape by retaining existing vegetation, including approved landscaping; and</li> <li>(D) locating the access to minimise the impact of vehicle noise on neighbouring/adjoining properties.</li> </ul> </li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>(d) within 20m of an unsignalised road intersection in a Commercial or Industrial Area;</li> <li>(e) within 10m of an unsignalised road intersection in a Community, Residential, Rural or Other Area;</li> <li>(f) within 1m of any infrastructure, including street signage, power poles, street lights, manholes, stormwater gully pits, or other Council/public utility asset;</li> <li>(g) within the Tree Protection Zone, as defined by Australian Standard 4970-2009;</li> <li>(h) for a lot with a frontage of 10m or less;</li> <li>(i) greater than 4m in width when for a lot with a frontage/width of more than 10m but less than 20m; and</li> <li>(j) greater than 6m in width when for a lot with a frontage/width of greater than 20m.</li> </ul> <p><i>Note: An additional site access is considered to be more than one site access.</i></p> <p>AO<sub>14.2</sub> Except where in a Rural Zone, residential vehicular access and driveway crossovers:</p> <ul style="list-style-type: none"> <li>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</li> <li>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the roadway;</li> </ul>	<p><b>N/A:</b> The proposed development does not involve any changes to the existing vehicle crossover.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, bus stop, or other structure within the road reserve;</p> <p>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</p> <p>(e) do not have access restricted by an access restriction strip or link reserve; or</p> <p>(f) do not access an unformed or unkerbed road;</p> <p>(g) are constructed from reinforced concrete;</p> <p>(h) are perpendicular to the road edge; and</p> <p>(i) are provided in accordance with the <i>Australian Standard AS 2890.1 – Off Street Car Parking and Australian Standard AS 2890.2 (where relevant), the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure and Australian Standard AS 4970-2009 –Protection of Trees on Development Sites.</i></p> <p>AO<sub>14.3</sub> Where in a Rural Zone, residential vehicular access and driveway crossovers:</p> <p>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</p> <p>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking</p>	<p><b>N/A:</b> The proposed development is not located in a Rural Zone.</p>

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	<p>bay, bus stop, or other structure within the roadway;</p> <p>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, loading bay, bus stop, or other structure within the road reserve;</p> <p>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</p> <p>(e) do not have access restricted by an access restriction strip or link reserve;</p> <p>(f) are sealed where accessing a sealed formed road;</p> <p>(g) are perpendicular to the road edge; and</p> <p>(h) are provided in accordance with the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure.</p>	
<b>Earthworks and Retaining Walls</b>		
PO <sub>15</sub> Earthworks result in stable landforms and structures.	AO <sub>15.1</sub> Earthworks and the construction of retaining walls and batters are undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>N/A:</b> The proposed development does not involve earthworks or construction of new retaining walls and batters.
PO <sub>16</sub> Earthworks do not result in the contamination of land or water and avoid risk to people and property.	AO <sub>16.1</sub> Earthworks are undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>N/A:</b> The proposed development does not involve earthworks or construction of new retaining walls and batters.
PO <sub>17</sub> Earthworks are undertaken in a manner that: (a) maintains natural landforms; (b) minimises height of retaining walls and batter faces;	AO <sub>17.1</sub> Earthworks and the construction of retaining walls and batters are undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>N/A:</b> The proposed development does not involve earthworks or construction of new retaining walls and batters.

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<p>(c) does not unduly impact on the amenity or privacy for occupants of the site or on adjoining land; and</p> <p>(d) does not unduly impact on the amenity of the streetscape.</p>		
<p>PO<sub>18</sub> Earthworks do not create or worsen any flooding or drainage problems on the site or on neighbouring properties.</p>	<p>AO<sub>18.1</sub> Earthworks and the construction of retaining walls and batters are undertaken in accordance with <i>SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure</i>.</p>	<p><b>N/A:</b> The proposed development does not involve earthworks or construction of new retaining walls and batters.</p>
<p>PO<sub>19</sub> Earthworks do not prevent access or create difficult access to the property.</p>	<p>AO<sub>19.1</sub> Earthworks and the construction of retaining walls and batters ensure driveways can be provided in accordance with <i>SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure</i>.</p>	<p><b>N/A:</b> The proposed development does not involve earthworks or construction of new retaining walls and batters.</p>
<p>PO<sub>20</sub> Earthworks do not cause a significant impact on the amenity of the locality or along routes taken to transport material as a result of truck volumes, dust or noise.</p>	<p>AO<sub>20.1</sub> Earthworks are undertaken in accordance with <i>SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure</i>.</p>	<p><b>N/A:</b> The proposed development does not involve earthworks or construction of new retaining walls and batters.</p>
<p>PO<sub>21</sub> The transportation of material minimises adverse impact on the road system.</p>	<p>AO<sub>21.1</sub> Material is transported in accordance with <i>SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure</i>.</p>	<p><b>N/A:</b> The proposed development does not involve earthworks or construction of new retaining walls and batters.</p>
<b>Waste Management</b>		
<p>PO<sub>22</sub> Where relevant, the development is capable of providing for the storage, collection, treatment and disposal of trade waste such that:</p>	<p>No acceptable outcome is nominated.</p>	<p><b>N/A:</b> The proposed development does not involve trade waste.</p>

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<ul style="list-style-type: none"> <li>(a) off-site releases of contaminants do not occur;</li> <li>(b) the health and safety of people and the environment are protected; and</li> <li>(c) the performance of the wastewater system is not put at risk.</li> </ul>		
<p>PO<sub>23</sub> Appropriate refuse container storage areas are provided which are:</p> <ul style="list-style-type: none"> <li>(a) in a building or enclosing structure or screened from public view;</li> <li>(b) of adequate size to accommodate the expected amount of refuse to be generated by the use;</li> <li>(c) in a position that is conveniently accessible for collection; and</li> <li>(d) able to be kept in a clean state at all times.</li> </ul>	<p>AO<sub>23.1</sub> Refuse container storage areas are provided which:</p> <ul style="list-style-type: none"> <li>(a) are in a building, outbuilding or other enclosed structure, or otherwise screened from public view, by a minimum 1.5 m high solid fence or wall or dense vegetation;</li> <li>(b) are provided with an imperviously sealed pad, on which to stand the bin(s), that is drained to an approved waste disposal system;</li> <li>(c) are within normal hose length of a hose cock;</li> <li>(d) are large enough to accommodate at least one (1) standard sized container per dwelling and, in commercial and industrial premises, one (1) or more industrial bins of a size appropriate to the nature and scale of use; and</li> <li>(e) are situated not closer than 6m to a road or 2m to any site boundary.</li> </ul> <p>AO<sub>23.2</sub> On sites greater than 2,000m<sup>2</sup> in area, provision is made for refuse collection vehicles to access the collection area and to enter and leave the site in a forward direction without having to make more than a 3-point turn.</p>	<p><b>Performance Solution:</b> The proposed development will involve storage of refuse containers consistent with the scale and nature of the use, and out of sight from the street where possible. The proposed development will involve relocation of container storage to the kerbside for collection every week by the property manager.</p> <p><b>N/A:</b> The proposed development is not located on land greater than 2,000m<sup>2</sup>.</p>

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	AO <sub>23.3</sub> For multiple dwelling and retirement facility, container storage areas are located not more than 50m from any dwelling.	<b>N/A:</b> The proposed development does not involve multiple dwellings or retirement facilities.
PO <sub>24</sub> Where the use is non-residential and generates recyclable waste, provision is made for conveniently located recycling bins on the premises, including in the refuse container storage area.	No acceptable outcome is nominated.	<b>N/A:</b> The proposed development does not involve a non-residential use.
<b>Construction Management</b>		
PO <sub>25</sub> Work is undertaken in a manner which minimises adverse impacts on vegetation that is to be retained.	AO <sub>25.1</sub> Works include, at a minimum: (a) installation of protective fencing around retained vegetation during construction; (b) erection of advisory signage; (c) no disturbance, due to earthworks or storage of plant, materials and equipment, of ground level and soils below the canopy of any retained vegetation; and (d) removal from the site of all declared noxious weeds and environmental weeds.	<b>N/A:</b> The proposed development does not involve new building work or construction.
PO <sub>26</sub> Work is undertaken in a manner which does not cause unacceptable impacts on surrounding areas as a result of dust, odour, noise or lighting.	AO <sub>26.1</sub> Construction is undertaken in accordance with the Environmental Standards Code.	<b>N/A:</b> The proposed development does not involve new building work or construction.
PO <sub>27</sub> While undertaking development works, the site and adjoining road are maintained in a tidy, safe and hygienic manner.	AO <sub>27.1</sub> Construction is undertaken in accordance with the Environmental Standards Code.	<b>N/A:</b> The proposed development does not involve new building work or construction.
PO <sub>28</sub> Traffic, parking and delivery of construction materials generated during construction are managed to minimise impact on the amenity of the surrounding area and to manage the safety of pedestrians, cyclists and motorists.	AO <sub>28.1</sub> Construction is undertaken in accordance with the Environmental Standards Code.	<b>N/A:</b> The proposed development does not involve new building work or construction.

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PO <sub>29</sub> Council and state infrastructure is not damaged by construction activities.	<p>AO<sub>29.1</sub> Construction, alterations and any repairs to infrastructure is undertaken in accordance with the <i>SC6.2 PSP No.2 Engineering Standards – Roads and Drainage Infrastructure, Queensland Development Code QDC MP1.4, and, where applicable, in consultation with the relevant service authority.</i></p> <p>AO<sub>29.2</sub> Construction, alterations and any repairs to State-controlled roads and rail corridors are undertaken in accordance with the <i>Transport Infrastructure Act 1994.</i></p>	<b>N/A:</b> The proposed development does not involve new building work or construction.