



## **DEVELOPMENT APPLICATION**

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Development Permit – Material Change of Use

# Multiple Dwelling

**9 Benjamin Street, Mount Lofty**

PLANNING REPORT

**APRIL 2026**

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Prepared For: Dung Le

Project Name: Multiple Dwelling

Project Reference: 2024-361

Date: 7 April 2026

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### Document History

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## EXECUTIVE SUMMARY

### SITE DETAILS

<b>Address of Site</b>	9 Benjamin Street, Mount Lofly
<b>Real Property Description</b>	Lots 0-2 BUP101164
<b>Area of Site</b>	1,023m <sup>2</sup>
<b>Road Frontage</b>	Benjamin Street
<b>Easements</b>	Easement A RP844642 (Burdens Lot 32 RP106237)
<b>Registered Owners</b>	Lot 0 BUP101164 - Body Corporate For Huntham Close Community Titles Scheme 17793 Lots 1-2 BUP101164 - Dung Thi Le

### PLANNING INSTRUMENTS

<b>Regional Plan</b>	Darling Downs and South East Queensland Regional Plans
<b>Regional Plan Designation</b>	Urban Footprint
<b>Planning Scheme</b>	<i>Toowoomba Regional Planning Scheme 2012</i>
<b>Zone</b>	Low Density Residential Zone
<b>Zone Precinct</b>	General Precinct
<b>Overlays</b>	Airport Environs Overlay
<b>Existing Use</b>	Dual Occupancy
<b>Current Approvals</b>	Nil (as available on Council's Development.i)

### APPLICATION OVERVIEW

<b>Application</b>	Development Application
<b>Approval Sought</b>	Development Permit
<b>Development Type</b>	Material Change of Use
<b>Land Use Definition</b>	Multiple Dwelling
<b>Level of Assessment</b>	Impact Assessable

Relevant Referral	Nil
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## PROPOSED DEVELOPMENT

Development Summary	Multiple Dwelling including: <ul style="list-style-type: none"> <li>• 2 x existing 2-bedroom dwellings</li> <li>• 1 x proposed 4-bedroom dwelling</li> </ul>
Gross Floor Area	Existing Units 1 & 2: As existing Proposed Dwelling Unit: 156.7m <sup>2</sup>
Building Footprint	Existing Units 1 & 2: Approx. 131m <sup>2</sup> (as existing) Proposed Dwelling Unit: 193.8m <sup>2</sup> <b>Total: Approx. 325m<sup>2</sup></b>
Building Site Cover	31.8% (approx.)
Building Setbacks	Existing Units 1 & 2: As existing Proposed Dwelling Unit: <ul style="list-style-type: none"> <li>• North-eastern Boundary: approximately 30m</li> <li>• Eastern Boundary: 1.5m</li> <li>• South-western Boundary: 1.5m</li> <li>• Western Boundary: 1.5m</li> </ul>
Building Height	Existing Units 1 & 2: As existing Proposed Dwelling Unit: 7.5m
Impervious Area	Approx. 425m <sup>2</sup> (41.54%)
Vehicular Access	Existing shared driveway to Benjamin Street
Carparking	4 spaces (3 enclosed & 1 open)

## APPLICANT DETAILS

Applicant	Dung Le c/- Precinct Urban Planning PO Box 3038 <b>TOOWOOMBA QLD 4350</b>
Contact Person	<b>James Williams</b> <b>Phone:</b> 07 4632 2535 <b>Mobile:</b> 0481 127 412 <b>Email:</b> <a href="mailto:james@precinctplan.com.au">james@precinctplan.com.au</a>
Our Reference	2024-361

## 1.0 INTRODUCTION

This report has been prepared in support of a Development Application for a Development Permit for Material Change of Use - Multiple Dwelling (3 units) on land at 9 Benjamin Street, Mount Lofty, described as Lots 0-2 BUP101164 (**the site**). The proposed development involves the start of a new use of the premises and accordingly comprises a Material Change of Use under Schedule 2 of the *Planning Act 2016* (**the Act**).

This town planning report addresses the merits of the development having regard to the provisions of the *Planning Act 2016* and the *Toowoomba Regional Planning Scheme 2012*. This planning report has been prepared on behalf of the applicant, Dung Le.

This Development Application for a Development Permit for Material Change of Use is Impact Assessable development under the *Toowoomba Regional Planning Scheme 2012*. The following report demonstrates the suitability of the proposal within the locality and its compliance with the relevant provisions of the *Toowoomba Regional Planning Scheme 2012*.

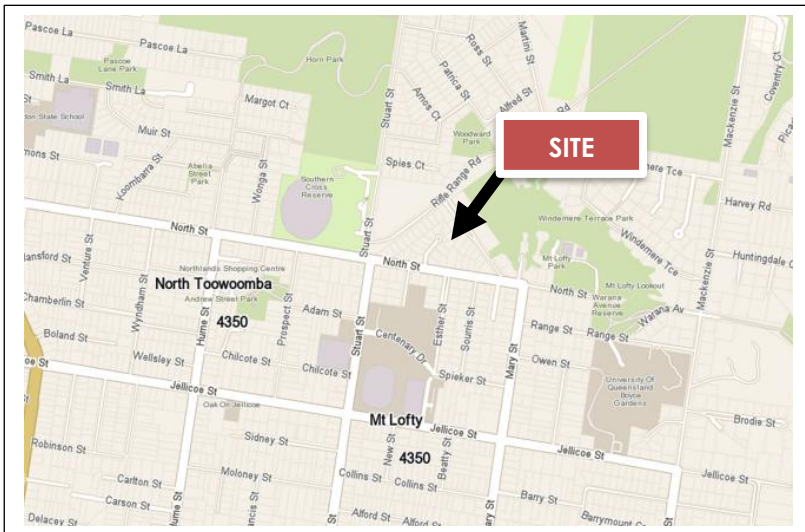
## 2.0 SITE AND LOCALITY

### 2.1 SITE

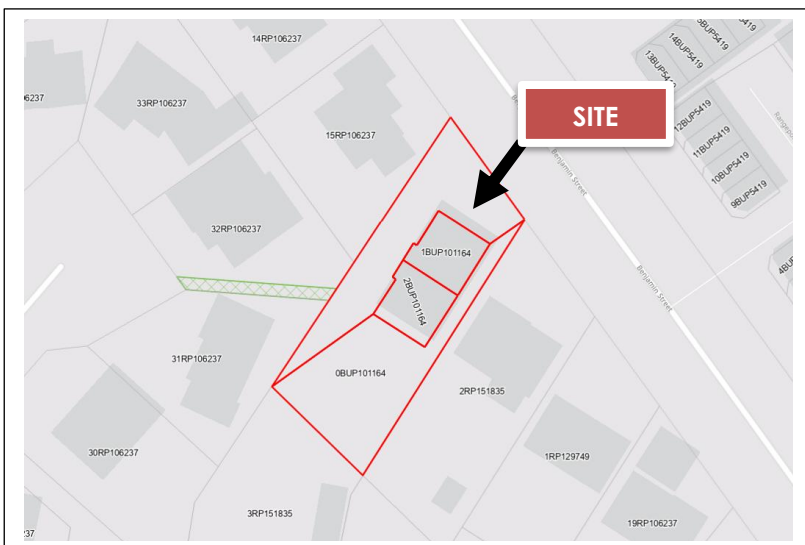
The site is located at 9 Benjamin Street, Mount Lofty, described as Lots 0-2 BUP101164 (**the site**). The site is located within the suburb of Mount Lofty and is approximately 2.5km north-east of the Toowoomba CBD. The location of the site in respect of the surrounding locality is illustrated in **Figure 1**.

The site is of irregular configuration comprising a strata titled premises with a total site area of 1,023m<sup>2</sup>. The site has sole frontage to Benjamin Street. The configuration of the site is illustrated in **Figure 2**.

**FIGURE 1 - LOCALITY PLAN**



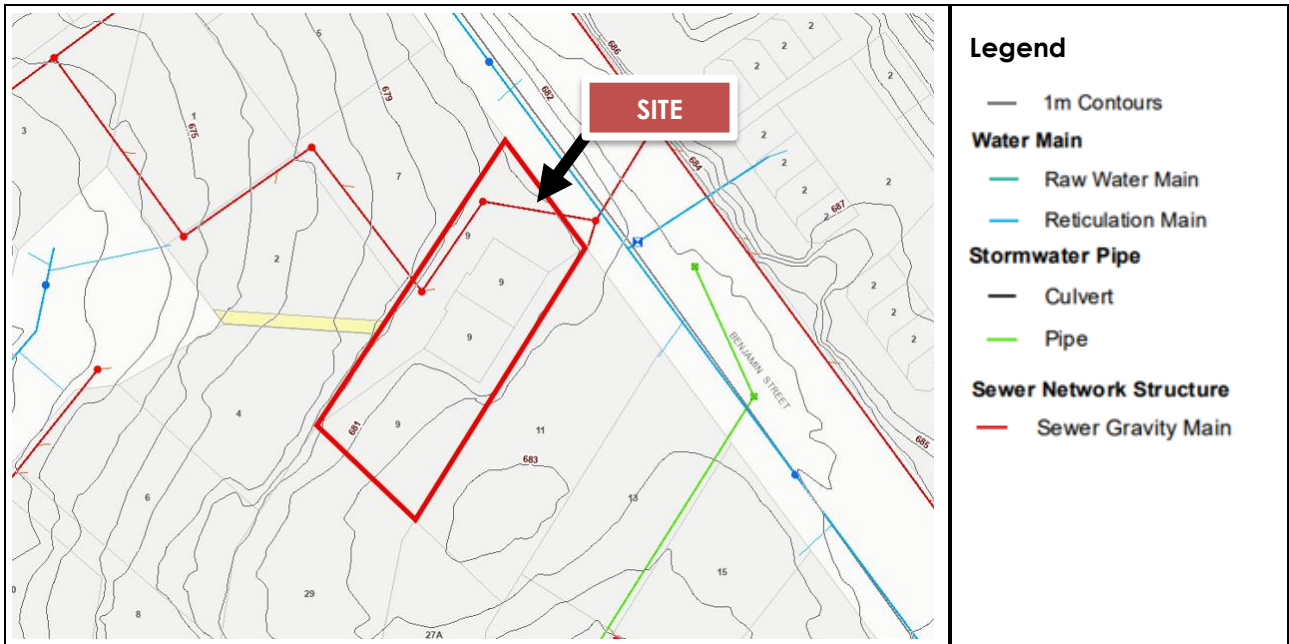
**FIGURE 2 - CADASTRAL PLAN**





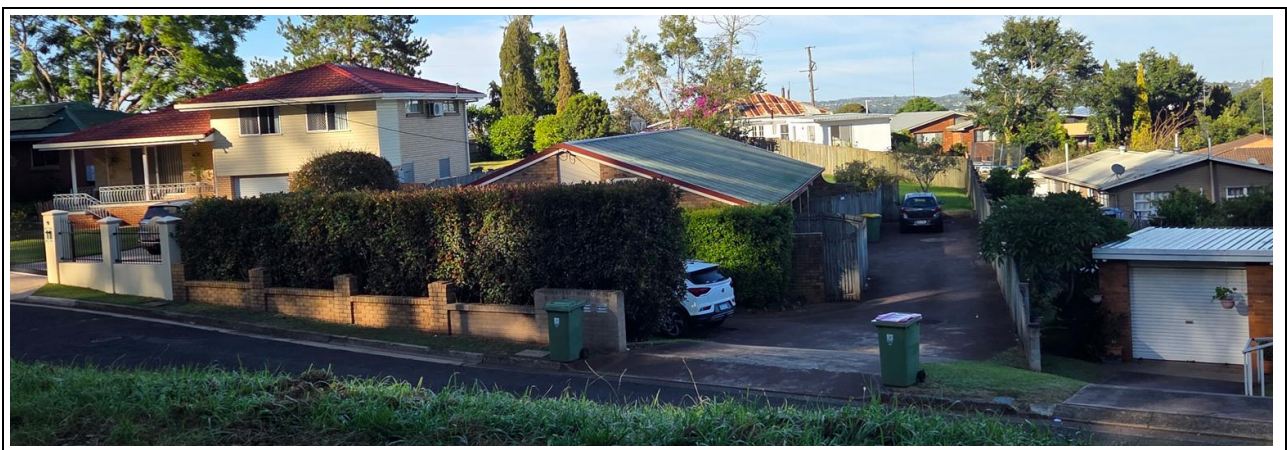
- The site is serviced by Council's reticulated water supply, sewerage and stormwater drainage infrastructure; refer to **Figure 4**. The site is also serviced by reticulated electricity and telecommunications supply networks.

**FIGURE 4 - CONTOUR AND INFRASTRUCTURE MAPPING**



**Figure 4** confirms that the site experiences a decline in land elevation between the eastern and western boundaries of the site. The figure also illustrates the locations of Council's reticulated water, sewerage and stormwater networks in relation to the site.

The features of the site are illustrated in **Photograph 1**.



**PHOTOGRAPH 1** - View of the site from Benjamin Street.

## 2.3 SURROUNDING LAND USE

### 2.3.1 SURROUNDING LOCALITY

The site is situated in a locality that is characterised by a mix of residential land uses. The scale and character of the built form in the locality is of a predominately low-rise residential character.

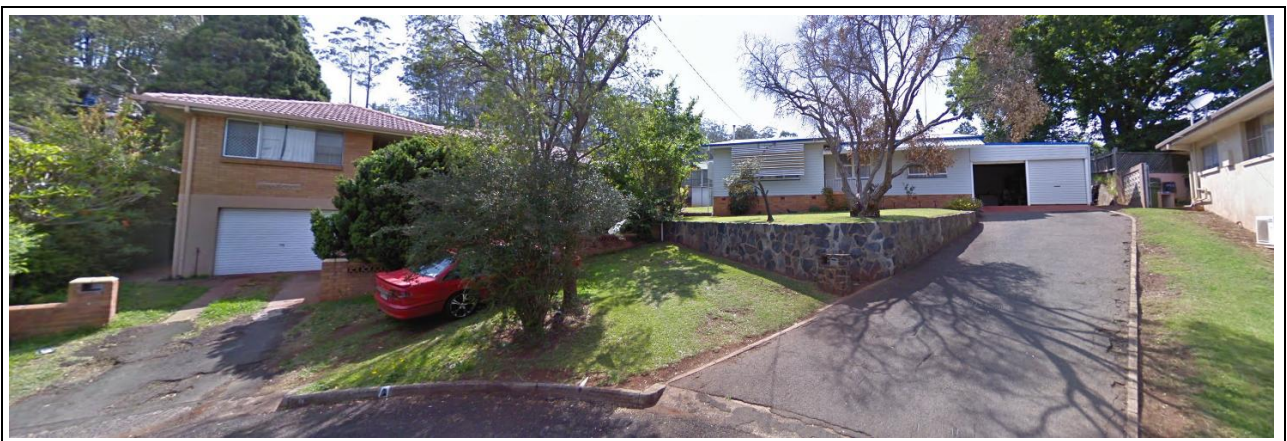
### 2.3.2 ADJOINING PROPERTIES

The nature and character of land use on surrounding properties may be summarised as follows:

- Land to the **north-west** of the site, located at 7 Benjamin Street and 2-4 Beck Street, comprises three (3) residential premises, each containing a dwelling house and ancillary structures.



PHOTOGRAPH 2 - View of the premises to the north of the site from Benjamin Street.



PHOTOGRAPH 3 - View of the premises to the north of the site from Benjamin Street.

- Land to the **south-east** of the site, located at 11 Benjamin Street, comprises a residential premises containing a Dwelling House and ancillary structures.



**PHOTOGRAPH 4** - View of the residential premises to the south-east of the site from Benjamin Street.

- Land to the **south-west** of the site, located at 29 North Street, comprises a residential premises containing a Dwelling House and ancillary structures.



**PHOTOGRAPH 5** - View of the residential premises to the south of the site from North Street.

- Land to the **north-east** of the site, located at 2 Benjamin Street, comprises a Multiple Dwelling (Rangepoint Apartments), which consists of approximately 39 apartments.



**PHOTOGRAPH 6** - View of the residential premises to the west of the site from Benjamin Street.

## 2.4 CURRENT DEVELOPMENT APPROVALS

The site does not currently benefit for any existing Development Permits as publicly available on Council's website.

### 3.0 PROPOSAL

This Development Application seeks approval for a Development Permit for Material Change of Use – Multiple Dwelling (3 units) on the site. The proposed development will seek to retain the existing attached dwelling units and establish an additional dwelling unit to the rear of the site, resulting in a Multiple Dwelling development (3 units).

Details regarding the proposed design and operation of the development are provided below.

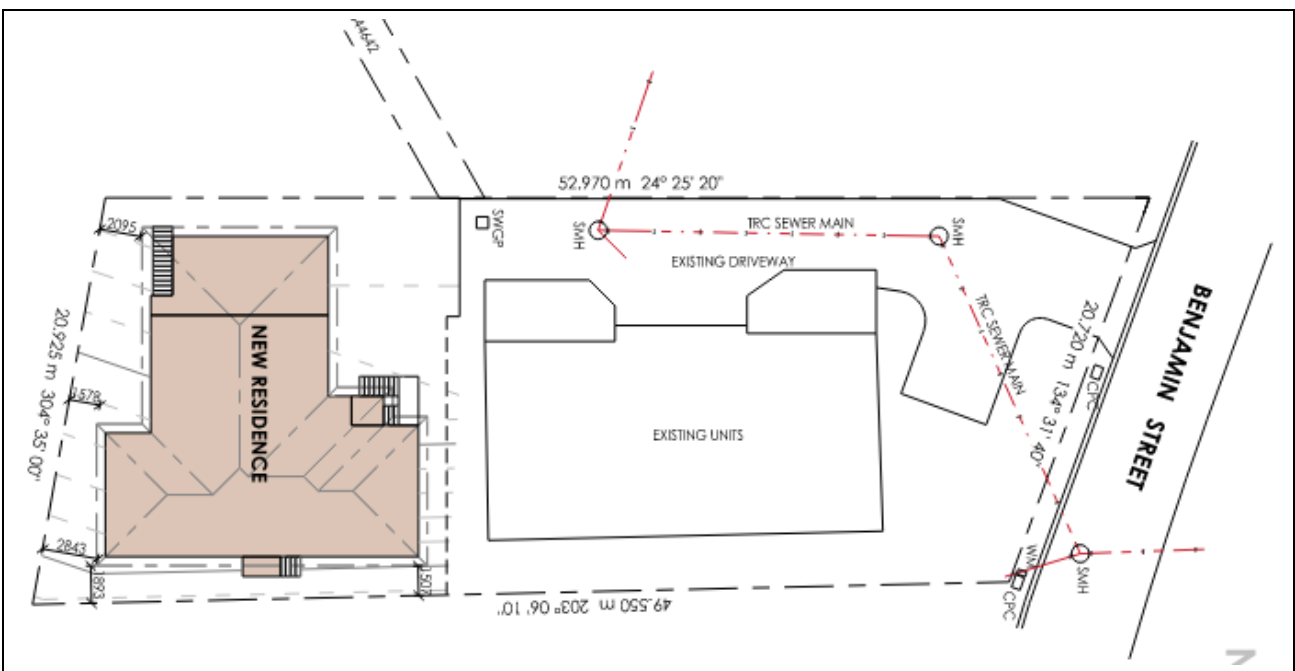
#### 3.1 SITE COMPOSITION AND LAYOUT

The proposed development will retain the existing two (2) bedroom units at the front of the site. Additionally, the development will involve the construction of a new four (4) bedroom dwelling at the rear of the site. This will result in a total of three (3) units across the site.

The proposed dwelling unit will have a gross floor area (GFA) of approximately 156.7m<sup>2</sup>, and a building footprint of approximately 193.8m<sup>2</sup>. This will result in a total building footprint across the site of approximately 325m<sup>2</sup> when considering the existing dwellings.

The proposed site layout will be generally in accordance with the proposed Site Plan, Project Number 250902, Sheet 2.02 - A, prepared by Justin Bowe Building, dated 8 September 2025, attached at **Appendix A** and reproduced as **Figure 5**.

**FIGURE 5 - EXTRACT OF PROPOSED SITE PLAN**



The proposed new dwelling unit will consist of four (4) bedrooms, two (2) bathrooms (including a private ensuite), laundry and open plan kitchen, living and dining area. The dwelling will also include a deck which will be directly accessible from the living area.

Building setback to site boundaries will correspond with the following:

- North-eastern Boundary (Benjamin Street frontage): approximately 30m
- Eastern Boundary: 1.5m
- South-western Boundary: 1.5m
- Western Boundary: 1.5m

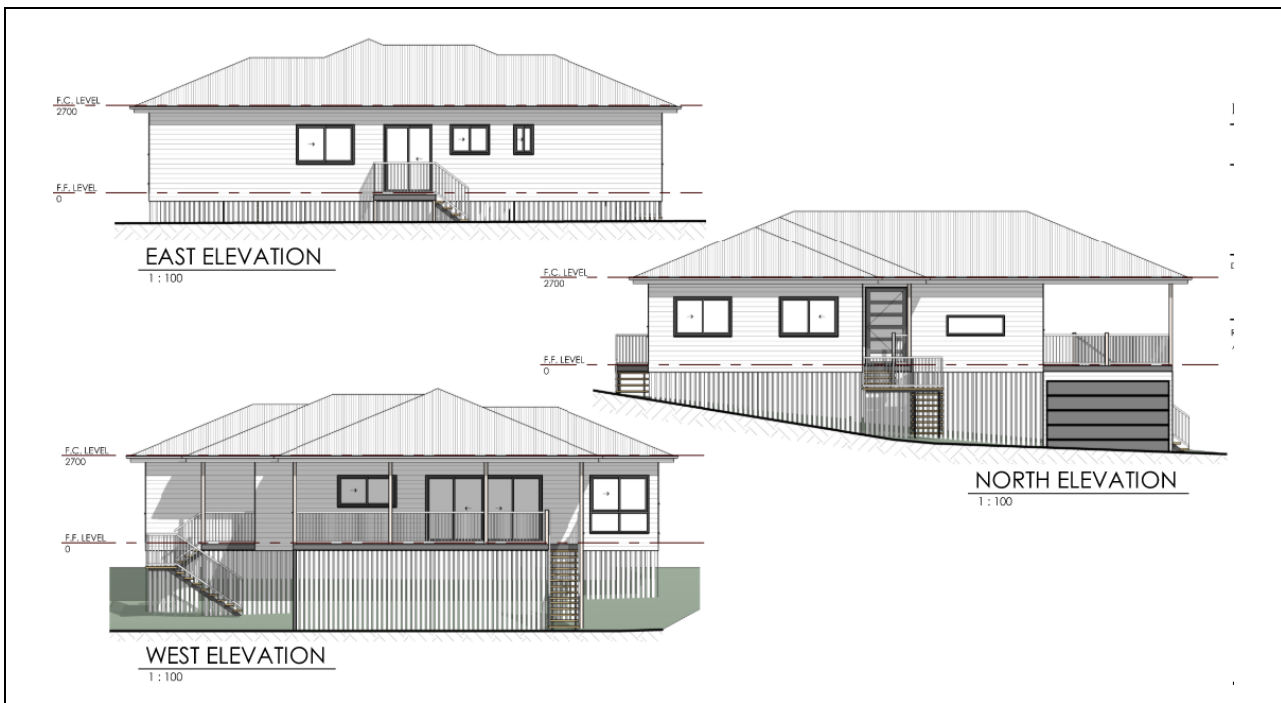
### 3.2 BUILT FORM AND CHARACTER

The built form and character of the development will be characterised by a consistent architectural theme to ensure visual compatibility and continuity is achieved. The development has been purposefully designed to be of contemporary residential scale and bulk compatible with typical low density residential use types.

Building materials will include weatherboard cladding, aluminium windows and doors and Colorbond sheet roofing. The proposed development will be of raised single storey construction with a maximum building height of approximately 7.5m from natural ground level. It is noted that the proposed dwelling unit is located at the rear of the site and will not be readily visible from Benjamin Street frontage.

The scale and appearance of development is to be generally in accordance with the Elevation Plans, Project Number 250902, Sheet 3.01 - A, prepared by Justin Bowe Building, dated 8 September 2025. The Elevation Plans are attached as **Appendix A**, and an excerpt of the plan is reproduced in **Figure 6**.

**FIGURE 6 - EXTRACTS OF ELEVATION PLANS**



### **3.3 ACCESS & CARPARKING**

All dwelling units will obtain access from Benjamin Street via the existing access arrangement, noting that the existing driveway will be extended to service the new dwelling unit.

The existing dwellings are currently provided with one (1) parking space per unit, plus one (1) visitor parking space at the front of the site. As there are no changes to how the two (2) existing dwellings will function, the existing single parking space per unit is considered suitable in this instance. The proposed new dwelling consists of four (4) bedrooms and accordingly is required to be provide with two (2) vehicle parking spaces. It is proposed that this dwelling will utilise a proposed new single garage as well as the unclosed parking space at the front of the site. This will result in a total of four (4) parking spaces across the site (2 for the new dwelling and 1 per existing dwelling), which is considered suitable in this instance.

### **3.4 LANDSCAPING**

Landscaping is proposed throughout the site to complement and soften the built-form, and to enhance aesthetic amenity. In this respect, each dwelling unit is provided with private open space areas at the rear of the dwellings. These areas are directly accessible from living areas within each of the dwellings.

### **3.5 INFRASTRUCTURE AND SERVICING**

All urban infrastructure networks are available to the land including Council's reticulated water supply, sewerage network, stormwater drainage, electricity and telecommunications services infrastructure.

## 4.0 PLANNING FRAMEWORK

### 4.1 THE PLANNING ACT 2016

The purpose of the *Planning Act 2016* (PA) is to establish an efficient, effective, transparent, integrated, coordinated, and accountable system of land use planning (planning), development assessment and related matters that facilitates the achievement of ecological sustainability.

The proposed development involves the start of a new use of the premises and accordingly comprises a Material Change of Use under Schedule 2 of the *Planning Act 2016* (**the Act**). A Development Permit for Material Change of Use must be obtained prior to the commencement of the use of premises. Assessment against the *Toowoomba Regional Planning Scheme 2012* indicates that this development application is Impact Assessable in accordance with Section 45(5) of the Act.

### 4.2 STATE PLANNING POLICY

Pursuant to the provisions of section 45(5) of the *Planning Act 2016*, the Assessment Manager, in considering an Impact Assessable Development Application, must assess the application in respect of the State Planning Policy.

The *State Planning Policy July 2017* (**SPP**) commenced on 3 July 2017 and replaces *State Planning Policy April 2016*. The SPP expresses the state's interests in land use planning and development and promotes these interests through plan making and development decisions of state and local government. The SPP applies, to the extent relevant, when:

- making or amending a local planning instrument;
- designating premises for infrastructure;
- making or amending a regional plan;
- development assessment by local government, if its planning scheme has not yet appropriately integrated the relevant SPP state interests policies; and
- an assessment manager or referral agency other than local government is assessing a development application.

The SPP addresses seventeen (17) state interests categorised under the following themes:

- (i) Liveable communities and housing.
- (ii) Economic growth.
- (iii) Environment and heritage.
- (iv) Safety and Resilience to Hazards.
- (v) Infrastructure.

A state interest is defined under Schedule 2 of the *Planning Act 2016* (the Act):

- (a) an interest that the Minister considers affects an economic or environmental interest of the State or a part of the State; or
- (b) an interest that the Minister considers affects the interest of ensuring this Act's purpose is achieved.

The proposed development has been assessed against the state interests included under *Part E: State Interest Policies and Assessment Benchmarks* of the SPP as follows:

#### 4.2.1 STATE INTEREST POLICIES AND ASSESSMENT BENCHMARKS

Part E of the SPP contains state interest policies and where relevant, the assessment benchmarks for each state interest. The assessment benchmarks for each relevant state interest are listed below. For each of these state interests, Part E of the SPP advises when the assessment benchmarks apply and if so, what matters the application must be assessed against, to the extent the SPP has not been identified in a local planning instrument as being appropriately integrated.

Part 2 of the *Toowoomba Regional Planning Scheme 2012* identifies the State interests under the State Planning Policy that have been integrated into the Planning Scheme. In this instance, the *Toowoomba Regional Planning Scheme 2012* confirms that some of the State interests relevant to the Toowoomba Regional Council region have been appropriately incorporated into the Planning Scheme.

The Planning Scheme incorporates the following State interest:

- Safety and Resilience to Hazards (as it relates to flood hazards).

Accordingly, where the proposed development achieves compliance with the assessment benchmarks of the Planning Scheme, the development is considered to comply with the provisions of the State Planning Policy as it relates to the above State interest. The development is therefore also required to be assessed against the assessment benchmarks under the State Planning Policy for the relevant State interests not integrated into the Planning Scheme. An assessment of each of the relevant State interests is provided below.

##### 1. Liveable Communities

These provisions apply to a development application in an urban area involving premises that is, or will be, accessed by common private title, for a material change of use or reconfiguring a lot where the application involves buildings (attached or detached) that are not covered by other legislation or planning provisions mandating fire hydrants.

Where these circumstances apply, the development:

- (i) ensures fire hydrants are installed and located to enable fire services to access water safely, effectively and efficiently;
- (ii) road widths, and construction within the development, are adequate for fire emergency vehicles to gain access to a safe working area close to buildings and near water supplies whether or not on-street parking spaces are occupied; and
- (iii) fire hydrants are suitably identified so that fire services can locate them at all hours.

In this instance, the proposed development does not involve any of the above circumstances, and accordingly, these provisions are not relevant to the assessment of this application.

##### 2. Mining and Extractive Resources

These provisions relate to the following developments:

- (1) Reconfiguring a Lot within a Key Resource Area (**KRA**);
- (2) A Material Change of Use within the resource/processing area of a KRA or the separation area for the resource/processing area of a KRA; or
- (3) A Material Change of Use within the transport route separation area of a KRA that will result in an increase in the number of people working or residing in the transport route separation area.

In this instance, the proposed development does not involve any of the above circumstances, and accordingly, these provisions are not relevant to the assessment of this application.

### 3. Water Quality

These provisions relate to the following matters of state interest:

- (1) Receiving waters - including development applications for:
  - (a) a material change of use for urban purposes that involves a land area of 2,500m<sup>2</sup> or greater that will result in:
    - (i) an impervious area greater than 25% of the net developable area; or
    - (ii) six or more dwellings; or
  - (b) reconfiguring a lot for urban purposes that involves a land area 2,500m<sup>2</sup> or greater and will result in six or more lots; or
  - (c) operational works for urban purposes that disturb 2,500m<sup>2</sup> or greater of land area.
- (2) For water supply buffer areas – a development application located wholly outside an urban area and relating to premises that is within, or partly within, a water supply buffer area, that involves: (i) material change of use activities listed in Part E of the SPP, or (ii) reconfiguring a lot to create five or more additional lots where one or more lots are less than 16ha and any of the created lots rely on on-site wastewater treatment.

In this instance the proposed development involves a Material Change of Use on land with an area less than 2,500m<sup>2</sup>. Accordingly, the assessment provisions for this State interest are not relevant to the assessment of this application.

### 4. Natural Hazards, Risk and Resilience

These provisions relate to development applications for a material change of use, reconfiguring a lot or operational work on land within:

- (i) a flood hazard area;
- (ii) a bushfire hazard area;
- (iii) a landslide hazard area,
- (iv) storm tide inundation areas; and
- (v) erosion prone area.

In this instance, the site is not identified as being impacted by natural hazards. Accordingly, the assessment provisions for this State interest are not relevant to the assessment of this application.

### 5. Strategic Airports and Aviation Facilities

These provisions relate to development applications that involve land located within a local government area that contains/impacted by a strategic airport identified in the SPP Part E, Table 2, or an aviation facility identified in Appendix 2 of the *strategic airports and aviation facilities guidance material* and involve:

- (i) a material change of use of premises that will result in work encroaching into the operational airspace of a strategic airport, or on land within the light restriction zone, lighting area buffer or the wildlife hazard buffer zone of a strategic airport, or results in work encroaching into the building restricted area of an aviation facility; or
- (ii) a material change of use or reconfiguring a lot where any part of the land is within the 20ANEF contour or greater, or the public safety area of a strategic airport; or
- (iii) building work not associated with a material change of use that will result in work intruding into the operational airspace of a strategic airport, or where any part of the premises is within the light restriction zone of lighting area buffer of a strategic airport, or results in work encroaching into the building restricted area of an aviation facility; or

- (iv) operational work not associated with a material change of use where any work or associated activity will intrude into the operational airspace of a strategic airport, or where any part of the premises is within the light restriction zone of lighting area buffer of a strategic airport, or results in work encroaching into the building restricted area of an aviation facility.

Where these circumstances apply, the development application is assessed against the strategic airports and aviation facilities assessment benchmarks listed under the SPP.

In this instance, the site is not impacted by a Strategic Airport. Accordingly, the assessment provisions for this State interest are not relevant to the assessment of this application.

The proposal has been assessed to comply with all applicable matters of state interest included in the State Planning Policy.

### 4.3 STATE REFERRAL AGENCIES

Section 54 of the *Planning Act 2016* and Section 22 and Schedule 10 of the *Planning Regulation 2017 (the Regulation)* are of relevance for the purposes of determining applicable referral agencies and their jurisdiction in terms of being advice or concurrence agencies as well as the relevant assessment benchmarks that are to be addressed.

The State Assessment and Referral Agency (**SARA**), established under the auspices of the Department of State Development, Infrastructure and Planning (**DSDIP**) is the assessment manager or referral agency for development applications where the State has a jurisdiction pursuant to Schedule 8 or 10 of the Regulation. Schedule 10 regulates when applications are referable. In accordance with Schedule 8, DSDIP will not be the assessment manager for this development application.

**TABLE 1 - REFERRAL TRIGGER ASSESSMENT**

Part	Application Involving	Applicable	Comment
Part 1	Airport land	No	The proposed development is not located on airport land.
Part 2A	Caboolture West Interim Structure Plan	No	The site is not located within the Caboolture West investigation or growth areas.
Part 3	Clearing native vegetation	No	The proposed development does not involve clearing native vegetation or give rise to accepted clearing.
Part 4	Contaminated land	No	The site is not identified as contaminated land.
Part 5	Environmentally relevant activities	No	An approval for an ERA is not sought.
Part 6	Fisheries	No	The proposed development is not associated with: <ul style="list-style-type: none"> <li>• aquaculture; or</li> <li>• the removal, destruction or damage of marine plants; or</li> <li>• the constructing or raising of waterway barrier works; and</li> <li>• is not within a declared fish habitat area.</li> </ul>
Part 7	Hazardous chemical facilities	No	The proposed development is not for a hazardous chemical facility.
Part 8	Heritage places	No	The proposed development is not associated with a locally

Part	Application Involving	Applicable	Comment
			listed place. In addition, the site is not associated with, nor does it adjoin a property on the Queensland heritage register.
Part 9	Infrastructure-related	No	The following relates to infrastructure-related referrals: <ul style="list-style-type: none"> <li>The proposed development does not exceed the thresholds identified at Schedule 20.</li> <li>The site is not associated with a designated premises.</li> <li>The site is not located within 25m of a State transport corridor or 100m of a State controlled intersection.</li> <li>The site is not identified as a future State-controlled road.</li> <li>The site is not subject to an easement for the benefit of an electricity distribution entity, or transmission entity and for a transmission grid or supply network.</li> <li>The site is not located within 100m of an electricity substation.</li> <li>The site is not associated with an easement for oil, gas or electricity infrastructure.</li> <li>The site is not in proximity to a State controlled transport tunnel (both existing and/or future).</li> </ul>
Part 10	Koala habitat area	No	The site is not mapped as containing koala habitat areas.
Part 11	Noise attenuation land	No	The site is not identified as being noise attenuation land and the site is not within proximity of an off-road motorcycle facility. The proposed development is not associated with prohibited development.
Part 12	Operational works for reconfiguring a lot	No	The proposed development does not involve Operational Works.
Part 13	Ports	No	The site is not associated with port land.
Part 14	Reconfiguring a lot under Land Title Act	No	The proposed development does not involve Reconfiguring a Lot under the Land Title Act.
Part 15	SEQ Development area	No	The site is not located within an SEQ development area.
Part 16	SEQ regional landscape and rural production area and SEQ rural living area	No	The site is not located within the SEQ regional landscape and rural production area or the SEQ rural living area.
Part 16AA	Solar Farms	No	The proposed development does not involve a solar farm.
Part 16A	Southport Spit	No	The site is not located in the Southport Spit.
Part 16B	SEQ Northern Inter-Urban Break	No	The site is not located within the identified SEQ Northern Inter-Urban Break area.
Part 17	Tidal works or work in a coastal management	No	The proposed development does not involve tidal works or works within a coastal management district.

Part	Application Involving	Applicable	Comment
	district		
Part 18	Urban design	No	The proposed development does not involve an increase in GFA that is greater than 25,000m <sup>2</sup> .
Part 19	Water-related development	No	The proposed development does not involve: <ul style="list-style-type: none"> <li>the taking or interfering of water; or</li> <li>removing quarry material from a watercourse or lake; or</li> <li>relates to a dam; or</li> <li>the construction of a levee.</li> </ul>
Part 20	Wetland protection area	No	The site is not identified as being within a wetland protection area.
Part 21	Wind farms	No	The proposed development does not involve a wind farm

The above assessment of the referral triggers under the *Planning Regulation 2017* confirms that the proposed development will not trigger referral in this instance.

#### 4.4 STATE DEVELOPMENT ASSESSMENT PROVISIONS

The State Development Assessment Provisions (**SDAP**) provide assessment benchmarks for the assessment of development applications where the chief executive administering the *Planning Act 2016 (the Act)* is the assessment manager or a referral agency. The chief executive through the State Assessment and Referral Agency (**SARA**) uses the SDAP to deliver a coordinated, whole-of-government approach to the state's assessment of development applications. The SDAP applies throughout the State and contains the matters of State interest the chief executive may have regard to when assessing/deciding a development application as either the assessment manager or referral agency.

The SDAP contains criteria for assessment within State Codes to clarify when the State is to be involved in the assessment of an application and the specific matters of State interest that apply. The SDAP is a specific assessment benchmark that a development must be assessed against as prescribed under the Regulation. In this instance, the proposed development does not trigger assessment against State Codes.

#### 4.5 REGIONAL PLANS

The site is located within the Toowoomba Urban Extent and therefore the provisions of the Darling Downs and South East Queensland Regional Plans apply in accordance with the provisions of the *Planning Regulation 2017*. An assessment of the development against the regional plan provisions is provided below.

##### 4.5.1 SOUTH-EAST QUEENSLAND REGIONAL PLAN

The *South East Queensland Regional Plan 2023 (ShapingSEQ 2023)* provides a regional framework for growth management, which sets the long-term planning direction for sustainable growth, a globally competitive economy, and high-quality living. *ShapingSEQ 2023* informs local planning across the region's twelve (12) local government areas and guides future infrastructure planning and investment to ensure regional growth is well-supported and contributes to a prosperous and liveable region.

*ShapingSEQ 2023* seeks to set a clear direction for the region through a series of Regional Priorities. These priorities are implemented through identified outcomes and strategies and sub-regional directions. These Regional Priorities are:

- A sustainable growth pattern;
- More homes, faster – supply, diversity and affordability;
- More social and affordable homes;
- Well-designed communities;
- Integrating land use and infrastructure;
- New economic opportunities and jobs;
- A sufficient pipeline of industrial land;
- Incorporating Aboriginal and Torres Strait Islander knowledge, culture, traditions and aspirations;
- Regional approach to natural hazards; and
- Better biodiversity outcomes for growth areas.

*ShapingSEQ* defines the desired future settlement pattern that aims to use land more efficiently, while protecting the values important to the community. The regional growth pattern designates land in SEQ in one of three regional land use classifications:

- Urban Footprint;
- Rural Living Area; or
- Regional Landscape and Rural Production Area.

The SEQ regulatory provisions ensure that land use planning and development throughout the region support *ShapingSEQ 2023*'s desired regional growth pattern, goals, elements and strategies.

Under the settlement pattern identified in *ShapingSEQ 2023*, the site has been included within the **Urban Footprint**.

The Urban Footprint identifies land sufficient to provide for the region's urban development needs over the life of the plan. The area includes land intended to accommodate the full range of acceptable urban uses, such as housing, industry, business, infrastructure, community facilities and urban open space.

The proposal involves a Material Change of Use for a Low Impact Industry and Warehouse uses in an area suitable for such purposes. Accordingly, the application complies with the provisions of *ShapingSEQ 2023*.

#### **4.5.2 DARLING DOWNS REGIONAL PLAN**

The Darling Downs Regional Plan was adopted on 14 October 2013 and covers the local authority areas of Balonne Shire Council and the Regional Councils of Goondiwindi, Maranoa, Southern Downs, Toowoomba and Western Downs.

The policies contained in the regional plan contribute towards the protection of strategic areas of priority agricultural land use from potentially incompatible resource activities and seek to maximise opportunities for co-existence of resources and agricultural land use.

The regional plan also safeguards areas required for the growth of towns in the regions through the establishment of Priority Living Areas while providing for resource activities to locate within these areas where it meets communities' expectations as determined by the relevant local government.

The site is located within the City of Toowoomba which is identified as Restricted Area (RA) 384 under Appendix 2 of the Regional Plan. This designation reflects the boundaries of the SEQ Regional Plan referred to under section 4.5.1 above. The development satisfies the requirements of the SEQ Regional Plan and accordingly will not compromise the outcomes sought under the Darling Downs Regional Plan.

The proposal involves the establishment of a Multiple Dwelling in an area suitable for such purposes. Accordingly, the application complies with the provisions of Darling Downs Regional Plan.

## 4.6 TOOWOOMBA REGIONAL PLANNING SCHEME 2012

### 4.6.1 INTRODUCTION

Pursuant to the provisions of section 45(5) of the *Planning Act 2016*, an Impact Assessable Development Application must be assessed against the applicable assessment benchmarks of the applicable categorising instrument. The applicable categorising instrument in this instance is the *Toowoomba Regional Planning Scheme 2012*. A summary of the assessment of the proposal against the provisions of this instrument is outlined below.

### 4.6.2 DEFINITIONS

Under the provisions of the *Toowoomba Regional Planning Scheme 2012*, the proposed use is defined as 'Multiple Dwelling'. The relevant use definition is as follows:

**Multiple dwelling** means a residential use of premises involving 3 or more dwellings, whether attached or detached, for separate households.

### 4.6.3 ZONING

Under the *Toowoomba Regional Planning Scheme 2012*, the region is divided into twenty (20) land use zones. Certain zones are further divided into precincts for the purposes of conveying preferred land use intent or in order to assign assessment status to individual uses.

The site is predominately located within the **Low Density Residential Zone**. The zoning of the site and surrounding locality is illustrated in **Figure 7**.

**FIGURE 7 - ZONING MAPPING**



Under the assessment tables applicable to the Low Density Residential Zone at Section 5.5 of the Planning Scheme, a Material Change of Use for a Multiple Dwelling is identified as being subject to **Impact Assessment**.

#### 4.6.4 OVERLAY MAPPING

The Planning Scheme includes overlay maps that identify land characterised by particular features or subject to physical constraints that are likely to influence the use and development potential of affected areas. Overlay maps also identify those lands subject to assessment against specific area codes. In this instance, the subject land is impacted the Airport Environs Overlay.

The applicability of this overlay is illustrated in **Figure 8**.

**FIGURE 8 - AIRPORT ENVIRONS OVERLAY MAPPING**



**Figure 8** confirms that the site is impacted by the Airport Environs Overlay. In particular, the site is located within the 8km Wildlife Hazard Buffer Area and Lighting Area Buffer (6km) of the Toowoomba Airport. Under the overlay assessment tables at Section 5.10 of the *Toowoomba Regional Planning Scheme 2012*, the level of assessment for a Material Change of Use for a Multiple Dwelling does not change, however does trigger assessment against the Airport Environs Overlay Code. An assessment of the proposed development against this code is considered at section 4.7.2.

#### 4.7 ASSESSMENT BENCHMARKS

As identified in section 4.6, this Development Application is subject to Impact Assessment and therefore requires assessment against the following provisions of the *Toowoomba Regional Planning Scheme 2012*.

- Strategic Framework; refer to section 4.7.1
- Overall Outcomes – Low Density Residential Zone; refer to section 4.7.2
- Applicable Codes; refer to section 4.7.3

#### 4.7.1 STRATEGIC FRAMEWORK

Part 3 of the *Toowoomba Regional Planning Scheme 2012* provides the Strategic Framework that sets the policy direction for the planning scheme and forms the basis for ensuring development occurs at appropriate locations within the planning scheme area. The structure of the Strategic Framework comprises:

- seven (7) themes supported by strategic outcomes;
- elements that further refine the strategic outcomes sought; and
- specific outcomes and land use strategies for each of the elements.

The development has been assessed against the applicable provisions of the Strategic Framework and a summary of this assessment is provided in the table attached at **Appendix A**. An assessment of the proposal against the provisions of the Strategic Framework indicates the proposal complies with the integrated policy direction sought for the planning scheme area and will not compromise the relevant strategic outcomes/elements sought for the individual themes.

#### 4.7.2 OVERALL OUTCOMES – LOW DENSITY RESIDENTIAL ZONE

Section 6.2.1.2 of the *Toowoomba Regional Planning Scheme 2012* outlines the general intent for development within the Low Density Residential Zone through the collective identification of a series of overall outcomes for the zone. The proposed development generally complies with the intent of the Low Density Residential Zone as detailed in **Table 2**.

**TABLE 2 - ASSESSMENT OF OVERALL OUTCOMES**

Overall Outcome	Response
(a) the provision of detached dwelling houses as the primary form of housing within the zone and dual occupancy as a secondary form of housing, on a range of lot sizes greater than 500m <sup>2</sup> ;	<b>Performance Solution.</b> The proposed development involves the construction of an additional dwelling on the site, resulting in a Multiple Dwelling. The proposed development has been designed to ensure that the new dwelling has limited visibility from the street and is of an appearance which is compatible with the surrounding residential dwellings. The proposed development provides for an efficient land-use pattern and infill development of land without impacting on the residential amenity or character. It is noted that the street currently contains medium density housing (including Rangepoint Apartments), however the street is dominated by detached dwelling houses, ensuring it remains the primary form of housing.
(b) medium density forms of residential development are supported within greenfield areas in this zone where within walking distance of a local, district or major centre providing a wide range of goods, services and facilities, including provision of goods and services to meet daily convenience needs or where adjacent or opposite a district or regional park offering a high level of residential amenity.	<b>Not Applicable.</b> The site is not located within a Greenfield Area.
(c) retirement villages are established in the Low Density Residential Zone only where they are designed to achieve a transition in scale and	<b>Not Applicable.</b> The proposed development involves a Multiple Dwelling.

Overall Outcome	Response
density from one that is consistent with the surrounding area to a more dense form consistent with a typical retirement village;	
(d) dual occupancy development is dispersed throughout residential neighbourhoods and does not become the dominant housing form in the street;	<b>Not Applicable.</b> The proposed development involves a Multiple Dwelling.
(e) lot reconfiguration within greenfield areas in the zone achieves a minimum dwelling yield of 15 dwellings per hectare;	<b>Not Applicable.</b> The proposed development involves a Multiple Dwelling.
(f) new hatchet lots are not created within this zone and existing hatchet lots are occupied by a single dwelling house only	<b>Not Applicable.</b> The proposed development does not involve the creation of a hatchet lot.
(g) development provides for an efficient land-use pattern and is well connected to other parts of the local government area;	<b>Complies.</b> The proposed development is an example of maximising the residential yield of the site whilst also complying with the density sought within the zone. The site is located on a road which intersects with a higher order road and provides connectivity to the wider region.
(h) development is designed to provide safe and walkable neighbourhoods that connect residents to desirable destinations including schools, parks, shops and community facilities;	<b>Complies.</b> Despite the site not being directly serviced by pedestrian footpaths, such infrastructure is located approximately 100m north-west of the site which will provide the site with connectivity to attractors in the surrounding locality which includes parks and educational establishments.
(i) small scale short-term accommodation occurs only on land with direct access to a road at the distributor, sub-arterial and regional arterial level in the road hierarchy and in proximity to existing development of a similar nature, in buildings of a scale that is consistent with the surrounding residential area, all car parking needs can be met on site, and the use does not detract from the residential amenity of the area.	<b>Not Applicable.</b> The proposed development involves a Multiple Dwelling.
(j) development maintains a high level of residential amenity avoiding uses that introduce adverse impacts such as excessive traffic, noise, dust, odour, lighting and other locally specific impacts;	<b>Complies.</b> The proposed development is a residential use in a residential area and accordingly, will not result in impacts from dust, odour and lighting. The development will generate traffic volumes commensurate with a low scale residential use and accordingly, will not result in an unacceptable number of vehicle movements.
(k) development reflects and enhances the existing low density scale and character of the area, and is no more than 2 storeys above ground level;	<b>Complies.</b> The proposed development has been designed to exhibit a high standard of visual amenity and integrates with the established residential scale of the

Overall Outcome	Response
	surrounding locality. The proposed dwelling unit is of a raised single storey construction.
(l) development is designed to incorporate sustainable practices including maximising energy efficiency, water conservation and encourage sustainable transport use such as walking, cycling and public transport use;	<b>Complies.</b> The proposed development has been designed to incorporate sustainable practices to the greatest extent possible.
(m) development is supported by necessary community facilities, open space and recreational areas and appropriate infrastructure including utility installations to support the needs of the local community;	<b>Complies.</b> The proposed development is located within proximity to a number of open space facilities and educational establishments to meet the needs of residents. Additionally, the site will be connected to infrastructure networks.
(n) non -residential uses occur only where such uses : (i) primarily function to directly support the day to day convenience needs of the immediate local residential community; (ii) provide a local community or limited business function, and include: a) Child care centre. b) Club. c) Community care centre. d) Community use. e) Educational establishment. f) Emergency services g) The following uses only where reuse of an existing building used for an existing business activity: i. Food and drink outlet ii. Shop (only convenience). iii. Health care services. iv. Place of worship. v. Sales office. vi. Veterinary services.	<b>Not Applicable.</b> The proposed development involves a Multiple Dwelling.
(o) non -residential uses only occur within the zone where they: (i) are accessible to the immediate local residential community they serve; (ii) are located on land with direct access to a road at the distributor, sub - arterial and regional arterial level in the road hierarchy and minimises intrusion of through traffic into local residential streets; (iii) have a built form that is compatible with surrounding residential character and amenity, including: a) reuse of existing non -residential premises or b) extensions and/or new buildings have height, bulk and scale consistent with intended low density residential built form	<b>Not Applicable.</b> The proposed development involves a Multiple Dwelling.

Overall Outcome	Response
<p>intent of the zone and compatible with existing streetscape character;</p> <p>(iv) do not have a significant detrimental impact on the neighbourhood amenity expected within a predominantly low density residential environment having regard to sunlight and solar access, privacy, hours of operation, generation of odours, noise, waste products, dust, traffic, electrical interference, lighting and visual impacts;</p> <p>(v) ensure all car parking needs can be met on site, and off-street car parking areas do not dominate the appearance in the streetscape and are landscaped to provide an attractive frontage that maximise pedestrian activity and safety.</p>	
<p>(p) Development responds to the natural topography and environmental constraints of the land and avoids or where this is not possible, mitigates any adverse impacts on areas of ecological significance, including creeks, gullies, steep land, waterways, wetlands, habitats and vegetation and bushland through location, design, operation and management.</p>	<p><b>Not applicable.</b> The site is not located within areas of ecological significance.</p>
<p>(q) Development for a sensitive use on land within 250m of land within the Medium Impact Industry Zone is designed to mitigate industrial air, noise and odour emissions with potential to adversely impact on human health, amenity and wellbeing.</p>	<p><b>Not Applicable.</b> The site is not located within 250m of the Medium Impact Industry Zone.</p>

The above assessment demonstrates that the proposed development generally satisfies the intent and outcomes sought for land included in the Low Density Residential Zone.

### 4.7.3 APPLICABLE CODES

The codes applicable to the assessment of the proposed development are identified in **Table 3** followed by a summary of the assessment outcomes.

**TABLE 3 - APPLICABLE CODES**

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#### Zone Code(s)

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- **Low Density Residential Zone Code**

An assessment of the proposed development against the Overall Outcomes of the Low Density Residential Zone is provided at section 4.7.2. Additionally, an assessment of the development against the Assessment Benchmarks of the Low Density Residential Zone Code is provided at **Appendix B**. These assessment confirm that the proposed development generally complies with the outcomes sought within this code.

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#### Overlay Codes

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- **Airport Environs Overlay Code**

An assessment of the proposed development against this overlay code is provided at **Appendix B** and confirms that the development complies with the outcomes sought within this code.

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#### Development Codes

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- **Medium Density Residential Code**

An assessment of the proposed development against this development code is provided at **Appendix B** and confirms that the development complies with the outcomes sought within this code.

- **Environmental Standards Code**

An assessment of the proposed development against this development code is provided at **Appendix B** and confirms that the development complies with the outcomes sought within this code.

- **Integrated Water Cycle Management Code**

An assessment of the proposed development against this development code is provided at **Appendix B** and confirms that the development complies with the outcomes sought within this code.

- **Landscaping Code**

An assessment of the proposed development against this development code is provided at **Appendix B** and confirms that the development complies with the outcomes sought within this code.

- **Transport, Access and Parking Code**

An assessment of the proposed development against this development code is provided at **Appendix B** and confirms that the development complies with the outcomes sought within this code.

- **Works and Services Code**

An assessment of the proposed development against this development code is provided at **Appendix B** and confirms that the development complies with the outcomes sought within this code.

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## 5.0 TECHNICAL ASSESSMENTS

The following technical assessments and documentation have been completed to respond to and/or demonstrate the compliance of the proposal with the applicable standards:

- Conceptual Stormwater Report prepared by Baker Rossow Consulting Engineers. Refer to **Appendix D**.
- Engineering Plans, prepared by Baker Rossow Consulting Engineers. Refer to **Appendix E**.

The above assessment confirms that the proposed development can achieve compliance with the applicable standards and benchmarks.

## **6.0 CONSULTATION**

### **6.1 STATUTORY NOTIFICATION**

This Development Application is Impact Assessable and accordingly is required to be publicly notified under the provisions of the *Planning Act 2016* and its associated regulations. Public notification will be undertaken at the appropriate stage of the assessment process, as set out in the Development Assessment Rules prescribed under the *Planning Regulation 2017*.

### **6.2 PRE-LODGEMENT MEETINGS**

The proposed development was not the subject of pre-lodgement discussions.

## 7.0 CONCLUSION

This Development Application seeks approval for a Material Change of Use - Multiple Dwelling on land at 9 Benjamin Street, Mount Lofty. The assessment that has been undertaken has demonstrated the following:

- The proposed development involves the construction of a third dwelling at the rear of the site which allows for appropriate infill development while maintaining the residential character and amenity of the area.
- The development is residential in nature within an established residential area and accordingly is compliant with the intent of the Low Density Residential Zone.
- The proposed development has been designed to ensure it does not adversely impact on the surrounding environment or the safety of people and/or property as a result of natural hazards.
- The proposed development achieves compliance with the benchmarks within the relevant zone, overlay and development codes (as outlined in section 4.7).
- The technical assessments supporting the application (as detailed in section 5.0) demonstrate that the proposed development will not have an adverse impact on the surrounding locality.

Having regard to the matters and issues raised in this report it is recommended that Council support this Development Application for a Development Permit for Material Change of Use.

The proposal warrants approval subject to the imposition of reasonable and relevant conditions.

## **APPENDIX A - STRATEGIC FRAMEWORK**

*Toowoomba Regional Planning Scheme 2012*

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

Theme/Element	Relevance Yes/No	Assessment
<b>3.3 SETTLEMENT PATTERN</b>		
3.3.1 Strategic Outcomes	<b>Yes</b>	The proposed development involves an urban use, being a Multiple Dwelling, in an area suitable for such purposes. The proposal involves the construction of an additional dwelling at the rear of the site which allows for logical infill development, while maintaining the character of the area. The development is located within 1km of employment opportunities across a number of educational and community facilities and a Local Centre, which support the need for higher density development within this area.
3.3.2 Element – network of towns	No	
3.3.3 Element – compact urban form	<b>Yes</b>	The proposed development contributes to developing a safe, convenient and attractive neighbourhood by proposing higher density housing in an area that is supported by a range of nearby services including education, open space and retail. The proposed additional dwelling is located at the rear of the site which ensures that it allows for appropriate infill development while maintaining the existing residential amenity of the area. The site is also serviced by the 903 bus route which runs along Benjamin Street and connects the site to the university and Toowoomba CBD.
3.3.4 Element – suburban neighbourhoods	<b>Yes</b>	Although the proposed development involves a housing form that is not specifically identified as consistent with the intent of the Low Density Residential Zone, the proposed development has been designed to respond to the character of the locality. The proposed development involves the establishment of one (1) additional dwelling unit at the rear of the site and accordingly will maintain the visual amenity and residential character, with detached dwelling houses still comprising of the primary housing form for the surrounding locality.
3.3.5 Element – urban neighbourhoods	No	
3.3.6 Element – centres of activity	No	
3.3.7 Element – central business district (CBD)	No	
3.3.8 Element – sustainable urban development	<b>Yes</b>	The proposed development has been sensitively designed to ensure that the established character of the surrounding locality is retained. The proposed development's proximity to public transport and other uses promotes active and public transport use which contributes to providing an interconnected neighbourhood.
3.3.9 Element – rural landscape	No	
3.3.10 Element – natural places	No	

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

Theme/Element	Relevance Yes/No	Assessment
3.3.11 Element – development constraints	No	
3.3.12 Element – incompatible land uses	No	
<b>3.4 NATURAL ENVIRONMENT</b>		
3.4.1 Strategic Outcomes	No	
3.4.2 Element - protect ecosystems with biodiversity values	No	
3.4.3 Element – waterways, wetlands and aquifers	No	
3.4.4 Element - air quality	No	
3.4.5 Element – climate change	<b>Yes</b>	The proposal seeks to provide additional housing in a well-located area which reduces the distance and need to travel by private vehicle and supports the use of integrated active transport (e.g. cycle and walking tracks) and public transport. The site is located on the 903 bus route which runs along Benjamin Street and connects the site to the university and Toowoomba CBD.
3.4.6 Element – environmental offsets	No	
<b>3.5 COMMUNITY IDENTITY AND DIVERSITY</b>		
3.5.1 Strategic Outcomes	<b>Yes</b>	The proposed development is located within proximity to a range of non-residential uses which provides increased choice and proximity to employment opportunities for future residents. The higher density housing proposed responds to the direct needs of the community for alternative housing forms and low maintenance living. The number of open space facilities within proximity of the site (e.g. Southern Cross Reserve, Horn Park and Boyce Gardens) ensures that residents have access to recreational facilities and encourages healthy lifestyles.
3.5.2 Element – rural community identity	No	
3.5.3 Element – Toowoomba City identity	No	
3.5.4 Element – urban design	<b>Yes</b>	The built form of the proposed development has been designed such that the proposed building is located behind the existing dwelling units onsite. This reduces the overall visual impact of the development. The development has also been designed to respond to the natural features of the land and ensures that characteristics such as natural drainage are retained.
3.5.5 Element – community facilities and services	No	

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

Theme/Element	Relevance Yes/No	Assessment
3.5.6 Element – sport and recreation	No	
3.5.7 Element – urban parks and public places	No	
3.5.8 Element – housing diversity and affordability	<b>Yes</b>	The proposed development provides for higher density housing located within proximity to a range of non-residential uses which provide opportunities for employment diversity. The proposed development has been designed to provide a compact urban form which will provide a low maintenance housing option for future residents who are not able to maintain and/or afford a single dwelling on a large lot. The proposal is an example of maximising development of urban land and provides housing diversity whilst retaining the character of the locality.
3.5.9 Element – cultural diversity and heritage	No	
<b>3.6 NATURAL RESOURCES AND LANDSCAPE</b>		
3.6.1 Strategic Outcomes	No	
3.6.2 Element – scenic amenity	No	
3.6.3 Element – water resources	No	
3.6.4 Element – sustainable production	No	
3.6.5 Element – natural economic resources	No	
3.6.6 Element – stock routes	No	
<b>3.7 ACCESS AND MOBILITY</b>		
3.7.1 Strategic Outcomes	<b>Yes</b>	The proposed development has been designed to retain the site's existing integration with the surrounding movement network and connection to infrastructure that provides connections to key services in the locality. The site is located on the 903 bus route which runs along Benjamin Street and connects the site to the university and Toowoomba CBD.
3.7.2 Element – integrated transport system	No	
3.7.3 Element – active transport	<b>Yes</b>	The proposed development is designed to encourage residents to utilise active transport infrastructure located within the surrounding locality as a means of travelling to the range of services provided within the area.
3.7.4 Element – public transport	<b>Yes</b>	The site is located within 100m of a bus stop on 903 bus route which runs along Benjamin Street and connects the site to the university and Toowoomba CBD.
3.7.5 Element – road network	No	

**TOOWOOMBA REGIONAL PLANNING SCHEME 2012 – STRATEGIC FRAMEWORK**

Theme/Element	Relevance Yes/No	Assessment
<b>3.8 INFRASTRUCTURE AND SERVICES</b>		
3.8.1 Strategic Outcomes	<b>Yes</b>	The proposed development will be serviced by all necessary infrastructure connections including reticulated water, sewer, stormwater, telecommunications and electricity supply networks.
3.8.2 Element – coordinated infrastructure planning and delivery	<b>Yes</b>	See response to 3.8.1.
3.8.3 Element – integrating water management and infrastructure	<b>Yes</b>	See response to 3.8.1.
3.8.4 Element – waste water management infrastructure and services	<b>Yes</b>	See response to 3.8.1.
3.8.5 Element – utility infrastructure and services	<b>Yes</b>	See response to 3.8.1.
3.8.6 Element – waste management and recycling	<b>Yes</b>	The proposed development will utilise waste recycling to minimise impacts on the environment.
<b>3.9 ECONOMIC DEVELOPMENT</b>		
3.9.1 Strategic Outcomes	<b>Yes</b>	The proposed housing provides additional accommodation for existing and future residents of the Toowoomba Region, noting that there currently is a shortage of housing to accommodate prospective workers and students looking to move to the region. The proposed housing will contribute to additional rental stock thereby enabling businesses to recruit employees and attract new business to the region.
3.9.2 Element - economic growth	<b>Yes</b>	Refer to response to 3.9.1.
3.9.3 Element – activity centres and employment	No	
3.9.4 Element – creative and knowledge-based industries	No	
3.9.5 Element – tourism	No	

## **APPENDIX B - ASSESSMENT BENCHMARKS**

*Toowoomba Regional Planning Scheme 2012*

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Low Density Residential Zone Code – requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Caretaker's Accommodation</b>		
<p>PO<sub>1</sub> Development provides for the accommodation of a caretaker, and their family members, involved in the running of a non-residential use, in a manner that:</p> <ul style="list-style-type: none"> <li>(a) does not compromise the productivity of the use;</li> <li>(b) is safe and comfortable for the amenity of residents; and</li> <li>(c) has regard to the landscape and private recreation needs of the residents.</li> </ul>	<p>AO<sub>1.1</sub> A caretaker's accommodation is:</p> <ul style="list-style-type: none"> <li>(a) separated from significant levels of emissions (adverse to human health or amenity) generated by the non-residential use/s of the site by at least 6m;</li> <li>(b) provided with a private landscape and recreation area which:               <ul style="list-style-type: none"> <li>(i) is directly accessible from a habitable room;</li> <li>(ii) if at ground level, has a minimum area of 16m<sup>2</sup> with minimum dimensions of 4m; and</li> <li>(iii) where provided as a balcony, verandah or deck has a minimum area of 8m<sup>2</sup> with a minimum dimension of 2.4m.</li> </ul> </li> </ul> <p>AO<sub>1.2</sub> No more than one (1) caretaker's accommodation is established per non-residential use.</p>	<p><b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.</p>
<b>Dwelling Unit</b>		
<p>PO<sub>2</sub> Development incorporating a dwelling unit is provided in a manner that:</p> <ul style="list-style-type: none"> <li>(a) integrates visually with the non-residential use through the use of materials, colours and finishes, architectural treatments and landscaping treatments;</li> <li>(b) ensures residents have a high level of safety, security, privacy and amenity;</li> </ul>	<p>AO<sub>2.1</sub> Separate entrances are provided to the dwelling unit and non-residential uses on the same site.</p> <p>AO<sub>2.2</sub> Entries to the dwelling unit are clearly identifiable from the street and have a defined pathway.</p> <p>AO<sub>2.3</sub> Dwelling units are readily distinguishable from the non-residential use for emergency service providers.</p>	<p><b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Low Density Residential Zone Code – requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(c) ensures residents are not exposed to noise and light nuisance from the non-residential use; and</p> <p>(d) provides residents access to private outdoor recreation spaces directly from the dwelling unit.</p>	<p>AO<sub>2.4</sub> The dwelling unit number is clearly displayed on the unit and letter box.</p> <p>AO<sub>2.5</sub> The dwelling unit is provided with a private landscape and recreation area which:</p> <p>(a) is directly accessible from a habitable room;</p> <p>(b) if at ground level, has a minimum area of 16m<sup>2</sup> with minimum dimensions of 4m; and</p> <p>(c) if a balcony, a veranda or a deck, has a minimum area of 8m<sup>2</sup> with minimum dimensions of 2.4m.</p> <p>AO<sub>2.6</sub> Habitable rooms of the dwelling unit are protected from overlooking of the non-residential use/area by:</p> <p>(a) separation of 9m from a window or activity area of the non-residential use; or</p> <p>(b) screening the outlook from windows, balconies or activity area of the non-residential use within a direct view of 9m to the habitable room or private open space of the dwelling unit.</p>	
<b>Non-residential Uses and building work – Scale of use where involving the reuse of an existing building</b>		
<p>PO<sub>3</sub> The non-residential use is of a small scale and intensity that is compatible with the character of the streetscape and the low density residential built form appearance of the locality.</p>	<p>AO<sub>3.1</sub> The use:</p> <p>(a) is carried out in an existing building;</p> <p>(b) only increases the Gross Floor Area of the existing building by a maximum of 25m<sup>2</sup> and does not reduce existing front or side setbacks;</p> <p>(c) is a single tenancy only;</p> <p>(d) has a maximum gross floor area of 200m<sup>2</sup>;</p> <p>(e) is conducted wholly within an enclosed building; and</p>	<p><b>Not Applicable.</b></p> <p>The proposed development is for a Multiple Dwelling.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	(f) does not involve outdoor dining or drive through facility.	
<b>Non-residential Uses and building work – Noise Amenity</b>		
PO <sub>4</sub> The non-residential use does not adversely impact on the amenity of the surrounding residential land uses and/or the intended residential streetscape character.	AO <sub>4.1</sub> New building plant or air-conditioning equipment is located central to the building and screened from view of the street or adjoining residential uses.	<b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.
<b>Non-residential Uses and building work – Privacy and Screening</b>		
PO <sub>5</sub> Non-residential uses provide adequate separation, buffering and screening from adjoining residential premises so that the privacy and amenity of residential premises is protected.	AO <sub>5.1</sub> A 2m wide vegetated buffer is provided to any vehicle movement and parking areas that adjoin a residential boundary. AO <sub>5.2</sub> A 1.8m high solid screen fence and 1.5m wide strip of screen landscaping are provided along all boundaries shared with an adjoining residential use. AO <sub>5.3</sub> Windows that have direct views into adjoining residential buildings are provided with fixed screening that is a maximum of 75% transparent to obscure views into the adjoining residential building and maintain privacy for those residents.	<b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.
<b>Non-residential Uses and building work – Outdoor Lighting</b>		
PO <sub>6</sub> Outdoor lighting for non-residential uses maintains the amenity of the surrounding residential area and does not adversely impact the safety for vehicles or pedestrians on the adjoining street as a result of light emissions, either directly or by reflection.	AO <sub>6.1</sub> Outdoor lighting for non-residential uses is restricted to low level security lighting only. AO <sub>6.2</sub> Outdoor lighting is designed, installed and maintained in accordance with the parameters and requirements of AS4282 – <i>Control of the Obtrusive Effects of Outdoor Lighting</i> .	<b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.

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Performance outcomes	Acceptable outcomes	Compliance summary
<b>Utilities and Stormwater</b>		
PO <sub>7</sub> A water supply is provided that is adequate for the current and future needs of the intended use.	AO <sub>7.1</sub> Where within a water supply area, the development is connected to Council's reticulated water supply system in accordance with SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure.	<b>Complies.</b> The proposed development will be connected to Council's reticulated water supply in accordance with the relevant standards.
PO <sub>8</sub> Wastewater treatment and disposal is provided that is appropriate for the level of demand generated, protects public health and avoids environmental harm.	AO <sub>8.1</sub> Where within a wastewater area, the development is connected to the Council's reticulated wastewater system in accordance with SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure.	<b>Complies.</b> The proposed development will be connected to Council's wastewater system in accordance with the relevant standards.
PO <sub>9</sub> The development is equipped with an adequate energy supply approved by and installed in accordance with the standards of the relevant energy regulatory authority.	AO <sub>9.1</sub> Premises are connected to an electricity supply approved by the relevant energy regulatory authority.	<b>Complies.</b> The proposed development will be connected to an electricity supply.
PO <sub>10</sub> Stormwater resulting from roofed areas and impervious surfaces is collected and discharged in a manner that does not adversely affect the stability of buildings or the use of adjacent land.	AO <sub>10.1</sub> Roof water and impervious surfaces water runoff is collected and discharged in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies.</b> Roof water and impervious surface runoff will be collected and discharged in accordance with the Conceptual Stormwater Report attached at <b>Appendix D.</b>
<b>Waste Management</b>		
PO <sub>11</sub> Appropriate refuse container storage areas are provided which are: (a) in a building or enclosing structure or screened from public view; (b) of adequate size to accommodate the expected amount of refuse to be generated by the use; (c) in a position that is conveniently accessible for collection; and	AO <sub>11.1</sub> Refuse container storage areas are provided that: (a) are located behind the building line and screened from public view: (i) in a building, outbuilding or other enclosed structure; (ii) screened by a minimum 1.5 m high solid fence or wall that is surrounded by minimum 1m wide landscaping	<b>Complies.</b> Each dwelling will be provided with a refuse container storage area that is screened from public view by fencing and will accommodate a general waste, recycling and green waste container to ensure the refuse storage needs of each dwelling can be met. Containers will be moved to kerbside for collection by Council's regular waste collection service.

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>(d) able to be kept in a clean state at all times, and waste is captured and discharges to an approved collection point.</p>	<p>(excluding container storage access point) where not adjoining a residential boundary; or;</p> <p>(iii) screened by a minimum 1.8m high solid fence where adjoining a residential boundary</p> <p>(b) are provided with an imperviously sealed pad, on which to stand the bin(s), that is drained to an approved waste disposal system;</p> <p>(c) are within normal hose length of a hose cock; and</p> <p>(d) are large enough to accommodate at least one (1) standard/sized container per dwelling and, in commercial and industrial premises, one (1) or more industrial bins of a size appropriate to the nature and scale of use.</p>	
<b>Non-residential uses and building work – Access, on-site car parking and manoeuvring</b>		
<p>PO<sub>12</sub> Provision is made for on-site vehicle parking to meet the demand likely to be generated by the development and:</p> <p>(a) to avoid on-street parking where that would adversely impact on the safety or capacity of the road network or unduly impact on local amenity.</p> <p>(b) to ensure that off-street car parking areas do not dominate the appearance in the streetscape.</p>	<p>PO<sub>12.1</sub> Where not involving the reuse of an existing premises used for a business activity car parking is provided in accordance with the Transport, Access and Parking Code.</p> <p>PO<sub>12.2</sub> Where involving re-use of premises used for a business activity:</p> <p>(a) There is no reduction in existing or previously approved on-site car parking;</p> <p>(b) There are no alterations to the location of existing or previously approved access (driveways and vehicle cross-overs) and on-site car parking and manoeuvring areas.</p>	<p><b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
<b>Non-residential uses and building work – Landscaping</b>		
PO <sub>13</sub> Landscaping makes a positive contribution to the site and the amenity of the surrounding area and existing landscaping is not diminished.	PO <sub>13.1</sub> Where involving the reuse of a premises there is no reduction in the area or quantity of established or previously approved on-site landscaping.	<b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.

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**Low Density Residential Zone Code – requirements for accepted development and assessment benchmarks for assessable development where not involving Building Work (not associated with a Material Change of Use)**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Non-residential uses - Hours of Operation</b>		
PO <sub>1</sub> Hours of operation are controlled so that the non-residential use does not impact on the amenity or privacy of adjoining residential uses.	AO <sub>1.1</sub> Hours of operation, including for deliveries, are limited to hours between 6:00 am to 9:00 pm.	<b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.
<b>Non-residential Uses – Impact on Road Network</b>		
PO <sub>2</sub> Non-residential uses are located on major roads and do not introduce non-residential traffic into local streets.	AO <sub>2.1</sub> Non-residential uses: (a) have frontage and vehicle access to a Regional Arterial, Sub-Arterial or Distributor Road; and (b) vehicle access to the development does not occur from a local street.	<b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.

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Performance outcomes	Acceptable outcomes	Compliance summary
<b>Residential Development</b>		
PO <sub>1</sub> The zone accommodates predominantly dwelling houses and other housing forms at a low density scale and intensity.	AO <sub>1.1</sub> Uses which are consistent with the intent of the zone include: (a) dwelling house; and (b) dual occupancy, except where located in the Clifford Park Stables and Park Residential Precincts	<b>Performance Solution.</b> The proposed development will seek to retain the existing attached dwelling units and establish an additional dwelling unit to the rear of the site, resulting in a Multiple Dwelling (3 units). The proposed density is 29dw/ha which is less than the 40-60dw/ha intended for the Low-Medium Density Residential Zone and is therefore considered suitable for the Low Density Residential Zone. Further, the site is located within an urban area which is within walking distance of a variety of educational institutions, open space and centres. The proposed development has been designed to retain the residential amenity of the area and ensure that Dwelling Houses will remain as the predominate housing form.
PO <sub>2</sub> The density of accommodation activities in the Clifford Park Stables Precinct is not increased, other than where directly associated with the management of stables on the same site.	No acceptable outcome is nominated.	<b>Not Applicable.</b> The proposed development is not located in the Clifford Park Stables Precinct.
PO <sub>3</sub> Short-term accommodation occurs where: (a) it is of a small scale that is consistent with the intensity of development in the surrounding residential area; (b) adjoining or located on the opposite side of a road to existing Short-term	No acceptable outcome is nominated.	<b>Not Applicable.</b> The proposed development is for a Multiple Dwelling and does not involve short-term accommodation.

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>accommodation or other Accommodation activities of a similar scale and density ;</p> <p>(c) all car parking needs can be met on site;</p> <p>(d) have direct vehicle access to a distributor, sub-arterial and regional arterial level road that is not a State-controlled road; and</p> <p>(e) do not unduly detract from the amenity of nearby residences.</p>		
<p>PO<sub>4</sub> Higher density forms of Accommodation activities (i.e. other than caretaker's accommodation, community residence, dwelling house and dual occupancy) are designed to reflect the residential scale and density of the surrounding area.</p>	<p>AO<sub>4.1</sub> The number of dwellings on the site does not exceed one per 500m<sup>2</sup>.</p> <p>AO<sub>4.2</sub> The site has a minimum frontage of 12m.</p>	<p><b>Performance Solution.</b></p> <p>The site has a 21m frontage and a proposed dwelling density of one dwelling per 341m<sup>2</sup> which is higher than what is sought by AO<sub>4.1</sub> but reflects the modest nature/size of the existing and proposed units and the site's proximity to services and public transport, and thus suitability to accommodate additional density. The site has been designed to reflect the character and scale of the surrounding locality by utilising single storey design and locating the additional unit at the rear of the site where it is hidden from the street.</p>
<b>Non-Residential Development</b>		
<p>PO<sub>5</sub> Non-residential uses establish only where they:</p> <p>(a) except for child care centres, provide for the day to day convenience needs or local</p>	<p>No acceptable outcome is nominated.</p>	<p><b>Not Applicable.</b></p> <p>The proposed development is for a Multiple Dwelling.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>service of the immediate local residential community;</p> <p>(b) do not undermine the viability of a nearby centre and are not of a scale that impacts on the role or function centres network;</p> <p>(c) do not contribute to strip development or expansion of an existing centre;</p> <p>(d) are a of a small scale, and have low intensity operation and employment;</p> <p>(e) are highly accessible to the immediate local community it serves and have direct vehicle access to a distributor, sub-arterial and regional arterial;</p> <p>(f) do no introduce non-local traffic into a local street;</p> <p>(g) are in buildings, including extensions and alterations to an existing building, that have a low rise bulk and scale compatible with a dwelling house and consistent with the character of adjoining residential buildings and the surrounding streetscape;</p> <p>(h) do not adversely impact the amenity, safety or privacy of nearby residences.</p>		
<b>Built Form</b>		
<p>PO<sub>6</sub> Buildings are of a height which is consistent with the intended character of the zone and overlay, and which do not unduly reduce privacy or access to sunlight to habitable rooms, private open space and solar panels on adjoining land.</p>	<p>AO<sub>6.1</sub> Buildings have a maximum height of two (2) storeys or 8.5m above ground level.</p>	<p><b>Complies.</b> The proposed development involves the establishment of a raised single storey dwelling.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>7</sub> Except for Dwelling Houses and Dual Occupancies site coverage:</p> <ul style="list-style-type: none"> <li>(a) maximizes setbacks;</li> <li>(b) maximizes landscaping;</li> <li>(c) ensures adequate useable outdoor areas;</li> <li>(d) ensures adequate space for vehicle movement and parking areas;</li> <li>(e) maximizes solar access for internal and external living spaces;</li> <li>(f) does not compromise solar access for adjoining premises; and</li> <li>(g) does not result in overshadowing of adjoining properties.</li> </ul>	<p>AO<sub>7.1</sub> For development up to two (2) storeys site cover does not exceed 50% of the site area.</p> <p>For development over two (2) storeys no acceptable outcome is nominated.</p>	<p><b>Complies.</b> The proposed development does not exceed 50% of the site area.</p>
<p>PO<sub>8</sub> Impervious site coverage:</p> <ul style="list-style-type: none"> <li>(a) ensures development maximizes on-site infiltration and minimizes the additional burden on drainage infrastructure;</li> <li>(b) reduces the visual impact of additional hardstand;</li> <li>(c) respects the existing or preferred neighbourhood character and responds to the features of the site; and</li> <li>(d) allows for the provision of an appropriate supply of landscaping and open space.</li> </ul>	<p>AO<sub>8.1</sub> Impervious areas of the site do not exceed 60% of the site area.</p>	<p><b>Complies.</b> The proposed development involves an impervious area of approximately 41.54%.</p>
<p>PO<sub>9</sub> The front building setback is consistent with the prevailing front setbacks in the street.</p>	<p>AO<sub>9.1</sub> Where the site has frontage to a collector or local road, buildings are set back from that frontage:</p>	<p><b>Complies.</b> The proposed building is located behind the existing building which has a setback greater than 6m from the front boundary. The proposed building is setback 30m from the front boundary and located behind existing buildings onsite.</p>

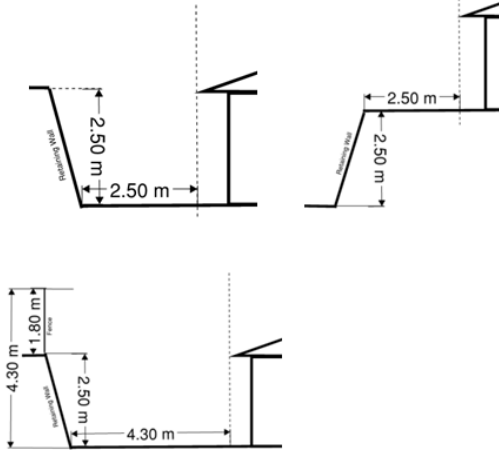
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**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(a) within 20% of the average front setback of adjoining buildings; or</p> <p>(b) where there are no adjoining buildings, 4m.</p> <p>AO<sub>9.2</sub> Where the site has frontage to a road other than a collector or local road:</p> <p>(a) residential buildings are set back from that frontage in accordance with the requirements of the Queensland Development Code.</p> <p>(b) non-residential buildings are set back from that frontage 6m.</p> <p>AO<sub>9.3</sub> Where involving non-residential use, buildings are set back a minimum 6m from the road frontage.</p>	<p><b>Not Applicable.</b> The proposed development has frontage to a local road.</p> <p><b>Not Applicable.</b> The proposed development is for a residential use.</p>
<p>PO<sub>10</sub> Side and rear building setbacks:</p> <p>(a) enhance the appearance and character of streets and buildings;</p> <p>(b) are appropriate to the scale of the development and the intended low density character of the zone or precinct in which the site is located;</p> <p>(c) provide for adequate daylight for habitable rooms and open space areas on and adjoining the site;</p> <p>(d) are sufficient to minimise overshadowing and overlooking of adjoining premises;</p> <p>(e) provide adequate separation and buffering between residential and non-residential premises; and</p>	<p>AO<sub>10.1</sub> Buildings are set back from a side boundary:</p> <p>(a) Ground Floor (up to 3.5m high) – 1.5m; and</p> <p>(b) First floor (up to 7.5m high) – 2m;</p> <p>(c) Above 7.5m – 3m; or</p> <p>(d) the distance of the height of the retaining wall on the side boundary or combination of the height of the retaining wall and a fence, whichever is greater (Figure 1). Where the retaining wall extends into the site, the setback is measured from the base or top of the retaining wall.</p> <p>AO<sub>10.2</sub> Buildings are set back:</p> <p>(a) a minimum of 3m from a rear boundary; or</p>	<p><b>Performance Solution.</b> The proposed dwelling unit achieves side setbacks of 1.5m however, is also setback a minimum of 1.5m from the rear boundary. This reduced setback is due to the angled rear boundary which does not run perpendicular to the side boundaries and makes accommodating rectilinear building forms challenging. It is noted that the proposed building involves a raised single storey construction and will not result in adverse overshadowing or overlooking of adjoining premises.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>(f) maximise opportunities for landscaping.</p>	<p>(b) the distance equivalent to of the cumulative height of the retaining wall and fence on the rear boundary (Figure 1). Where the retaining wall extends into the site, the setback is measured from the base or top of the retaining wall.</p>  <p>Figure 1</p> <p>For non-residential development:</p> <p>AO<sub>10.3</sub> Buildings are set back a minimum of 2.5m from a side boundary.</p> <p>AO<sub>10.4</sub> Buildings are set back from a rear boundary whichever is the greater -</p> <p>(a) a minimum of 3m; or.</p>	<p><b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	(b) The distance equivalent to the cumulative height of the retaining wall and fence on the rear boundary. Where the retaining wall extends into the site, the setback is measured from the base or top of the retaining wall.	
PO <sub>11</sub> The development is designed to a high aesthetic standard and to integrate with and enhance the locality, having regard to the following: (a) built form; (b) open space; (c) landscaping; (d) the public realm; (e) parking and access; (f) solar access; (g) views and outlook; (h) setbacks; (i) height; (j) scale and bulk; and (k) provision of services	In partial compliance with the performance outcome:  AO <sub>11.1</sub> A minimum 1m wide landscaping strip is provided for the full length of the driveway and parking areas where parallel to a boundary common with a residential premises.  <i>Note: Such landscaping areas must not be compromised by the maneuvering areas required for the vehicle.</i>	<b>Performance Solution.</b> The site contains an existing driveway that is located adjacent to the western boundary of the site for a length of approximately 36m. Accordingly, the proposed development involves the logical extension of this driveway to facilitate access to the proposed dwelling unit at the rear of the site. In this respect, it is considered that the development appropriately integrates with the existing built form of the site and will not adversely impact on the amenity of the adjoining premises.
PO <sub>12</sub> Non-residential use buildings, including extensions or additions, exhibit design elements that are characteristic of and compatible with surrounding houses in the streetscape including:- (a) buildings orient to and addresses the street frontage; (b) the main building entry faces the street at ground level; (c) building mass is broken up by articulation, fenestration, recesses and landscape elements to avoid large expanses of blank surfaces ;	No acceptable outcome is nominated	<b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>(d) differentiate between private houses and the non-residential use through variation in materials, patterns, textures and/or colours</p> <p><b>Editor's note</b> – examples of façade treatment to avoid expanses of blank surface may include (but is not limited to):</p> <ul style="list-style-type: none"> <li>• windows are provided in any building façade facing the street or other public and communal space;</li> <li>• awnings, verandahs, pitched roofs and rectangular building plan forms are used in the building design.</li> </ul>		
<p>PO<sub>13</sub> Where appropriate, development facilitates active transport and open space connections through the neighbourhood.</p>	<p>No acceptable outcome is nominated.</p>	<p><b>Not Applicable.</b> The proposed development does not make provision for features that promote active transport and open space connections.</p>
<p>PO<sub>14</sub> The site layout responds sensitively to adjoining land uses as well as on-site and surrounding topography, drainage patterns, utility services, access, built forms and vegetation such that:</p> <ul style="list-style-type: none"> <li>(a) any hazards or nuisance to people or property on the site or offsite are avoided;</li> <li>(b) any earthworks are minimised and design alternatives are prioritised over earthworks;</li> <li>(c) natural drainage lines are retained;</li> <li>(d) existing vegetation is retained or replaced;</li> <li>(e) damage or disruption to sewer, stormwater and water infrastructure is avoided; and</li> <li>(f) there is adequate buffering, screening a separation to adjoining development.</li> </ul>	<p>No acceptable outcome is nominated.</p>	<p><b>Complies.</b> The proposed development has been designed to respond to the physical characteristics of the site and surrounding locality through the provision of a single-storey building at the rear of the site and the retention of existing access locations and infrastructure arrangements.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
<b>Amenity and Safety</b>		
PO <sub>15</sub> Development maintains a high level of residential amenity within the site and for surrounding areas, having regard to noise, odour, lighting, access to sunlight, privacy and outlook.	No acceptable outcome is nominated.	<b>Complies.</b> The proposed development has been designed to maintain residential amenity for surrounding sensitive land uses through thoughtful building design and siting, the provision of landscaping and fencing to protect privacy of existing and future residents.
PO <sub>16</sub> Site layout, building design and landscaping facilitates the security of people and property having regard to:  (a) opportunities for casual surveillance of and sightlines to publicly accessible areas such as car parks, pathways, public toilets and communal areas;  (b) exterior building design and orientation which promote safety;  (c) adequate definition of uses and public and private ownership;  (d) adequate lighting;  (e) appropriate way-finding mechanisms (e.g. signage);  (f) minimisation of entrapment locations; and  (g) building entrances, loading and storage areas being well lit and lockable after hours.	AO <sub>16.1</sub> Setbacks are provided from all boundaries in accordance with acceptable outcome AO <sub>10.2</sub> ; AO <sub>16.2</sub> The development does not introduce lighting which is inconsistent with a residential area; AO <sub>16.3</sub> Sunlight access for the private open space or habitable rooms on adjoining properties or private open spaces on the subject site is not reduced to less than 3 hours between 9am and 3pm on June 21, or AO <sub>16.4</sub> Where existing overshadowing by building and fences is greater than this, sunlight is not further reduced by 20%.	<b>Performance Solution.</b> The proposed development does not achieve the required rear setback due to the reasons provided at PO <sub>10</sub> , however is setback the required distance from front and side boundaries. Irrespective of this, the proposed layout does not adversely impact the ability for casual surveillance in and around the site..
PO <sub>17</sub> Development is designed to incorporate graffiti-prevention measures.	AO <sub>17.1</sub> Building design and layout incorporates the following features where practical:  (a) designs with an absence of 'natural ladders';  (b) minimal unbroken vertical surface areas; and	<b>Complies.</b> The proposed development will incorporate graffiti deterrent measures where possible.

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Performance outcomes	Acceptable outcomes	Compliance summary
	(c) graffiti-deterrent surface treatments.	
PO <sub>18</sub> Development for non-residential uses provides landscaping that: <ul style="list-style-type: none"> <li>(a) is consistent with the dominant landscape character appearance of the streetscape in an established area and intended in the zone and character overlay in which the site is located;</li> <li>(b) provides an attractive interface between the use, the streetscape and adjoining residential uses</li> <li>(c) provides and maintains:               <ul style="list-style-type: none"> <li>(i) the privacy and amenity for adjoining residential uses</li> <li>(ii) sight lines and overlooking to public spaces and the street to enable casual surveillance</li> <li>(iii) a clearly defined pedestrian entry point for visitors and customers that is separated from the driveway;</li> <li>(iv) established trees (including street trees) and other significant existing vegetation.</li> </ul> </li> </ul>	AO <sub>18.1</sub> Where not involving reuse of a premises development provides a minimum 3m wide landscaped garden strip along the frontage of the site.	<b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.
PO <sub>19</sub> Development for non-residential uses provide car parking and loading and servicing areas that: <ul style="list-style-type: none"> <li>(a) are located to minimise impact on any adjoining residential premises</li> <li>(b) are located behind the building, and hardstand areas do not dominate the streetscape;</li> </ul>	No acceptable outcome is nominated.	<b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>(c) prioritise the movement and safety of pedestrians along the frontage of the site, and between the street frontage and the entrance to the building;</p> <p>(d) are integrated into the building design and include screening and buffering to reduce negative impact on adjoining residential uses</p>		
<p>PO<sub>20</sub> Development for a sensitive use on land within 250m of land within the Medium Impact Industry Zone must not result in that use being exposed to industrial air, noise or odour emissions that impact on human health, amenity and wellbeing.</p>	<p>AO<sub>20.1</sub> The use is designed to ensure that:</p> <p>(a) the indoor noise objectives set out in the <i>Environmental Projection (Noise) Policy 2008</i> are met; and</p> <p>(b) the air quality objectives in the <i>Environmental Protection (Air) Policy 2008</i>, are met.</p> <p><i>Note: Design measure that may assist in achieving the acceptable outcome may include:</i></p> <p>(a) landscaping;</p> <p>(b) setbacks;</p> <p>(c) the orientation of buildings away from the industrial area; and</p> <p>(d) barriers, mounds and fencing; and/or screening.</p>	<p><b>Not Applicable.</b></p> <p>The proposed development is not located within 250m of the Medium Impact Industry Zone.</p>
<p>PO<sub>21</sub> Where located on land that is affected by environmental emissions generated by an operational rail corridor, sensitive development mitigates noise generated by the railway to the extent that it adversely impacts on the development.</p>	<p>No acceptable outcome is nominated.</p>	<p><b>Not Applicable.</b></p> <p>The proposed development is not located on land affected by a local rail corridor.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Development involving Animal Keeping (where for Stables) in the Clifford Park Stables Precinct</b>		
PO <sub>22</sub> The minimum site area for Animal Keeping (where for stables) is sufficient to: <ul style="list-style-type: none"> <li>(a) meet the needs of the use;</li> <li>(b) accommodate a dwelling house on the same premises; and</li> <li>(c) maintain an overall scale and intensity of development consistent with the character of the local area.</li> </ul>	AO <sub>22.1</sub> The site area for a Animal Keeping (where for stables) is a minimum of 1,200m <sup>2</sup> .	<b>Not Applicable.</b> The proposed development is not located within the Clifford Park Stables Precinct.
PO <sub>23</sub> Animal Keeping (where for stables) does not adversely affect the health and safety of residents of the site or adjoining premises and do not unduly impact on the amenity of the local area.	AO <sub>23.1</sub> No horse is stabled closer than 15m to a residential building on an adjoining lot. AO <sub>23.2</sub> Stable buildings are constructed with impervious reinforced concrete floors, gravity drained to the effluent collection/treatment point. AO <sub>23.3</sub> Exterior walls of buildings are constructed of sound absorbent material being brick, concrete, masonry or other similar material. AO <sub>23.4</sub> A person who is responsible for the supervision of the stables is accommodated on the premises at all times. AO <sub>23.5</sub> A minimum 1.8m high solid screen fence is provided on the site along all common boundaries. AO <sub>23.6</sub> Wastes are collected and disposed of daily. AO <sub>23.7</sub> All food/waste holding areas and receptacles are contained and covered. AO <sub>23.8</sub> A health management plan approved by Council identifies potential health and amenity hazards associated with the stables such as vermin and other pests, animal waste, other odour sources and the methods by which these hazards are to be addressed and managed.	<b>Not Applicable.</b> The proposed development is not located within the Clifford Park Stables Precinct.

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Low Density Residential Zone Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>24</sub> Residential development maintains an overall low density, domestic scale and intensity to: (a) maintain suitable development sites for Animal Keeping (where for stables) to co-locate with residential uses; and (b) limit conflicts between Animal Keeping (where for stables) and neighbouring residential uses.	AO <sub>24</sub> Accommodation activities are limited to Dwelling Houses and Community Residences.	<b>Not Applicable.</b> The proposed development is not located within the Clifford Park Stables Precinct.
<b>Development in the Park Residential Precinct</b>		
PO <sub>25</sub> Development does not create lots smaller than 2,250m <sup>2</sup> within the Park Residential Precinct.	No acceptable outcome is nominated.	<b>Not Applicable.</b> The proposed development is not located within the Park Residential Precinct.
PO <sub>26</sub> Effluent generated by a development is capable of being treated and disposed of on site.	No acceptable outcome is nominated.	<b>Not Applicable.</b> The proposed development is not located within the Park Residential Precinct.

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Height of Buildings and Other Structures</b>		
<p>PO<sub>1</sub> The height of buildings or other structures does not affect the operational efficiency of the Toowoomba Airport or Oakey Army Aviation Centre or create a hazard to the safe navigation of aircraft using the airport.</p>	<p>AO<sub>1.1</sub> Buildings and other structures in areas adjacent to the Toowoomba Airport do not penetrate the Airport Obstacle Limitation Surfaces shown on the Airport Environs Overlay maps.</p> <p>AO<sub>1.2</sub> Buildings or other structures (including private forestry plantations) in areas adjacent to the Oakey Army Aviation Centre shown on the Airport Environs Overlay maps do not exceed the following heights:</p> <p>(a) Area A – 0m;            (b) Area B – 7.5m;            (c) Area C – 15m;            (d) Area D – 45m; and            (e) Area F – 90m.</p> <p>AO<sub>1.3</sub> Any cranes or other equipment used during the construction do not exceed the heights set out in AO<sub>1.1</sub> or AO<sub>1.2</sub>.</p>	<p><b>Complies.</b>            The proposed development will not penetrate the Obstacle Limitation Surface (OLS) of the Toowoomba Airport.</p> <p><b>Not Applicable.</b>            The proposed development is not within proximity of the Oakey Army Aviation Centre.</p> <p><b>Complies.</b>            Equipment used in construction will not penetrate the OLS.</p>
<b>Acoustic Amenity</b>		
<p>PO<sub>2</sub> Development is appropriately located and designed to prevent adverse impacts from aircraft noise in accordance with Australian Standards AS2021 – Acoustics – Aircraft Noise Intrusion – Building Siting and Construction.</p>	<p>AO<sub>2.1</sub> Premises are not developed for:</p> <p>(a) dwelling house, multiple dwelling, dual occupancy, retirement facility, tourist park, hospital, health care services, relocatable home park, community residence, child care centre, educational establishment, residential care facility, resort complex, rooming accommodation or rural worker's</p>	<p><b>Not Applicable.</b>            The proposed development is not located within ANEF contours.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>accommodation on land within the 25 or higher ANEF contour;</p> <p>(b) short term accommodation, community use, hotel, place of worship, rooming accommodation or theatre, on land within the 30 or higher ANEF contour;</p> <p>(c) business activities or commercial uses on land within the 35 or higher ANEF contour.</p>	
<p>PO<sub>3</sub> Development for:</p> <p>(a) caretaker's accommodation, community residence, dwelling house, dwelling unit, multiple dwelling, dual occupancy, child care centre, retirement facility, tourist park, educational establishment, hospital, health care services, rooming accommodation, relocatable home park, residential care facility, resort complex or rural worker's accommodation on land situated between the 20 and 25 ANEF contours;</p> <p>(b) short term accommodation, club, community uses, hotel, place of worship or theatre, on land situated between the 20 and 30 ANEF contours; or</p> <p>(c) business activities or commercial uses on land situated between the 20 and 35 ANEF contours;</p> <p>incorporates effective noise attenuation measures which meet Australian Standard AS2021 - Acoustics - Aircraft Noise Intrusion – Building Siting and Construction and Australian Standard AS2107 – Acoustics – Recommended</p>	<p>No acceptable outcome is nominated.</p>	<p><b>Not Applicable.</b> The site is not located within ANEF Contours.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
design sound levels and reverberation times for building interiors.		
<b>Lighting and Emission Hazards</b>		
<p>PO<sub>4</sub> The development of premises does not cause an obstruction or other potential hazard to aircraft movement associated with the airports by way of:</p> <p>(a) introducing reflective surfaces, very bright lighting, or lighting similar to aerodrome lighting, which can distract or confuse aircraft pilots;</p> <p>(b) interfering with navigation or communication facilities; or</p> <p>(c) emissions that may effect pilot visibility or aircraft operations.</p>	<p>AO<sub>4.1</sub> Street lighting and outdoor lighting within the airport dangerous light area shown on the Airport Environs Overlay maps does not involve:</p> <p>(a) lighting that shines light above the horizontal;</p> <p>(b) coloured or flashing lights;</p> <p>(c) sodium lights; or</p> <p>(d) flare plumes.</p> <p>AO<sub>4.2</sub> Street lighting and outdoor lighting within the airport dangerous light area shown on the Airport Environs Overlay maps does not involve configurations of lights in straight parallel lines 500m to 1,000m long.</p> <p>AO<sub>4.3</sub> Roofs of buildings within the airport dangerous light area shown on Airport Environs Overlay maps are of a non-reflective colour or material.</p>	<p><b>Complies.</b> The proposed development does not involve street lighting and outdoor lighting will be limited to low level security light.</p> <p><b>Complies.</b> See response to AO<sub>4.1</sub>.</p> <p><b>Complies.</b> Roofs of the proposed development will not be of a reflective colour or material.</p>
<p>PO<sub>5</sub> Development and operational activities do not generate emissions within the airport obstacle limitation surface shown on the Airport Environs Overlay maps that may affect pilot visibility or aircraft operation by way of:</p> <p>(a) gas plumes;</p> <p>(b) particulate emissions (e.g. dust or smoke); or</p> <p>(c) electromagnetic field radiations.</p>	<p>In partial fulfilment of the performance outcome:</p> <p>AO<sub>5.1</sub> Any cleared vegetation is mulched or removed from the site and not burnt on-site.</p> <p>AO<sub>5.2</sub> Any gas plumes do not have peak vertical velocities of more than 4.3m/sec and/or oxygen content of less than 50% of ambient levels.</p>	<p><b>Complies.</b> Any vegetation removed as part of the development can be mulched or burnt off site.</p> <p><b>Not Applicable.</b> The proposed development will not generate gas plumes.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Development within the Public Safety Area</b>		
<p>PO<sub>6</sub> Development does not introduce or intensify uses within the public safety area shown on the Airport Environs Overlay maps which are likely to increase risks to public safety.</p>	<p>AO<sub>6.1</sub> Premises within the Public Safety Area are not developed for:</p> <ul style="list-style-type: none"> <li>(a) accommodation activities;</li> <li>(b) uses that attract large numbers of people (e.g. business, community or industrial activities involving large numbers of workers or customers such as shopping centres, child care centres, health care services, major sport, recreation and entertainment facility, place of worship or club);</li> <li>(c) institutional uses (e.g. educational establishment, hospital or detention facility);</li> <li>(d) uses involving the manufacture or depot storage of hazardous materials (e.g. fuel, lubricants and other flammable materials, chemical, explosives, and poisonous or noxious gases); and</li> <li>(e) utility installations being transport terminals.</li> </ul>	<p><b>Not Applicable.</b> The proposed development is not located within the Public Safety Area.</p>
<b>Potential Wildlife Hazards</b>		
<p>PO<sub>7</sub> The development of premises does not cause an obstruction or other potential hazard to aircraft movement associated with the airport by way of attracting wildlife to the area which could cause or contribute to bird-strike hazard.</p>	<p>AO<sub>7.1</sub> The following uses are not located within the 3km Wildlife Hazard Buffer Zone shown on the Airport Environs Overlay maps:</p> <ul style="list-style-type: none"> <li>(a) animal keeping (being a bird sanctuary or fauna reserve);</li> <li>(b) aquaculture;</li> <li>(c) cropping (being fruit cropping or turf farming);</li> <li>(d) intensive animal industries (being a piggery or feedlot);</li> </ul>	<p><b>Not Applicable.</b> The proposed development is not located within the 3km Wildlife Hazard Buffer.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(e) special industries (being an abattoir or food processing plant); and</p> <p>(f) commercial fish processing.</p> <p>AO<sub>7.2</sub> The following uses are located within the 3km Wildlife Hazard Buffer Zone shown on the Airport Environs Overlay maps only where all food/waste holding areas and receptacles are contained and covered:</p> <p>(a) animal keeping (being a riding school, the keeping, handling and racing of horses or stables);</p> <p>(b) community use (being show grounds);</p> <p>(c) outdoor sport and recreation (being a drive in theatre); and</p> <p>(d) food and drink outlet (being a drive-through facility).</p> <p>AO<sub>7.3</sub> The following uses are located between the 3km and 8km Wildlife Hazard Buffer Zones shown on the Airport Environs Overlay maps only where all food/waste holding areas and receptacles are contained and covered:</p> <p>(a) animal keeping (being a bird sanctuary or fauna reserve);</p> <p>(b) aquaculture;</p> <p>(c) cropping (being fruit cropping or turf farming);</p> <p>(d) intensive animal industries (being a piggery or feedlot);</p> <p>(e) special industries (being an abattoir or food processing plant); and</p> <p>(f) commercial fish processing.</p>	<p><b>Not Applicable.</b> The proposed development is not located within the 3km Wildlife Hazard Buffer.</p> <p><b>Complies.</b> The proposed development does not involve any of the uses listed in AO<sub>7.3</sub>.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Airport Environs Overlay Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO7.4 Utility installations (being waste facilities that involve the disposal of putrescible waste) only establish within the 13km Wildlife Hazard Buffer zone shown on the Airport Environs Overlay Maps.</p> <p>AO7.5 Landscaping does not include species that at maturity would intersect the Airport Obstacle Limitation Surfaces shown on the Airport Environs Overlay maps.</p>	<p><b>Not Applicable.</b> The proposed development does not involve a utility installation.</p> <p><b>Complies.</b> Species selected for landscaping will not penetrate the OLS.</p>
<b>Transient Aviation Activities</b>		
<p>PO<sub>8</sub> Development does not adversely impact on the operational airspace of the Toowoomba Airport or Oakey Army Aviation Centre.</p>	<p>AO<sub>8.1</sub> Development that includes activities involving transient intrusions such as parachuting, hot-air ballooning or hang gliding are not located within the operational airspace shown on the Airport Environs Overlay maps.</p>	<p><b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Where a Dual Occupancy</b>		
<p>PO<sub>1</sub> Dual Occupancy development is dispersed within residential neighbourhoods, not clustering in groups of more than three adjoining or opposite sites, and does not become the dominant form of housing within a street by occupying more than 20 percent of properties within a street block.</p>	<p>AO<sub>1.1</sub> In Greenfield Areas Dual Occupancy development occurs on designated Dual Occupancy lots.</p>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>
<p>PO<sub>2</sub> The development is designed in a manner that responds to the sites opportunities and constraints, including any existing building on the lot, and to the broader urban and streetscape contexts.</p> <p>Note: Development should be designed and documented by a suitably qualified professional (i.e. architect or building designer). Details of professional qualifications should be noted on drawings submitted with any development application</p>	<p>AO<sub>2.1</sub> The design of the development considers and responds to:</p> <p>(a) Site specific qualities including:</p> <ul style="list-style-type: none"> <li>(i) site topography and slope (contours at a minimum of 1m intervals);</li> <li>(ii) Views</li> <li>(iii) solar aspect and access;</li> <li>(iv) prevailing breezes</li> <li>(v) existing buildings on the site;</li> <li>(vi) site access</li> <li>(vii) services</li> <li>(viii) identification of buildings to be demolished;</li> <li>(ix) existing vegetation (locations to be surveyed and the diameter of the trunk at 1m above ground level shown. On highly buttressed trees the extent of buttressed roots is to be shown);</li> <li>(x) proposed position of new buildings overlaid</li> </ul> <p>(b) Nearby features including:</p> <ul style="list-style-type: none"> <li>(i) opportunities for on street visitor car parking;</li> <li>(ii) buildings on adjoining land</li> </ul>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(iii) important views from neighbouring properties</li> <li>(iv) features and character of adjoining land and buildings including doors &amp; windows</li> <li>(v) street fixtures;</li> <li>(c) Local qualities and amenities including:               <ul style="list-style-type: none"> <li>(i) available public transport;</li> <li>(ii) nearby amenities such as parks, shops</li> </ul> </li> </ul> <p>AO2.2 Site design retains all existing street trees.</p> <p>Note: See Figure 1 below for examples of site analysis and nearby feature plans.</p>	

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

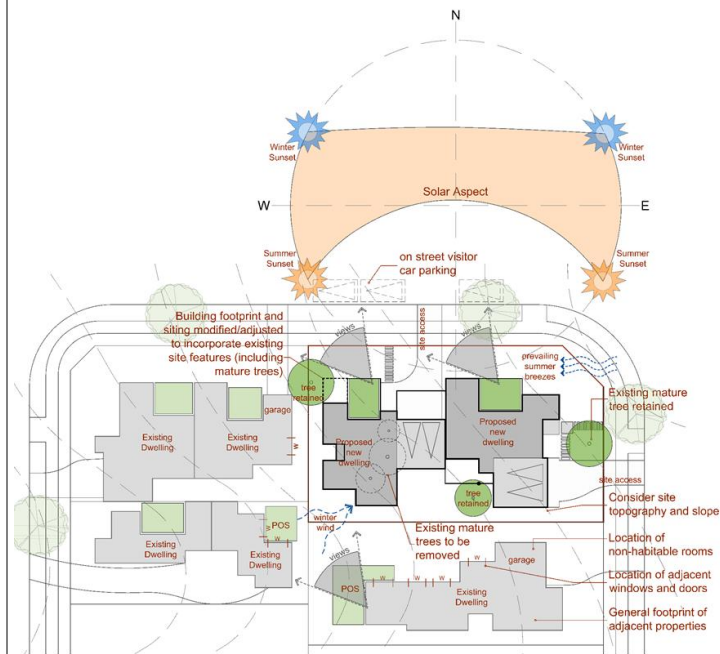


Figure 1

PO<sub>3</sub> Development responds to the slope of the land in the siting, design and form of buildings and structures to minimise requirements for cut and fill.

AO<sub>3.1</sub> Cut and fill beyond natural ground level does not exceed a vertical distance of 1m, unless contained within building walls and in accordance with the requirements outlined in SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.

Note: See Figure 2.

**Not Applicable.**

The proposed development is not a dual occupancy.

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	AO3.2 Batters and earth-retaining structures are constructed in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	

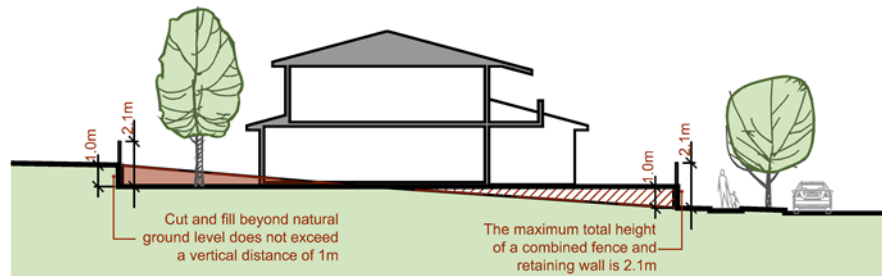


Figure 2

PO <sub>4</sub> Development is designed to facilitate the retention of any existing street tree adjacent to the site.	AO <sub>4.1</sub> Development does not compromise or require the removal of any street tree.	<b>Not Applicable.</b> The proposed development is not a dual occupancy.
PO <sub>5</sub> Development: (a) provides for the on-site infiltration of stormwater and minimises the additional burden on drainage infrastructure; (b) minimises the visual impact of hard surface areas; and (c) respects the existing or preferred neighbourhood character and responds to the features of the site.	AO <sub>5.1</sub> The impervious site cover does not exceed Table 1:	<b>Not Applicable.</b> The proposed development is not a dual occupancy.

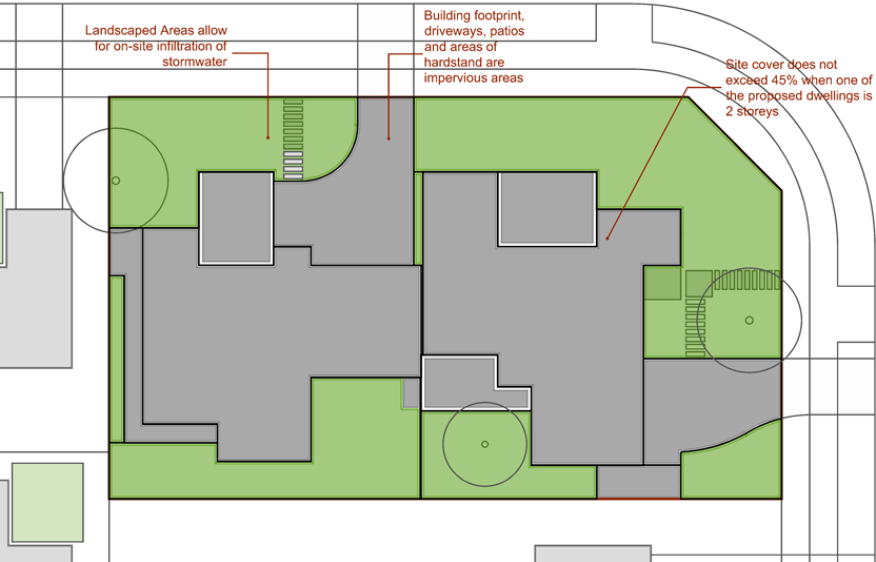
Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary																
	<table border="1" data-bbox="925 467 1321 715"> <thead> <tr> <th colspan="2">Table 1</th> </tr> <tr> <th>Average building height</th> <th>Maximum impervious site cover</th> </tr> </thead> <tbody> <tr> <td>If both of the proposed dwellings are 1 storey</td> <td>70%</td> </tr> <tr> <td>If one or both of the proposed dwellings is 2 storeys</td> <td>65%</td> </tr> </tbody> </table> <p data-bbox="801 730 1435 786">AO<sub>5.2</sub> No more than 50% of the front setbacks area is impervious.</p> <p data-bbox="801 791 1435 847">AO<sub>5.3</sub> Site cover does not exceed rates specified in Table 2.</p> <table border="1" data-bbox="884 860 1352 1102"> <thead> <tr> <th colspan="2">Table 2</th> </tr> <tr> <th>Dwelling Height</th> <th>Maximum site cover</th> </tr> </thead> <tbody> <tr> <td>If both of proposed dwellings are 1 storey</td> <td>50%</td> </tr> <tr> <td>If one of proposed dwellings is 2 storeys</td> <td>45%</td> </tr> </tbody> </table> <p data-bbox="801 1123 1025 1149">Note: See Figure 3.</p>	Table 1		Average building height	Maximum impervious site cover	If both of the proposed dwellings are 1 storey	70%	If one or both of the proposed dwellings is 2 storeys	65%	Table 2		Dwelling Height	Maximum site cover	If both of proposed dwellings are 1 storey	50%	If one of proposed dwellings is 2 storeys	45%	
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p><b>Figure 3</b></p> 	<p>AO<sub>6.1</sub> Landscaped areas are provided in accordance with Table 3:</p>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>
<p>PO<sub>6</sub> Areas of hardstand required for parking and pathways:</p> <ul style="list-style-type: none"> <li>(a) are minimised,</li> <li>(b) incorporate generous landscaping: <ul style="list-style-type: none"> <li>(i) to promote a garden streetscape character, and</li> <li>(ii) to minimise visual impacts of views to hardstand areas for occupants and the public, particularly in the front garden.</li> </ul> </li> </ul>		

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary																	
	<table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr> <th colspan="3">Table 3</th> </tr> <tr> <th rowspan="2">Building height</th> <th colspan="2">Minimum landscaped area</th> </tr> <tr> <th>Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:</th> <th>Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:</th> </tr> </thead> <tbody> <tr> <td>1-2 storeys</td> <td>10% site area</td> <td>15% site area</td> </tr> <tr> <td>3 storeys</td> <td>12.5% site area</td> <td>17.5% site area</td> </tr> <tr> <td>4+ storeys</td> <td>15% site area</td> <td>20% site area</td> </tr> </tbody> </table> <p>Note: Landscaped areas included in the calculations consist of garden beds and lawn areas throughout the site with a width of no less than 1.5m; Excludes all paved and other impervious areas such as driveways, patios, terraces, car parking spaces and bin and other utility areas.</p>	Table 3			Building height	Minimum landscaped area		Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:	Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:	1-2 storeys	10% site area	15% site area	3 storeys	12.5% site area	17.5% site area	4+ storeys	15% site area	20% site area	
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1-2 storeys	10% site area	15% site area																	
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	See Figure 4.	



Figure 4

<p>PO<sub>7</sub> The front building setback is consistent with the prevailing front setbacks of other residential buildings in the street.</p> <p>Note: The prevailing setback of the street is determined by the setbacks of the residential</p>	<p>AO<sub>7.1</sub> Buildings are setback from the road frontage:</p> <ul style="list-style-type: none"> <li>(a) within 10% of the average front setback of existing residential buildings within 100m of the site; or</li> <li>(b) where there is no adjoining buildings the front setback is provided in accordance with Table 4.</li> </ul>	<p><b>Not Applicable.</b></p> <p>The proposed development is not a dual occupancy.</p>
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary										
<p>buildings on any lot along the road within 100m of the site.</p>	<table border="1" data-bbox="846 469 1384 887"> <thead> <tr> <th colspan="2" data-bbox="846 469 1384 520">Table 4</th> </tr> <tr> <th data-bbox="846 520 1155 571">Building element</th> <th data-bbox="1155 520 1384 571">Minimum setback</th> </tr> </thead> <tbody> <tr> <td data-bbox="846 571 1155 655">Solid building wall (other than a garage)</td> <td data-bbox="1155 571 1384 655">4.0m</td> </tr> <tr> <td data-bbox="846 655 1155 740">Solid building wall (where for a garage)</td> <td data-bbox="1155 655 1384 740">5.5m</td> </tr> <tr> <td data-bbox="846 740 1155 887">Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)</td> <td data-bbox="1155 740 1384 887">3.5m (or 3m where secondary road frontage)</td> </tr> </tbody> </table> <p data-bbox="801 927 1025 959">Note: See Figure 5.</p>	Table 4		Building element	Minimum setback	Solid building wall (other than a garage)	4.0m	Solid building wall (where for a garage)	5.5m	Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)	3.5m (or 3m where secondary road frontage)	
Table 4												
Building element	Minimum setback											
Solid building wall (other than a garage)	4.0m											
Solid building wall (where for a garage)	5.5m											
Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)	3.5m (or 3m where secondary road frontage)											

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

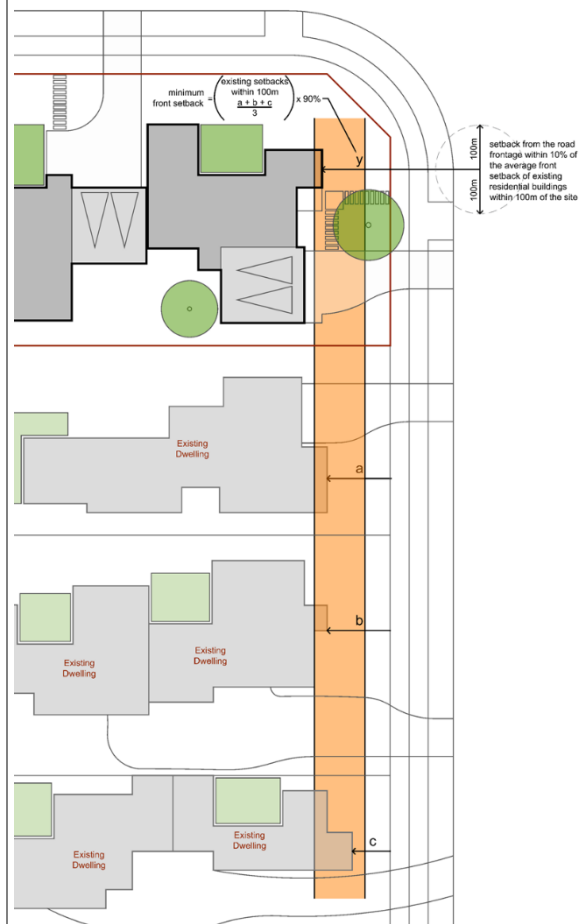


Figure 5

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>8</sub> Side and rear building setbacks:</p> <ul style="list-style-type: none"> <li>(a) are appropriate to the scale of the development and the intended character of the zone and/or precinct and local plan in which the site is located;</li> <li>(b) provide for adequate daylight for habitable rooms and open space areas on and adjoining the site; and</li> <li>(c) are sufficient to minimise overshadowing and overlooking of adjoining premises.</li> </ul>	<p>AO<sub>8.1</sub> Any part of a building built to a side boundary:</p> <ul style="list-style-type: none"> <li>(a) must not exceed a maximum height of 3.5m;</li> <li>(b) must not exceed a maximum length of 9m;</li> <li>(c) must be a minimum distance of 2.4m from any other wall built to the side boundary (whether on the site or an adjoining site) or be joined to another built to boundary wall if on a adjoining site; and</li> <li>(d) must not exceed a maximum total length of 50% of the side boundary.</li> </ul> <p>OR</p> <p>AO<sub>8.2</sub> Buildings are set back from side boundaries in accordance with the following:</p> <ul style="list-style-type: none"> <li>(a) For lots with frontages less than 20m:               <ul style="list-style-type: none"> <li>(i) 1.2m for any part of the building up to 3.5m high; and</li> <li>(ii) 2m for any part of the building above 3.5m high.</li> </ul> </li> <li>(b) For lots with frontages greater than 20m:               <ul style="list-style-type: none"> <li>(i) 1.8m for any part of the building up to 3.5m high; and</li> <li>(ii) 2.4m for any part of the building above 3.5m high.</li> </ul> </li> </ul> <p>AO<sub>8.3</sub> Buildings are set back from the rear boundary a minimum of 3m.</p> <p>AO<sub>8.4</sub> Development ensures that solar access to habitable rooms and private open space of adjoining premises: -</p> <ul style="list-style-type: none"> <li>(a) achieves a minimum of 3 hours of sunlight between 9am and 3pm on June 21; or</li> </ul>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(b) is not further reduced where existing solar access is less than that specified in (a).</p> <p>Note: See Figure 6.</p>	

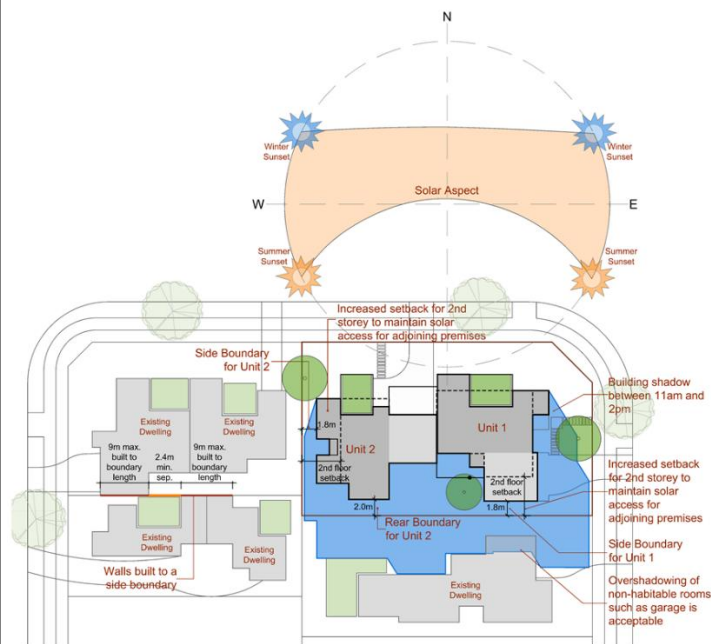


Figure 6

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>9</sub> Development provides adequate and suitable private open space to meet the needs of occupants for privacy, relaxation and entertainment and to accommodate visitors.</p>	<p>AO<sub>9.1</sub> Private open space has a minimum area of 16m<sup>2</sup> and minimum dimensions of 4 metres by 4 metres.</p> <p>AO<sub>9.2</sub> Private open space is:</p> <ul style="list-style-type: none"> <li>(a) is oriented to receive at least three hours of sun between 9am and 5pm on 21 June.</li> <li>(b) directly accessible from a living area or kitchen;</li> <li>(c) fenced or landscaped to achieve privacy for occupants;</li> <li>(d) clear of bins, clothes lines, hot water systems, air conditioning units, above ground water tanks and associated infrastructure and other forms of ancillary equipment and</li> <li>(e) not located within the front setback area.</li> </ul> <p>Note: See Figure 7.</p>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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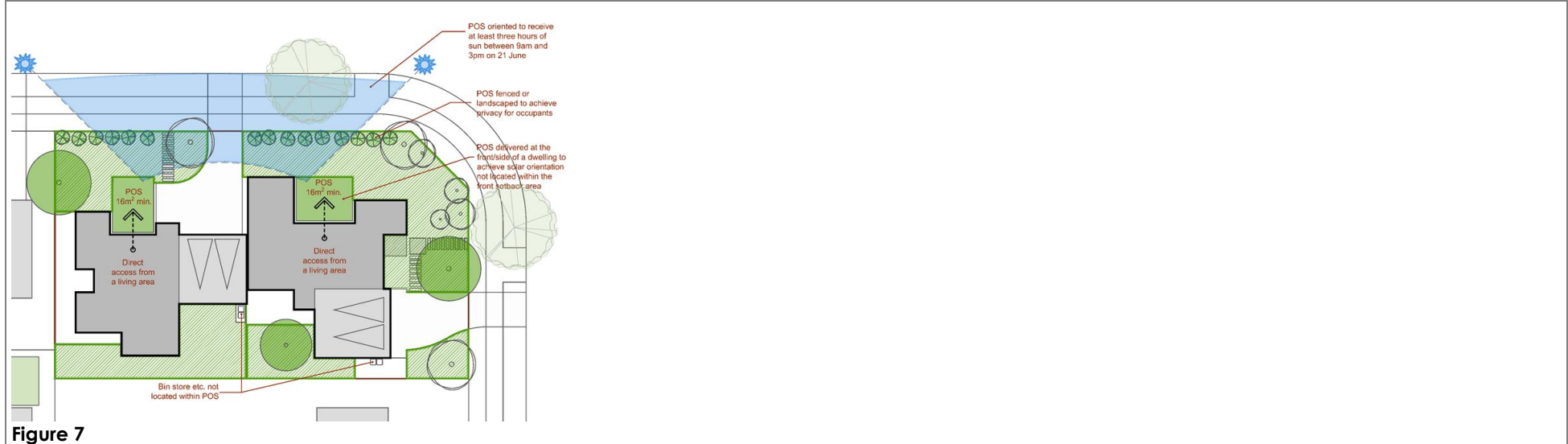


Figure 7

<p>PO<sub>10</sub> All car parking is designed to accommodate a standard car (B85 vehicle) and incorporates adequate manoeuvring area to parking spaces (including carports and garages).</p>	<p>AO<sub>10.1</sub> Design of the site layout must ensure a B85 vehicle (as defined in AS 2890.1:2004 Parking facilities - Off-street car parking) is able to enter an enclosed garage or visitor car park in a single forward manoeuvre and exit in a single reverse manoeuvre.</p>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>
<p>PO<sub>11</sub> Development provides resident and visitor car parking which is integrated into the site and building design and:</p> <ul style="list-style-type: none"> <li>(a) does not negatively impact the amenity of the site, adjoining sites or streetscape;</li> <li>(b) is not a prominent visual feature from the street or other public spaces; and</li> </ul>	<p>AO<sub>11.1</sub> Parking is only provided within the front setback area where it is:</p> <ul style="list-style-type: none"> <li>(a) located on the driveway directly in front of a carport or garage; and</li> <li>(b) not covered.</li> </ul> <p>Note: Where car parking is provided in accordance with AO<sub>11.1</sub>(a) the driveway should have a</p>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(c) contributes to the establishment of a garden setting by avoiding large areas of visible hardstand.</p>	<p>minimum length of 5 metres to avoid parked vehicles extending beyond the site boundary.</p> <p>AO<sub>11.2</sub> Where car parking is provided below ground:</p> <p>(a) Access ramps are not located within 6m of the street boundary and do not exceed an opening width of 6m; and</p> <p>(b) Entry to the site for pedestrians is provided separate from vehicle entries by way of a safe, well lit and clearly identifiable pathway or gatehouse;</p> <p>AO<sub>11.3</sub> All car parking is freely available for use by to the applicable occupants and visitors of the development.</p> <p>Note: See Figure 8.</p>	

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

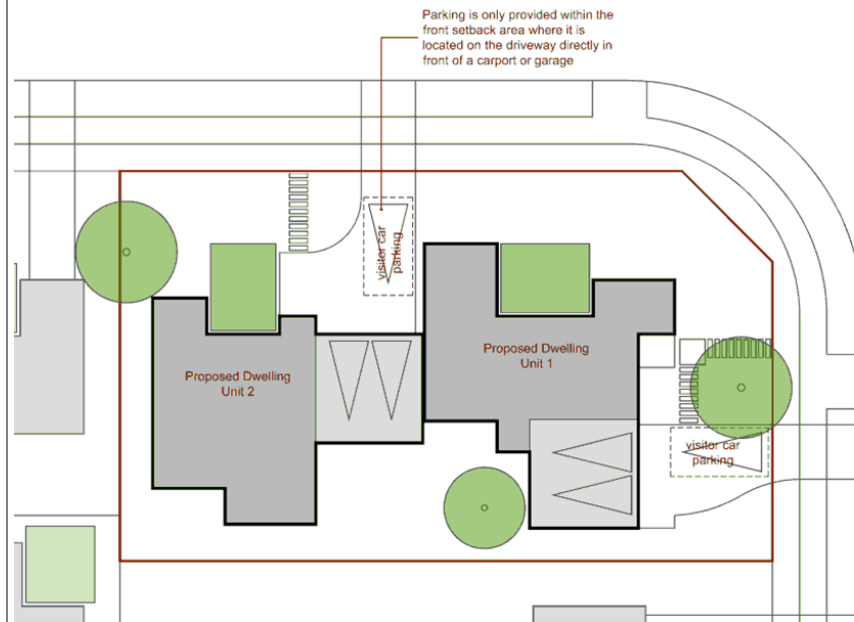


Figure 8

PO<sub>12</sub> Garages and carports (except those with frontage to a rear lane) do not dominate the street frontage.

AO<sub>12.1</sub> Garages and/or carports facing a street make up no more than 30% of the width of the site.  
 AO<sub>12.2</sub> Garages and/or carports facing a street are setback a minimum of 1 metre behind the main face of the dwelling.  
 Note: See Figures 9 and 10.

**Not Applicable.**  
 The proposed development is not a dual occupancy.

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

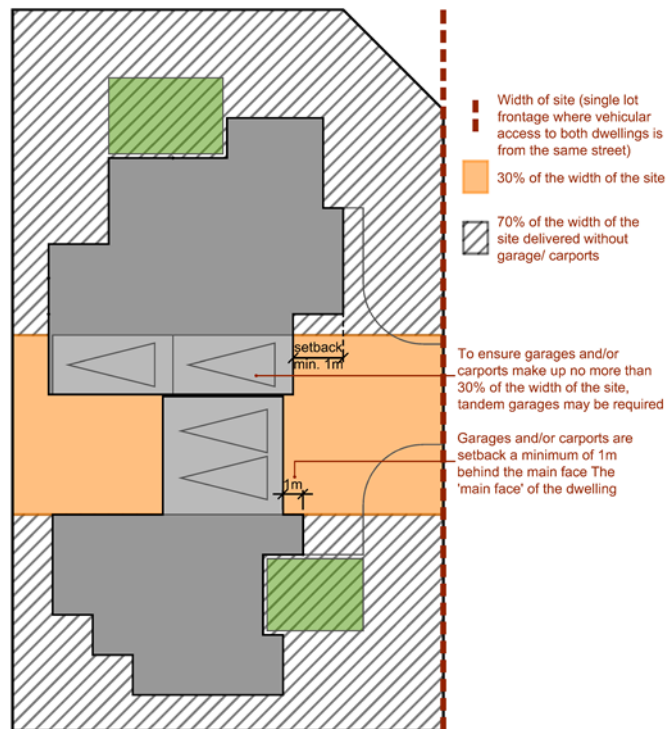


Figure 9

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

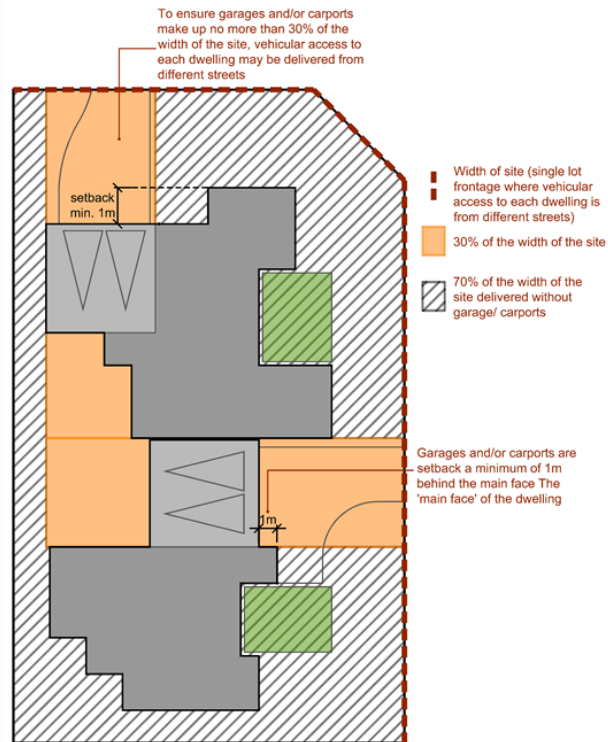


Figure 10

PO13 Safe and equitable access for both pedestrians and vehicles is provided, to and from buildings and the street whilst minimising the extent and number of driveways.

AO13.1 Where vehicular access to both dwellings is from the same street the driveway is shared and has a maximum crossover width of 5m.

**Not Applicable.**  
The proposed development is not a dual occupancy.

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>13.2</sub> Where vehicular access to each dwelling is from different streets the driveway has a maximum crossover width of 3m.</p> <p>AO<sub>13.3</sub> Where the site abuts a lane vehicle access is provided from the lane.</p> <p>AO<sub>13.4</sub> Separate pedestrian access is provided to each dwelling, clearly distinguishable from the vehicle zone.</p> <p>Note: See Figure 11.</p>	

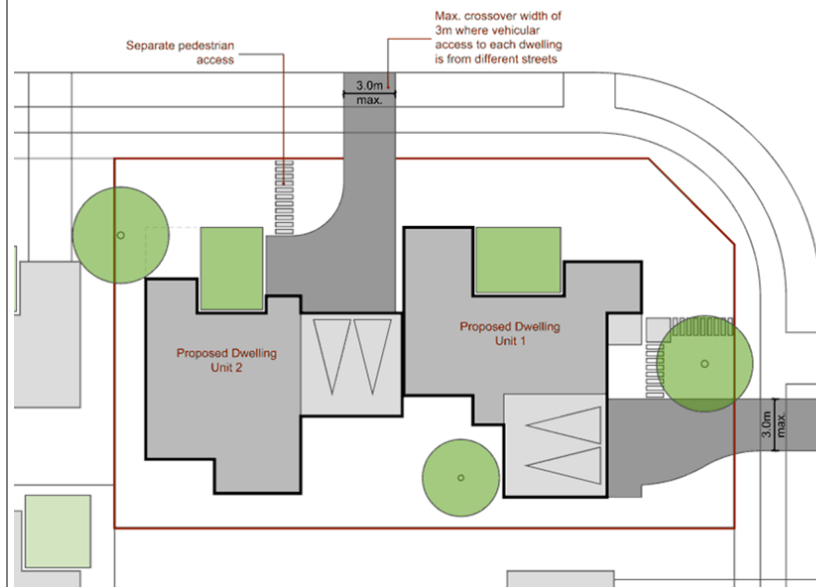


Figure 11

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>14</sub> The privacy of residents of the development or adjoining premises is protected when they are inside a dwelling or in a private open space area.</p>	<p>AO<sub>14.1</sub> Habitable room windows of a dwelling are separated a minimum of 9m from a habitable room window or private open space of another dwelling on the same or an adjoining site.</p> <p>OR</p> <p>AO<sub>14.2</sub> Where there is a direct view within 9m into a habitable room or private open space of another dwelling on the same or an adjoining site</p> <p>(a) the elements with the view such as windows, balconies, or terraces of a dwelling must be screened in accordance with AO<sub>14.3</sub>; or</p> <p>(b) the Windows have translucent glazing up to or sill heights of at least 1.5m from the internal floor level; or</p> <p>(c) a solid fence or masonry wall with a minimum height of 1.5m is positioned between the elements with the view.</p> <p>AO<sub>14.3</sub> Where screening is used, it:</p> <p>(a) is a solid translucent screen or a louvre perforated panel, trellis or the like that has a maximum of 50% open to solid ratio; and</p> <p>(b) is a fixed, permanent element of a design that complements the style, detailing and materials of the development.</p> <p>Note: See Figure 12.</p>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

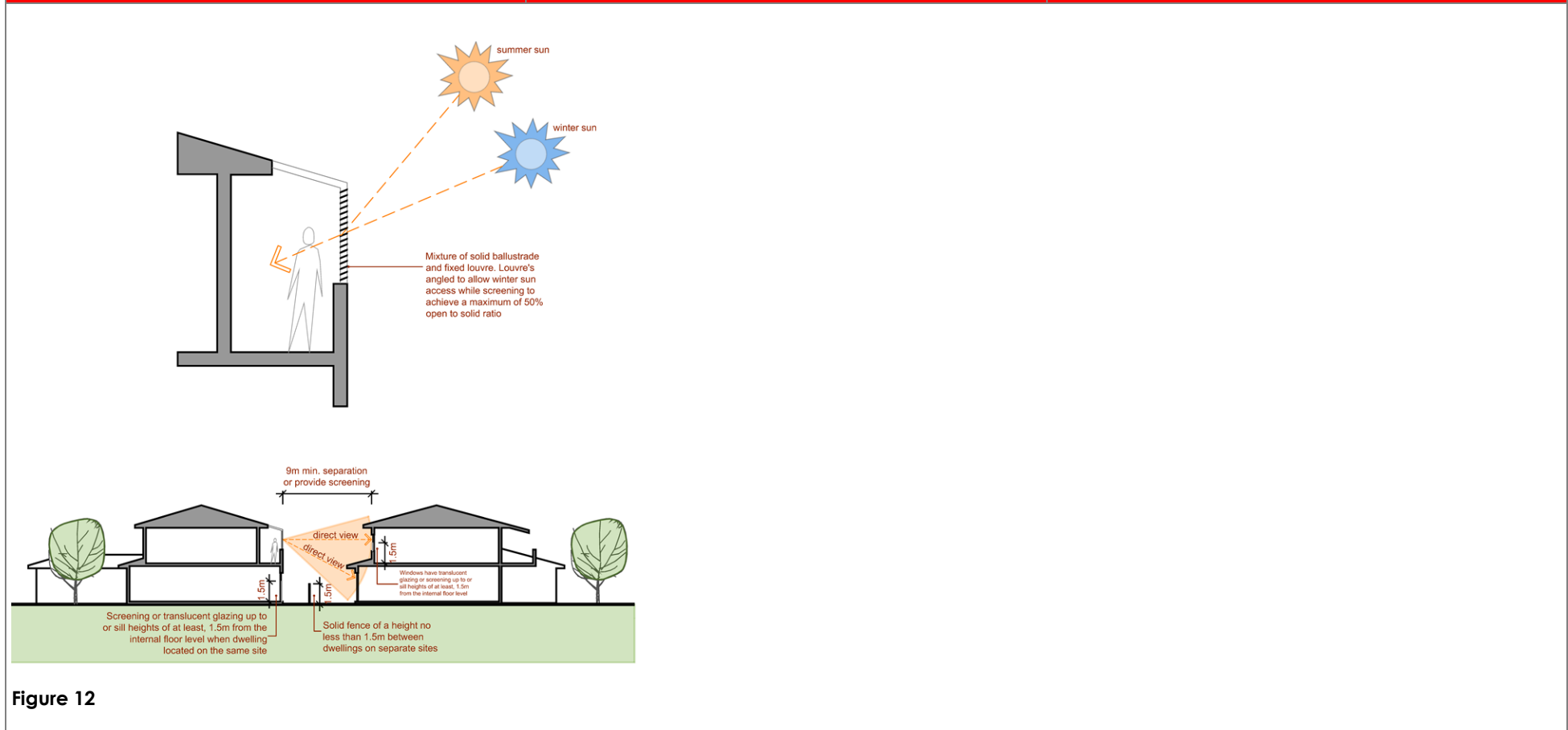


Figure 12

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>15</sub> The development is designed so that occupants are not adversely affected by adjoining non-residential uses.</p>	<p>AO<sub>15.1</sub> A 1.8m high solid screen fence and 2m wide vegetated buffer is provided along any section of the site boundary that is common with vehicle movement and/or parking areas associated with a non-residential use on an adjoining site.</p>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>
<p>PO<sub>16</sub> Buildings are designed to a high aesthetic standard providing:</p> <ul style="list-style-type: none"> <li>(a) prominent features towards the street frontage;</li> <li>(b) detailing and finishes appropriate to the scale, quality and character of the street and to help differentiate between dwellings</li> <li>(c) opportunities for informal surveillance of the street and other public spaces;</li> <li>(d) clear and visible entries; and elements that complement or enhance the character of the local neighbourhood.</li> </ul>	<p>AO<sub>16.1</sub> Buildings:</p> <ul style="list-style-type: none"> <li>(a) are oriented to and address the street frontage;</li> <li>(b) have front entries that provide full weather protection and where within 10m of any street, have clearly defined front entries that are visible from the street;</li> <li>(c) where within 10m of any street; have one or more habitable rooms facing the street boundary that have windows or doors with a minimum area of clear glazing of 0.25m<sup>2</sup> for each linear metre of street frontage; and</li> <li>(d) are designed to deliver depth in the façade facing the street or public space with a variation not less than 2m deep for a minimum 50% of the building width at ground level.</li> </ul> <p>Note: Variation in building façade may be achieved through delivery of balconies, patios or building footprint articulation. The roofline over a balcony or patio may remain consistent with the dominant roof form.</p> <p>Note: See Figure 13.</p>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

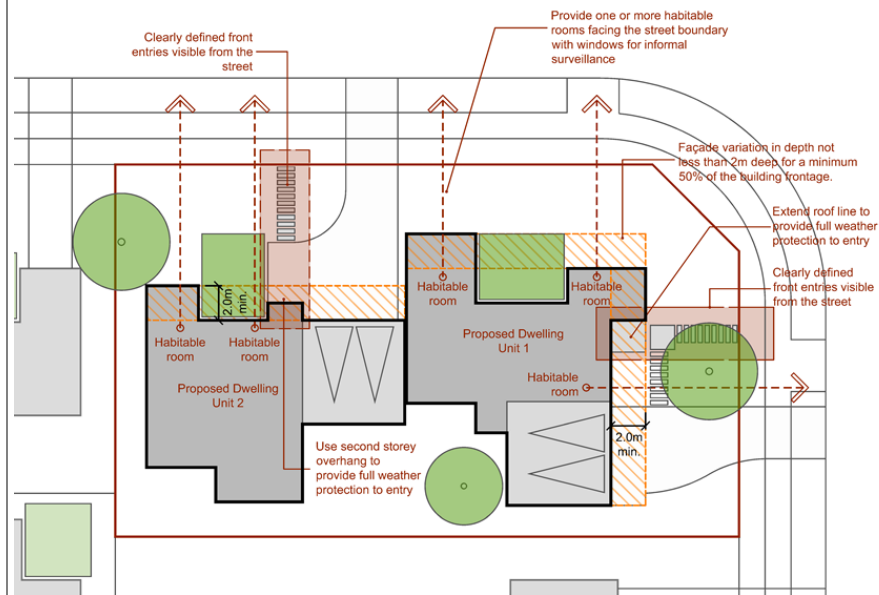


Figure 13

PO<sub>17</sub> Development incorporates typical domestic detailing to achieve a residential scale living environment with aesthetic standards and amenity for occupants similar to those of single dwellings in the neighbourhood.

AO<sub>17.1</sub> The maximum single wall length of a building is 32m with offsets of 600mm minimum provided every 7.5m;

Note: Building walls separated by a distance of less than 10% of their combined length are considered as one wall.

**Not Applicable.**

The proposed development is not a dual occupancy.

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>17.2</sub> Building bulk is minimised through articulation and other measures, including the provision of at least one (1) element from at least three (3) of the following element groups:</p> <ul style="list-style-type: none"> <li>(a) verandahs, porches, pergolas or balconies;</li> <li>(b) roof overhangs (minimum 600mm deep);</li> <li>(c) window hoods/screens;</li> <li>(d) awning and shade structures; or</li> <li>(e) 600mm vertical gutter offset to create roof articulation.</li> </ul> <p>Note: See Figure 14.</p>	

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Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary



Figure 14

PO<sub>18</sub> Development provides landscaping that contributes to the landscape character, shade and the micro-climate of the neighbourhood

AO<sub>18.1</sub> One (1) tree capable of achieving a height of at least six (6) metres and canopy spread of at least five (5) metres at maturity is provided.

**Not Applicable.**

The proposed development is not a dual occupancy.

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>and site through the retention or provision of at least one canopy tree.</p>		
<p>PO<sub>19</sub> The treatment of fencing and retaining walls in all situations reinforces the high quality, amenity and safety of the development.</p>	<p>AO<sub>19.1</sub> The maximum total height of a combined fence and retaining wall is 2.1m, as measured from the base of the retaining wall to the top of the fence.</p> <p>AO<sub>19.2</sub> Where a combined fence and retaining wall is located on the front boundary, the fence has a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>AO<sub>19.3</sub> Retaining walls and fences are constructed of high quality materials and contribute to residential amenity.</p> <p>AO<sub>19.4</sub> Any fence on the front boundary has a minimum 30% transparency when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>Note: Where retaining walls are set off the property boundary by a minimum of 600mm, they are not considered to be combined with the fencing. (See Figure 15.)</p>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>

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Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development

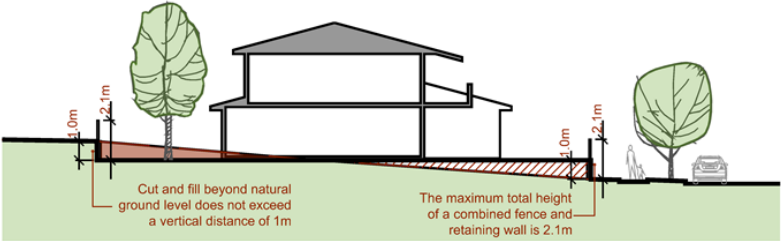
Performance outcomes	Acceptable outcomes	Compliance summary
 <p>Cut and fill beyond natural ground level does not exceed a vertical distance of 1m</p> <p>The maximum total height of a combined fence and retaining wall is 2.1m</p>		
<p>PO<sub>20</sub> Front fencing contributes to privacy, screening, security, and street character while maintaining a visual relationship between dwellings and public spaces.</p>	<p>AO<sub>20.1</sub> Fences located on a front property boundary or a common boundary with an adjacent park or within the front setback area:</p> <ul style="list-style-type: none"> <li>(a) have a maximum height of 1.2 metres; or</li> <li>(b) have a maximum height of 1.5 metres and a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).</li> </ul> <p>Note: In instances where the fence is located in conjunction with a retaining wall refer PO<sub>19</sub>.</p>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>
<p>PO<sub>21</sub> Stormwater discharge from the site is to a lawful point of discharge as defined in the Queensland Urban Drainage Manual (QUDM), without the use of pumped or charged pipe systems, and not to private land other than to an easement for stormwater purposes befitting the site and</p>	<p>AO<sub>21.1</sub> The site:</p> <ul style="list-style-type: none"> <li>(a) has a natural surface with an elevation that is higher than the abutting road and enables stormwater to drain gravitationally to the abutting road via subsurface pipes; or</li> <li>(b) has a natural surface with an elevation that is higher than Council's stormwater</li> </ul>	<p><b>Not Applicable.</b> The proposed development is not a dual occupancy.</p>

Figure 15

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
allowing discharge to a lawful point of discharge in land over which Council has tenure or control.	<p>drainage network in the abutting road and enables stormwater to drain gravitationally to the stormwater drainage network in the abutting road via subsurface pipes; or</p> <p>(c) has lawful access to an inter allotment drainage network with available capacity to meet the requirements of the development.</p>	
<b>Where a Dwelling House on a Hatchet Lot</b>		
<p>PO<sub>22</sub> The configuration and finish of the access strip/easement:</p> <p>(a) enhances the streetscape qualities of the area and any adjoining premises;</p> <p>(b) enhances the residential amenity of the area; and</p> <p>(c) contributes to a high quality residential outcome for the site, by the incorporation of landscaping, provision for adequate setbacks and use of high quality materials.</p>	<p>AO<sub>22.1</sub> Any area of hardstand, including the driveway, within the access strip / easement:</p> <p>(a) has a maximum width of 3m; and</p> <p>(b) is sealed with a rigid pavement.</p> <p>AO<sub>22.2</sub> The access strip/easement has a landscaped area/s with a combined minimum width of 2m for its full length.</p> <p>AO<sub>22.3</sub> The driveway landscaping must continue for the full length of the driveway for a minimum width of 0.5m.</p> <p>AO<sub>22.4</sub> Any fence on the inside boundary of the access strip / easement has a maximum height of one (1) metre for the first six (6) metres from the front boundary.</p>	<p><b>Not Applicable.</b></p> <p>The proposed development does not involve a hatchet lot.</p>
<p>PO<sub>23</sub> The dwelling does not result in a reduction of amenity, privacy, or loss of solar access for any adjoining property.</p>	<p>AO<sub>23.1</sub> The dwelling does not exceed a maximum height of 2 storeys or 8.5m above ground level.</p> <p>AO<sub>23.2</sub> The dwelling is setback a minimum of 2m from all boundaries.</p> <p>AO<sub>23.3</sub> North-facing windows to habitable rooms and/or the private outdoor space of an adjoining premises does not have sunlight</p>	<p><b>Not Applicable.</b></p> <p>The proposed development does not involve a hatchet lot.</p>

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**Medium Density Residential Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	reduced to less than 3 hours of solar gain between 9 a.m. and 3 p.m. on 21 June.	
<p>PO<sub>24</sub> Stormwater discharge from the site is to a lawful point of discharge as defined in the Queensland Urban Drainage Manual (QUDM), without the use of pumped or charged pipe systems, and not to private land other than to an easement for stormwater purposes befitting the site and allowing discharge to a lawful point of discharge in land over which Council has tenure or control.</p> <p>Note: Land over which Council has tenure or control does not include Council's open space network.</p>	<p>AO<sub>24.1</sub> The site:</p> <ul style="list-style-type: none"> <li>(a) has a natural surface with an elevation that is higher than the abutting road and enables stormwater to drain gravitationally to the abutting road via subsurface pipes; or</li> <li>(b) has a natural surface with an elevation that is higher than Council's stormwater drainage network in the abutting road and enables stormwater to drain gravitationally to the stormwater drainage network in the abutting road via subsurface pipes; or</li> <li>(c) has lawful access to an inter allotment drainage network with available capacity to meet the requirements of the development.</li> </ul>	<p><b>Not Applicable.</b></p> <p>The proposed development does not involve a hatchet lot.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

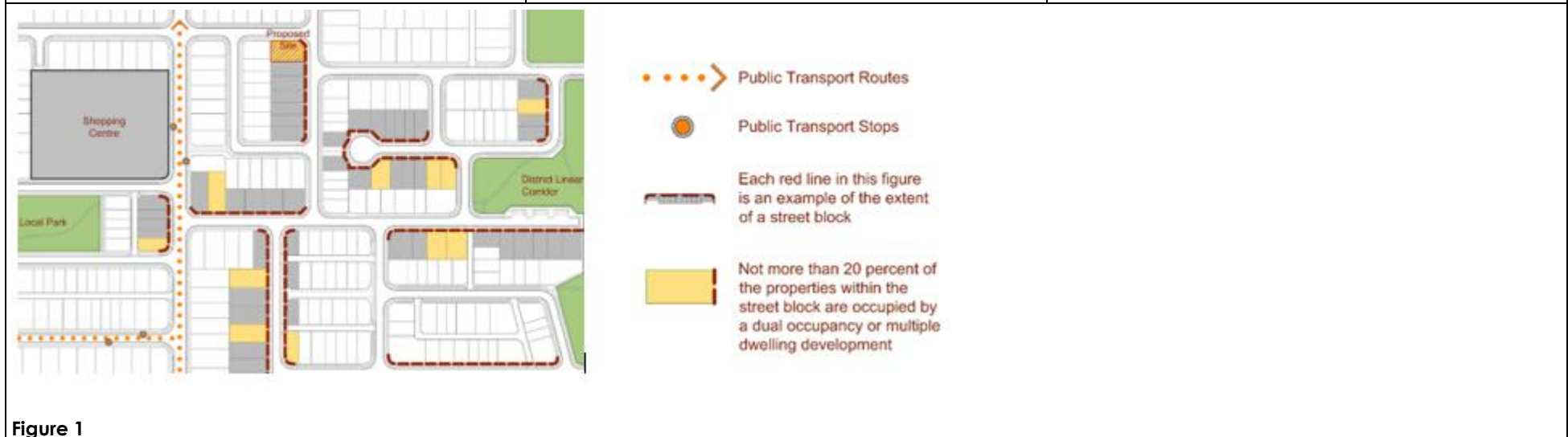
**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>1</sub> The development is designed in a manner that responds to the site's opportunities and constraints and to the broader urban and streetscape contexts.</p> <p>Note: Development should be designed and documented by a suitably qualified professional (i.e. architect or building designer). Details of professional qualifications should be noted on drawings submitted with any development application</p>	<p>AO<sub>1.1</sub> The design of the development considers and responds to:</p> <p>(a) Site specific qualities including:</p> <ul style="list-style-type: none"> <li>(i) site topography and slope (contours at a minimum of 1m intervals);</li> <li>(ii) Views</li> <li>(iii) solar aspect and access;</li> <li>(iv) prevailing breezes</li> <li>(v) existing buildings on the site;</li> <li>(vi) site access</li> <li>(vii) services</li> <li>(viii) identification of buildings to be demolished;</li> <li>(ix) existing vegetation (locations to be surveyed and the diameter of the trunk at 1m above ground level shown. On highly buttressed trees the extent of buttressed roots is to be shown);</li> <li>(x) proposed position of new buildings overlaid</li> </ul> <p>(b) Nearby features including:</p> <ul style="list-style-type: none"> <li>(i) opportunities for on street visitor car parking;</li> <li>(ii) buildings on adjoining land</li> <li>(iii) important views from neighbouring properties</li> <li>(iv) features and character of adjoining land and buildings including doors &amp; windows</li> <li>(v) street fixtures;</li> </ul> <p>(c) Local qualities and amenities including:</p> <ul style="list-style-type: none"> <li>(i) available public transport; and</li> </ul>	<p><b>Complies.</b></p> <p>The proposed development has been designed to appropriately address the items identified at AO<sub>1.1</sub> as follows:</p> <p>(a) The proposal has been designed by a suitably qualified person to a high standard and provides a contemporary building design which promotes amenity for residents and the surrounding locality. The design also considers the existing dwelling units, contours, solar access, breezes, views, constraints, opportunities and functionality of the proposed development to ensure the proposal is suited to the broader context of the site.</p> <p>(b) Nearby features including:</p> <ul style="list-style-type: none"> <li>• The design complements and enhances the character of the local neighbourhood.</li> <li>• The site is strategically located within walking distance to key services and facilities.</li> <li>• The proposal does not impact on the amenity of existing dwelling units, adjoining residences or impact the operation of nearby services or facilities.</li> </ul> <p>(c) The connection of the development to active and public transport infrastructure and nearby amenities such as parks and shops.</p>

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Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>(ii) nearby amenities such as parks, shops. AO1.2 Site design retains all existing street trees. Note: See Figures 1 and 2.</p>	





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Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>2</sub> Development responds to the slope of the land in the siting, design and form of buildings and structures to minimise requirements for cut and fill.</p>	<p>AO<sub>2.1</sub> Cut and fill beyond natural ground level does not exceed a vertical distance of 1m, unless contained within building walls and in accordance with the requirements outlined in SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure. The extent of cut and fill must be shown on the proposed site plan and elevations.</p> <p>AO<sub>2.2</sub> Batters and earth-retaining structures are constructed in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.</p> <p>Note: See Figure 3.</p>	<p><b>Complies.</b> The proposed development is located on a relatively flat site. Cut and fill beyond natural ground level exceeding a vertical distance of 1m is not proposed.</p> <p><b>Complies.</b> Any batters and earth-retaining structures, if required, will be constructed in accordance with the relevant requirements.</p>

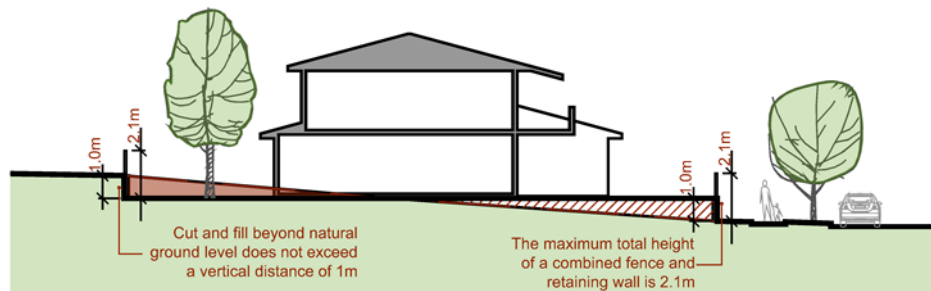


Figure 3

<p>PO<sub>3</sub> Development is designed to facilitate the retention of any existing street tree adjacent to the site.</p>	<p>AO<sub>3.1</sub> Development does not compromise or require the removal of any street tree.</p>	<p><b>Not Applicable.</b> There is no street trees located on the Benjamin Street frontage.</p>
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PO<sub>4</sub> Development:

- (a) provides for the on-site infiltration of stormwater and minimises the additional burden on drainage infrastructure;
- (b) minimises the visual impact of hard surface areas; and
- (c) respects the existing or preferred neighbourhood character and responds to the features of the site.

AO<sub>4.1</sub> The impervious site cover does not exceed Table 1:

Table 1	
Average building height	Maximum impervious site cover
If more than 50% of the proposed dwellings are 1 storey	70%
If more than 50% of the proposed dwellings are 2 storeys	65%
If more than 50% of the proposed dwellings are 3 or more storeys	60%

AO<sub>4.2</sub> No more than 50% of the front setbacks area is impervious.

AO<sub>4.3</sub> Site cover does not exceed rates specified in Table 2.

Table 2	
Dwelling Height	Maximum site cover
If more than 50% of proposed dwellings are 1 storey	50%
If more than 50% of proposed dwellings are 2 storeys	45%
If more than 50% of proposed dwellings are 3 storeys	40%
If more than 50% of proposed dwellings are 4 storeys or more	35%

Note: See Figure 4.

**Performance Solution.**

Reference is made to the responses to PO7 and PO8 of the Low Density Residential Zone Code.

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

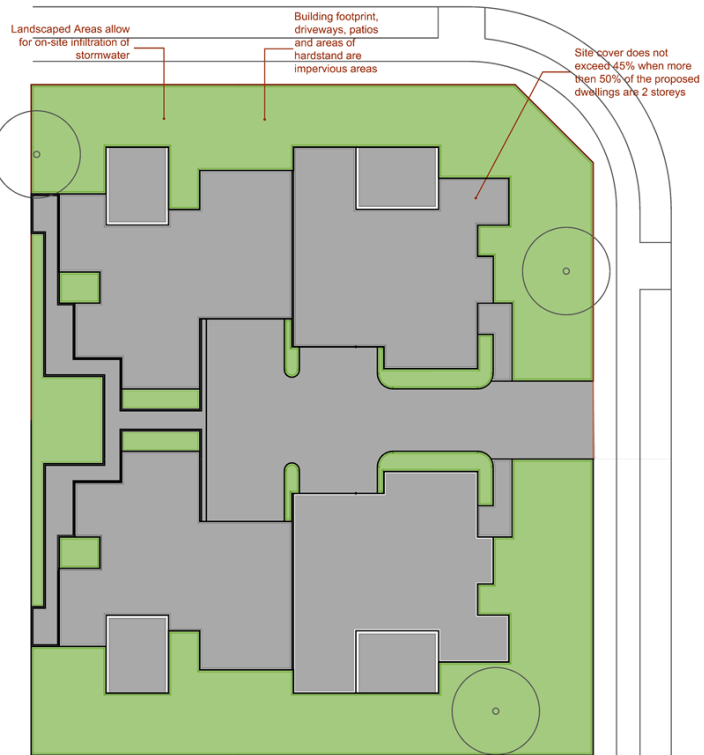


Figure 4

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary																	
<p>PO<sub>5</sub> Areas of hardstand required for parking and pathways:</p> <ul style="list-style-type: none"> <li>(a) are minimised,</li> <li>(b) Incorporate generous landscaping:               <ul style="list-style-type: none"> <li>(i) to promote a garden streetscape character, and</li> <li>(ii) (to minimise visual impacts of views to hardstand areas for occupants and the public, particularly in the front garden.</li> </ul> </li> </ul>	<p>AO<sub>5.1</sub> Landscaped areas are provided in accordance with Table 3:</p> <table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr> <th colspan="3">Table 3</th> </tr> <tr> <th rowspan="2">Building height</th> <th colspan="2">Minimum landscaped area</th> </tr> <tr> <th>Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:</th> <th>Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:</th> </tr> </thead> <tbody> <tr> <td>1-2 storeys</td> <td>10% site area</td> <td>15% site area</td> </tr> <tr> <td>3 storeys</td> <td>12.5% site area</td> <td>17.5% site area</td> </tr> <tr> <td>4+ storeys</td> <td>15% site area</td> <td>20% site area</td> </tr> </tbody> </table> <p>Note: Landscaped areas included in the calculations consist of garden beds and lawn areas</p>	Table 3			Building height	Minimum landscaped area		Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:	Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:	1-2 storeys	10% site area	15% site area	3 storeys	12.5% site area	17.5% site area	4+ storeys	15% site area	20% site area	<p><b>Performance Solution.</b> The proposed development involves the establishment of an additional dwelling unit at the rear of the site and does not propose any changes to the existing landscaping within the front setback. The proposed development has been designed to minimise the areas of hardstand where feasible by seeking to utilise the existing driveway and establishing landscaping in and around the building similar to the surrounding residential premises. The proposed development will not adversely impact on the visual appearance of the streetscape as it is located behind the existing dwelling units.</p>
Table 3																			
Building height	Minimum landscaped area																		
	Where 80% or more of the front setback is landscaped area additional landscaping for the balance of the site must be a minimum of:	Where less than 80% of the front setback area is landscaped additional landscaping for the balance of the site must be a minimum of:																	
1-2 storeys	10% site area	15% site area																	
3 storeys	12.5% site area	17.5% site area																	
4+ storeys	15% site area	20% site area																	

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>throughout the site with a width of no less than 1.5m;</p> <p>Excludes all paved and other impervious areas such as driveways, patios, terraces, car parking spaces and bin and other utility areas.</p> <p>See Figure 5.</p>	

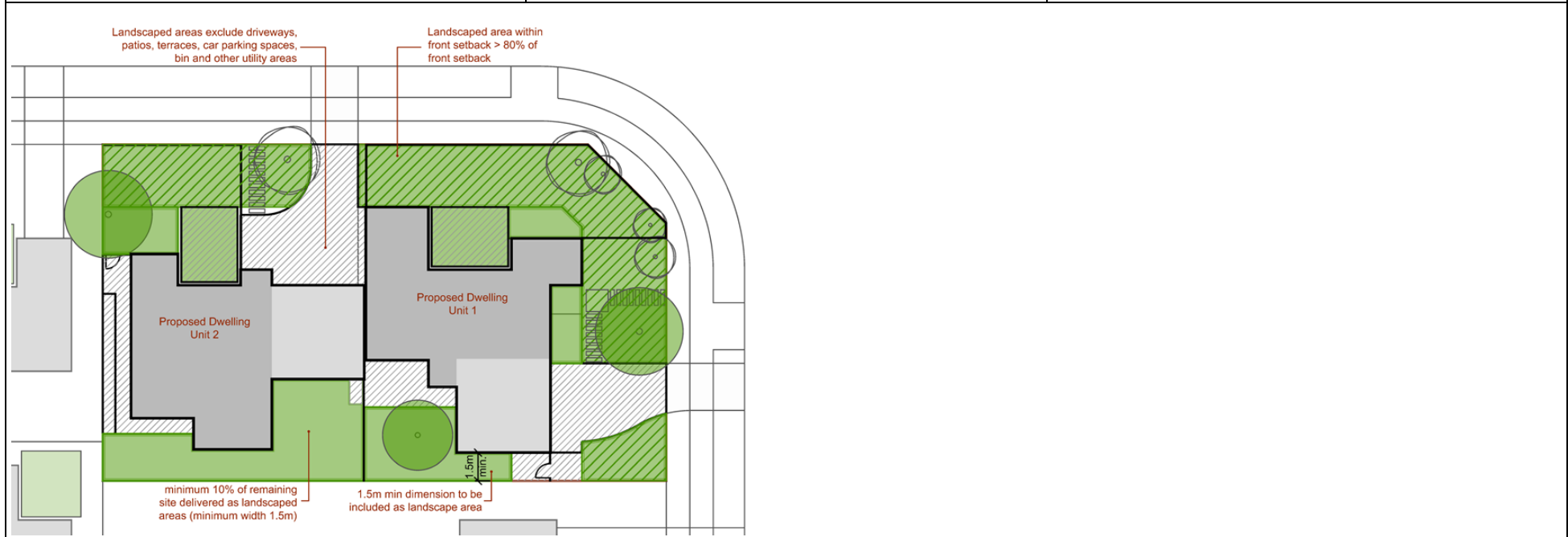


Figure 5

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary										
<p>PO<sub>6</sub> The front building setback is consistent with the prevailing front setbacks of other residential buildings in the street.</p> <p>Note: The prevailing setback of the street is determined by the setbacks of the residential buildings on any lot along the road within 100m of the site.</p>	<p>AO<sub>6.1</sub> Buildings are setback from the road frontage:</p> <p>(a) within 10% of the average front setback of existing residential buildings within 100m of the site; or</p> <p>(b) where there is no adjoining buildings the front setback is provided in accordance with Table 4.</p> <table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr> <th colspan="2" style="text-align: center;">Table 4</th> </tr> <tr> <th style="text-align: left;">Building element</th> <th style="text-align: left;">Minimum setback</th> </tr> </thead> <tbody> <tr> <td>Solid building wall (other than a garage)</td> <td style="text-align: center;">4.0m</td> </tr> <tr> <td>Solid building wall (where for a garage)</td> <td style="text-align: center;">5.5m</td> </tr> <tr> <td>Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)</td> <td style="text-align: center;">3.5m (or 3m where secondary road frontage)</td> </tr> </tbody> </table> <p>Note: See Figure 6.</p>	Table 4		Building element	Minimum setback	Solid building wall (other than a garage)	4.0m	Solid building wall (where for a garage)	5.5m	Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)	3.5m (or 3m where secondary road frontage)	<p><b>Complies.</b></p> <p>Refer to response at AO<sub>9.1</sub> of the Low Density Residential Zone Code.</p>
Table 4												
Building element	Minimum setback											
Solid building wall (other than a garage)	4.0m											
Solid building wall (where for a garage)	5.5m											
Permeable or non-enclosed elements (e.g. porch, verandah, balcony or carport)	3.5m (or 3m where secondary road frontage)											

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

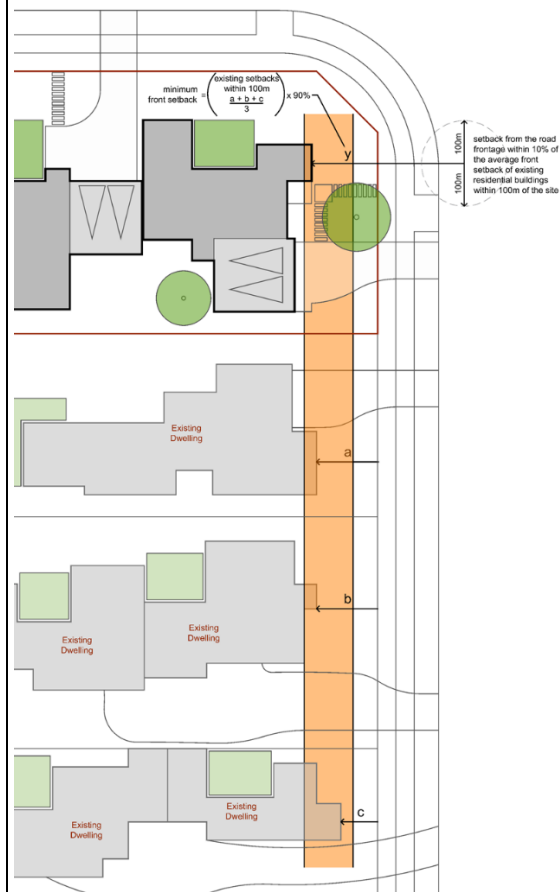


Figure 6

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO7 Side and rear building setbacks:</p> <ul style="list-style-type: none"> <li>(a) are appropriate to the scale of the development and the intended character of the zone and/or precinct or local plan in which the site is located;</li> <li>(b) provide for adequate daylight for habitable rooms and open space areas for buildings on and adjoining the site; and</li> <li>(c) are sufficient to minimise overshadowing and overlooking of adjoining premises.</li> </ul>	<p>In partial fulfilment of the performance outcome</p> <p>AO7.1 Any part of a building built to a side boundary:</p> <ul style="list-style-type: none"> <li>(a) must not exceed a maximum height of 3.5m</li> <li>(b) must not exceed a maximum length of 9m;</li> <li>(c) must be a minimum distance of 2.4m from any other wall built to the side boundary (whether on the site or an adjoining site) or be joined to another built to boundary wall if on an adjoining site; and</li> <li>(d) must not exceed a maximum total length of 50% of the side boundary.</li> </ul> <p>OR</p> <p>AO 7.2 Buildings are set back from side boundaries in accordance with the following:</p> <ul style="list-style-type: none"> <li>(a) For lots with frontages less than 20m: <ul style="list-style-type: none"> <li>(i) 1.2m for any part of the building up to 3.5m high; and</li> <li>(ii) 2m for any part of the building above 3.5m high.</li> </ul> </li> <li>(b) For lots with frontages greater than 20m: <ul style="list-style-type: none"> <li>(i) 1.8m for any part of the building up to 3.5m high; and</li> <li>(ii) 2.4m for any part of the building above 3.5m high.</li> </ul> </li> </ul> <p>AO7.3 Buildings are set back from the rear boundary a minimum of 2m.</p>	<p><b>Not Applicable.</b> The proposed development does not involve buildings which are built to boundary.</p> <p><b>Performance Solution.</b> Refer to response at PO10 of the Low Density Residential Zone.</p> <p><b>Performance Solution.</b> Refer to response at PO10 of the Low Density Residential Zone.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO7.4 Development ensures that solar access to habitable rooms and private open space of adjoining premises: -</p> <ul style="list-style-type: none"> <li>(a) achieves a minimum of 3 hours of sunlight between 9am and 3pm on June 21; or</li> <li>(b) is not further reduced where existing solar access is less than that specified in (a).</li> </ul> <p>Note: See figure 7.</p>	<p><b>Complies.</b></p> <p>The proposed development has been designed to ensure appropriate solar access is provided to habitable rooms and private open space areas.</p>

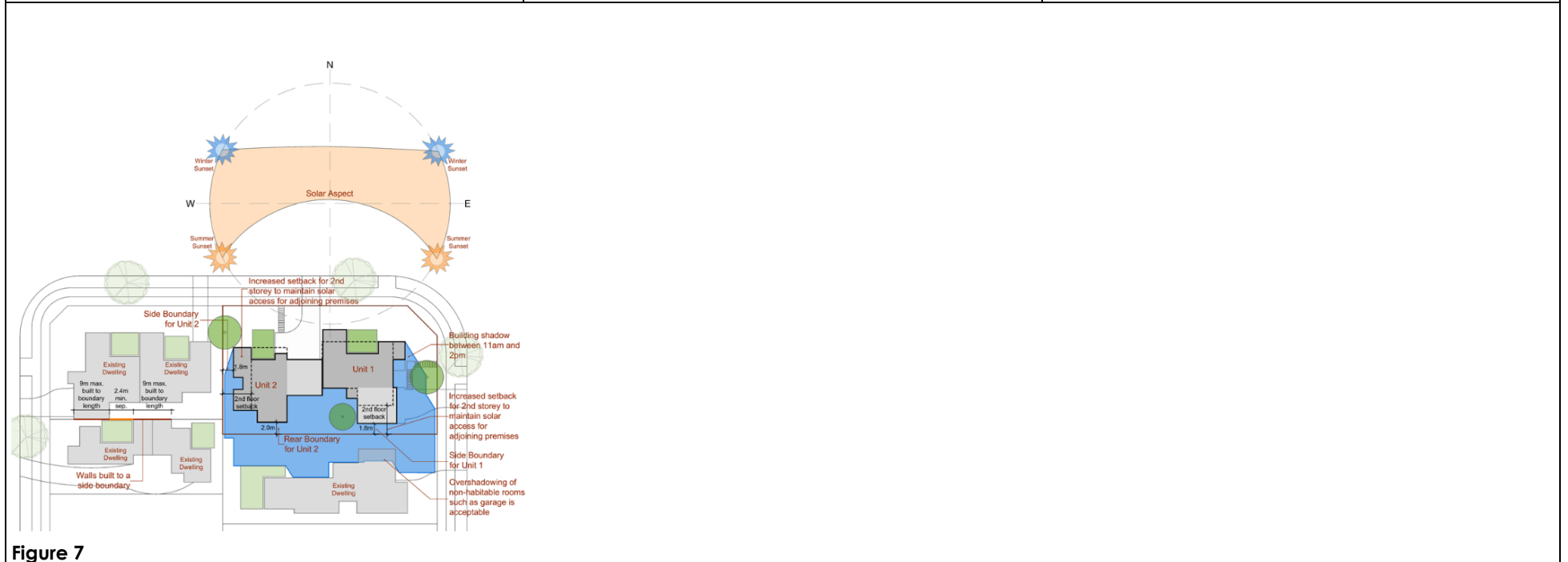


Figure 7

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

**Performance outcomes**

PO<sub>8</sub> Development provides adequate and suitable communal and private open space to meet the needs of occupants for privacy, relaxation and entertainment and accommodate visitors.

**Acceptable outcomes**

AO<sub>8.1</sub> Development provides communal and private open space in accordance with Table 5.

Table 5			
No. of Dwellings	Min. Communal Open Space per Dwelling	Min. Ground Floor Private Open Space per Dwelling	Min. Private Open Space above Ground Floor per Dwelling
1-10	0m <sup>2</sup>	16m <sup>2</sup>	8m <sup>2</sup>
11-15	20m <sup>2</sup>	16m <sup>2</sup>	8m <sup>2</sup>
16+	25m <sup>2</sup>	16m <sup>2</sup>	8m <sup>2</sup>

AO<sub>8.2</sub> Communal and private open space has a minimum dimension in accordance with Table 6.

Table 6		
Type	Area	Minimum Dimension
Private Open Space		
Ground Floor	16m <sup>2</sup>	4m*
First Floor	8m <sup>2</sup>	2.7m*
Communal Open Space		
All	<100m <sup>2</sup>	5m
	100m <sup>2</sup> +	Length to breadth ratio of 2.5:1

\*Min length required for both length and width.

**Compliance summary**

**Complies.**

The proposed development includes the provision of a minimum of 16m<sup>2</sup> of private open space for each dwelling unit.

**Performance Solution.**

All dwelling units (existing and proposed) are provided with a minimum of 16m<sup>2</sup> of private open space, however, are unable to achieve the minimum dimension of 4m. Notwithstanding this, the private open space area for all existing and proposed dwelling units is considered sufficient to cater for the needs of each residence.

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>8.3</sub> Private open space is:</p> <ul style="list-style-type: none"> <li>(a) is oriented to receive at least three hours of sun between 9am and 3pm on 21 June.</li> <li>(b) directly accessible from a living area or kitchen;</li> <li>(c) fenced or landscaped to achieve privacy for occupants;</li> <li>(d) clear of bins, clothes lines, hot water systems, air conditioning units, above ground water tanks and associated infrastructure and other forms of ancillary equipment; and</li> <li>(e) not located within the front setback area.</li> </ul> <p>Note: See Figure 8.</p>	<p><b>Complies.</b></p> <p>Private open space has been designed and orientated to ensure it receives adequate sunlight and is directly accessible from living areas. The areas will be fenced to achieve privacy and will not be impeded by bin storage or other plant or equipment.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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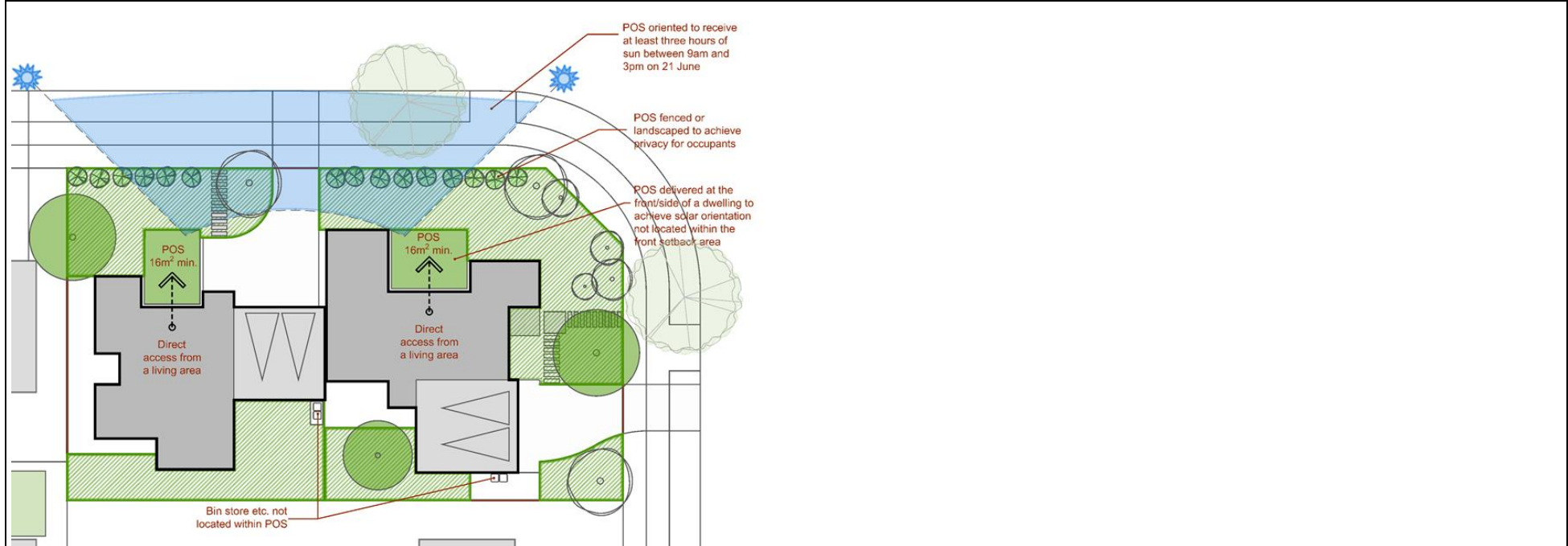


Figure 8

<p>PO<sub>9</sub> All car parking is designed to accommodate a standard car (B85 vehicle for occupant car parking and B99 vehicle for visitor car parking) and:</p> <p>(a) is clearly identifiable and of adequate dimensions;</p>	<p>AO<sub>9.1</sub> Design of the site layout must ensure a B85 vehicle (as defined in AS 2890.1:2004 Parking facilities - Off-street car parking) is able to enter an enclosed garage or visitor car park in a single forward manoeuvre and exit in a single reverse manoeuvre prior to departing the site in a forward direction.</p>	<p><b>Performance Solution.</b> The proposed development has been designed to ensure that vehicles can access the proposed new garage and enter and exit in a forward gear. Reference is made to the Vehicle Manoeuvring Plans attached at <b>Appendix E.</b></p>
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

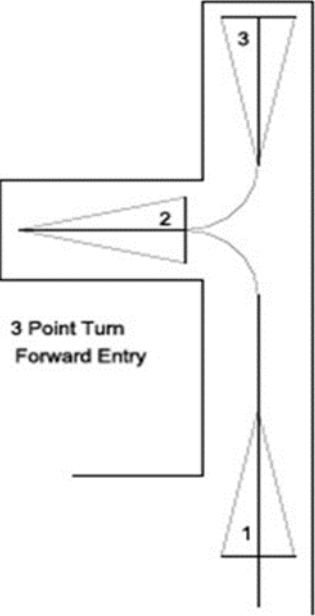
Performance outcomes	Acceptable outcomes	Compliance summary
<p>(b) incorporates adequate manoeuvring area to parking spaces (including carports and garages); and</p> <p>(c) incorporates a clear, safe and effective circulation system.</p> 	<p>AO9.2 Design of the site layout must ensure a B99 vehicle (as defined in AS 2890.1:2004 Parking facilities - Off-street car parking) is able to enter a visitor car park in a single forward manoeuvre and exit in a single reverse manoeuvre prior to departing the site in a forward direction.</p> <p>Note: See Figure 9.</p> <p>AO9.3 Each parking space has minimum dimensions as per Table 7:</p>	<p><b>Not Applicable.</b> The proposed development does not involve or require a visitor parking space.</p> <p><b>Complies.</b> All parking spaces provided on the site comply with the minimum dimensions outlined in Table 7.</p>

Figure 9

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary																
	<table border="1"> <thead> <tr> <th colspan="2">Table 7</th> </tr> <tr> <th>Parking Space</th> <th>Minimum Dimensions</th> </tr> </thead> <tbody> <tr> <td>Single uncovered parking space (with at least 400mm clearance between either side of the parking space and the nearest wall.)</td> <td>5.4 metres long by 2.6 metres wide</td> </tr> <tr> <td>Single uncovered parking space (with less than 400mm clearance between either side of the parking space and the nearest wall.)</td> <td>5.4 metres long by 3 metres wide</td> </tr> <tr> <td>Single carport</td> <td>5.4 metres long by 3 metres wide</td> </tr> <tr> <td>Double carport</td> <td>5.4 metres long by 5.8 metres wide</td> </tr> <tr> <td>Single garage</td> <td>6.4 metres long by 3 metres wide internally</td> </tr> <tr> <td>Double garage</td> <td>6.4 metres by 6.4 metres wide internally</td> </tr> </tbody> </table>	Table 7		Parking Space	Minimum Dimensions	Single uncovered parking space (with at least 400mm clearance between either side of the parking space and the nearest wall.)	5.4 metres long by 2.6 metres wide	Single uncovered parking space (with less than 400mm clearance between either side of the parking space and the nearest wall.)	5.4 metres long by 3 metres wide	Single carport	5.4 metres long by 3 metres wide	Double carport	5.4 metres long by 5.8 metres wide	Single garage	6.4 metres long by 3 metres wide internally	Double garage	6.4 metres by 6.4 metres wide internally	
Table 7																		
Parking Space	Minimum Dimensions																	
Single uncovered parking space (with at least 400mm clearance between either side of the parking space and the nearest wall.)	5.4 metres long by 2.6 metres wide																	
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Double carport	5.4 metres long by 5.8 metres wide																	
Single garage	6.4 metres long by 3 metres wide internally																	
Double garage	6.4 metres by 6.4 metres wide internally																	

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>10</sub> Development provides resident and visitor car parking which is integrated into the site and building design and:</p> <ul style="list-style-type: none"> <li>(a) Does not negatively impact the amenity of the site, adjoining sites or streetscape;</li> <li>(b) Is not a prominent visual feature from the street or other public spaces; and</li> <li>(c) Contributes to the establishment of a garden setting by avoiding large areas of visible hardstand.</li> </ul>	<p>AO<sub>10.1</sub> Parking is only provided within the front setback area where it is:</p> <ul style="list-style-type: none"> <li>(a) uncovered and located on a driveway directly in front of a carport or garage; or</li> <li>(b) the front setback is a least 6m, and</li> <li>(c) the car park surface is permeable;</li> <li>(d) a minimum 2m wide densely planted landscape buffer is provided between the car park and the front boundary for the full length of the car park;</li> <li>(e) the location of the car park does not compromise the ability of the dwelling to provide casual surveillance of the public realm; and</li> <li>(f) at least 60% of the front setback area is landscaped area.</li> </ul> <p>AO<sub>10.2</sub> Where car parking is provided below ground:</p> <ul style="list-style-type: none"> <li>(a) Access ramps are not located within 6m of the street boundary and do not exceed an opening width of 6m; and</li> <li>(b) Entry to the site for pedestrians is provided separate from vehicle entries by way of a safe, well lit and clearly identifiable pathway or gatehouse;</li> </ul> <p>AO<sub>10.3</sub> All car parking:</p> <ul style="list-style-type: none"> <li>(a) is clearly signposted; and</li> <li>(b) is freely available for use by to the applicable occupants and visitors of the development.</li> </ul> <p>Note: See Figures 10 and 11.</p>	<p><b>Complies.</b> The proposed development will retain the one (1) existing car parking space within the front setback.</p> <p><b>Not Applicable.</b> No Parking is provided below ground.</p> <p><b>Complies.</b> The proposed development involves car parking spaces within the single-garage of each unit. These parking spaces are freely available for use by occupants of the development at all times. The development does not propose and is not required to provide on-site visitor carparking, and accordingly signage and the availability of parking for guests is not considered to be required in this instance.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

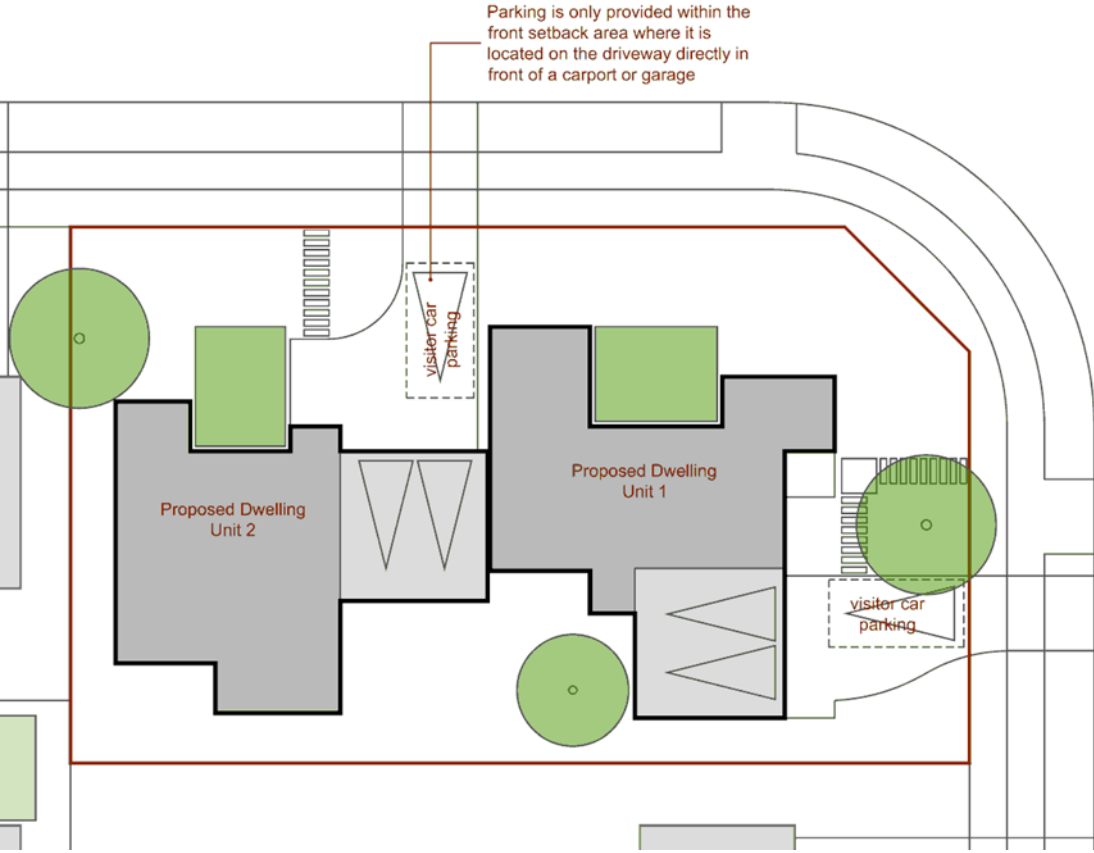
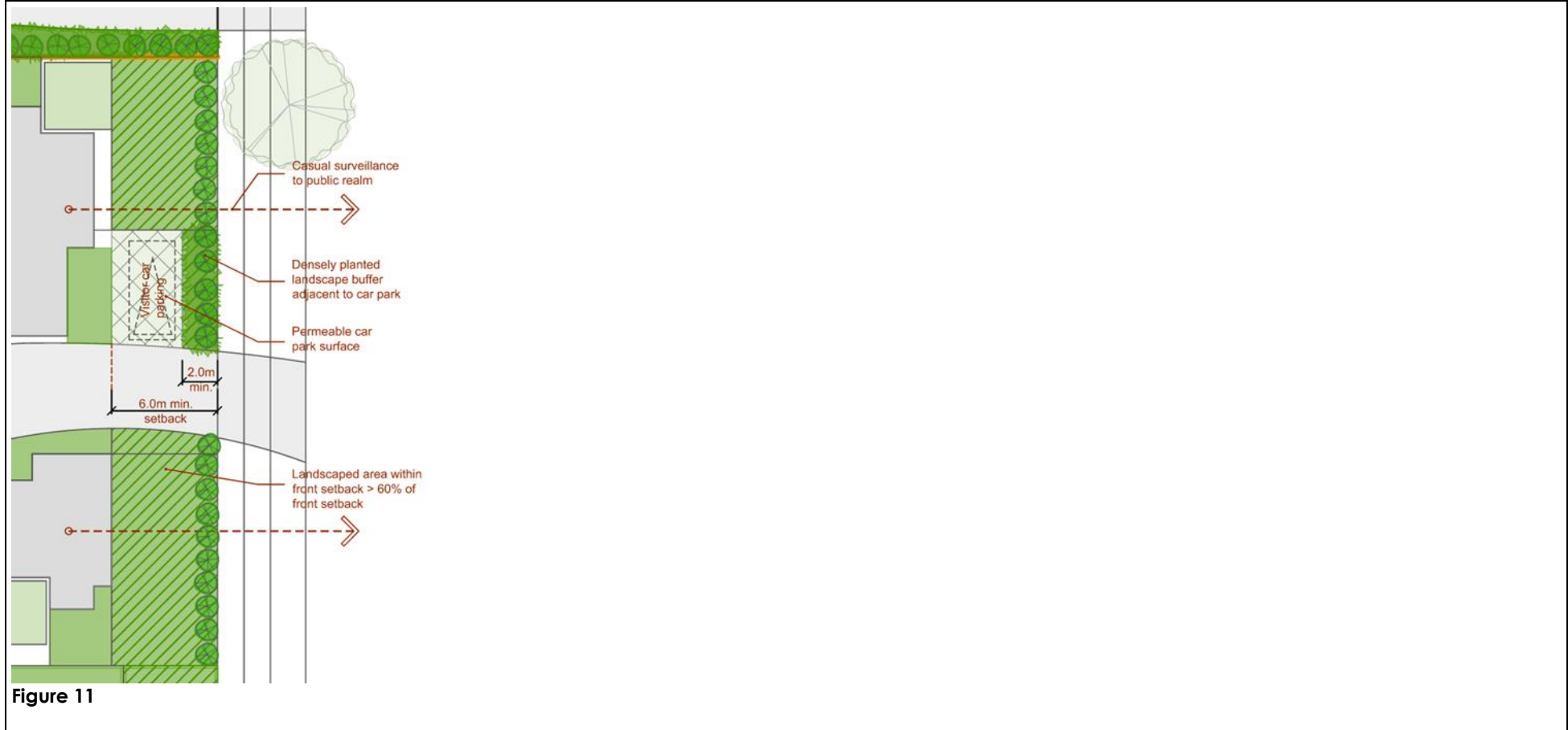
Performance outcomes	Acceptable outcomes	Compliance summary
<p>Parking is only provided within the front setback area where it is located on the driveway directly in front of a carport or garage</p>  <p>Proposed Dwelling Unit 2</p> <p>Proposed Dwelling Unit 1</p> <p>visitor car parking</p> <p>visitor car parking</p>		

Figure 10

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>11</sub> Safe and equitable access for both pedestrians and vehicles is provided, to and from buildings and the street whilst minimising the extent and number of driveways.</p>	<p>In partial fulfilment of the performance outcome:            AO<sub>11.1</sub> Vehicular access from the street is provided via a single shared driveway with a maximum crossover width of 5m.            AO<sub>11.2</sub> Where the site abuts a lane vehicle access is provided from the lane.            Note: See Figure 12.</p>	<p><b>Complies.</b>            The proposed development will utilise the existing shared driveway crossover. The development does not propose any changes to the existing crossover.   <b>Not Applicable.</b>            The site does not abut a laneway.</p>

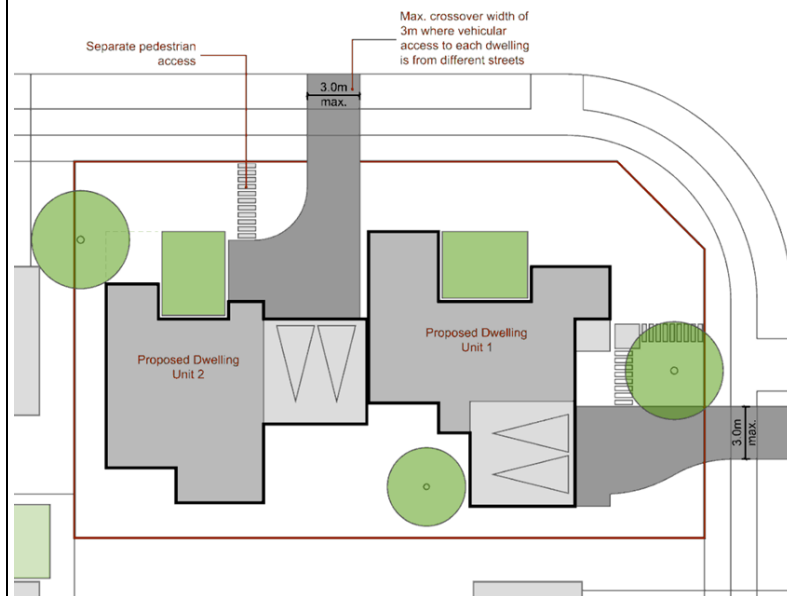


Figure 12

<p>PO<sub>12</sub> The privacy of residents of the development or adjoining premises is protected when they are</p>	<p>AO<sub>12.1</sub> Habitable room windows of a dwelling are separated a minimum of 9m from a habitable</p>	<p><b>Not Applicable.</b>            See response to AO<sub>12.2</sub>.</p>
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**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>inside a dwelling or in a private open space area.</p>	<p>room window or private open space of another dwelling on the same or an adjoining site.</p> <p>OR</p> <p>AO<sub>12.2</sub> Where there is a direct view within 9m into a habitable room or private open space of another dwelling on the same or an adjoining site:</p> <ul style="list-style-type: none"> <li>(a) The elements with the view such as windows, balconies, or terraces of a dwelling must be screened in accordance with AO<sub>12.3</sub>; or</li> <li>(b) The Windows have translucent glazing up to or sill heights of at least, 1.5m from the internal floor level; or</li> <li>(c) A solid fence or masonry wall with a minimum height of 1.5m is positioned between the elements with the view.</li> </ul> <p>AND</p> <p>AO<sub>12.3</sub> Where screening is used, it:</p> <ul style="list-style-type: none"> <li>(a) is a solid translucent screen or a louvre perforated panel, trellis or the like that has a maximum of 50% open to solid ratio; and</li> <li>(b) is a fixed, permanent element of a design that complements the style, detailing and materials of the development.</li> </ul> <p>Note: See Figure 13.</p>	<p><b>Complies.</b></p> <p>The proposed new dwelling will be located approximately 7m from the nearest dwelling on the adjoining premises. The adjoining premises to the north is approximately 2m lower than the site. This difference in height, in conjunction with existing fencing ensures that there are no direct views into habitable rooms of the adjoining dwelling.</p> <p><b>Complies.</b></p> <p>As detailed in response to AO<sub>12.2</sub>, the development will maintain the existing fencing along the northern boundary which screens views between the proposed and existing dwellings.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes

Acceptable outcomes

Compliance summary

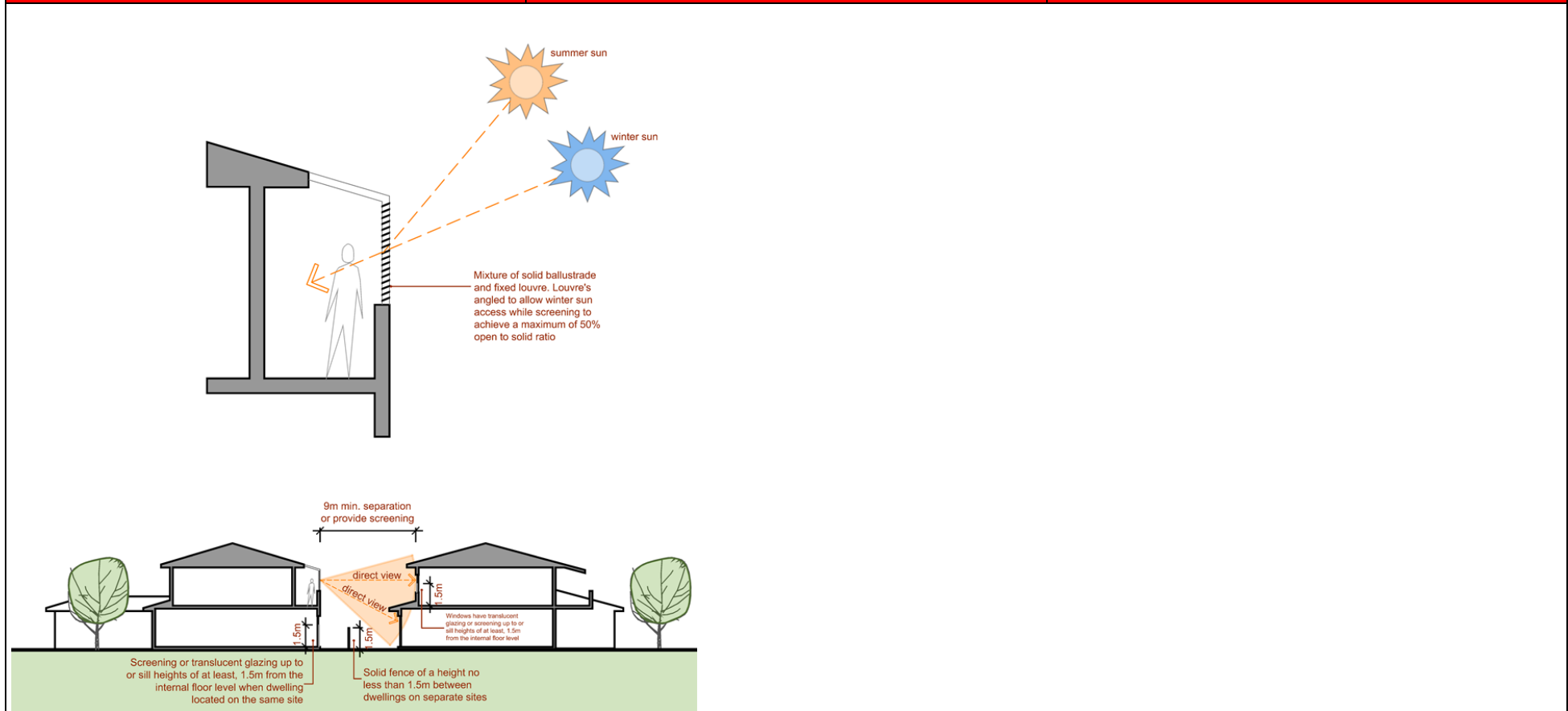


Figure 13

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>13</sub> The development is designed so that occupants are not adversely affected by adjoining non-residential uses.	In partial fulfilment of the performance outcome AO <sub>13.1</sub> A 1.8m high solid screen fence and 2m wide vegetated buffer is provided along any section of the site boundary that is common with vehicle movement and/or parking areas associated with a non-residential use on an adjoining site.	<b>Not Applicable.</b> The proposed development does not adjoin a non-residential use.
PO <sub>14</sub> Noise from the development does not unreasonably impact on existing or likely future dwellings on nearby land.	AO <sub>14.1</sub> For developments of six (6) dwellings or more, vehicle movement areas are located a minimum of 3m from any adjoining dwellings.  AO <sub>14.2</sub> Swimming pools, BBQs, dining areas and other recreational facilities located within communal open areas are located a minimum of 3m from adjoining dwellings.  OR AO <sub>14.3</sub> Provide acoustic screening in accordance with an acoustic engineer's design where vehicle movement or recreational areas are located closer than 3m to any adjoining dwellings.  Note: A minimum 1.5m vegetation buffer will be required with any acoustic fence to provide amenity. (See Figures 14 and 15.)	<b>Not Applicable.</b> The proposed development only involves three (3) dwellings.

Toowoomba Regional Planning Scheme 2012 (v28.0)

Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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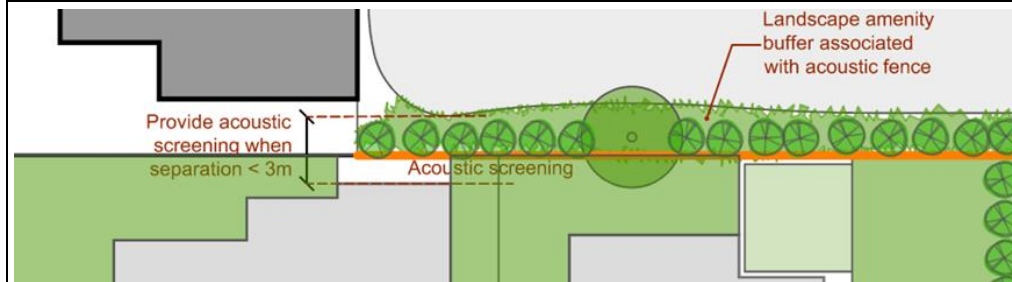


Figure 14

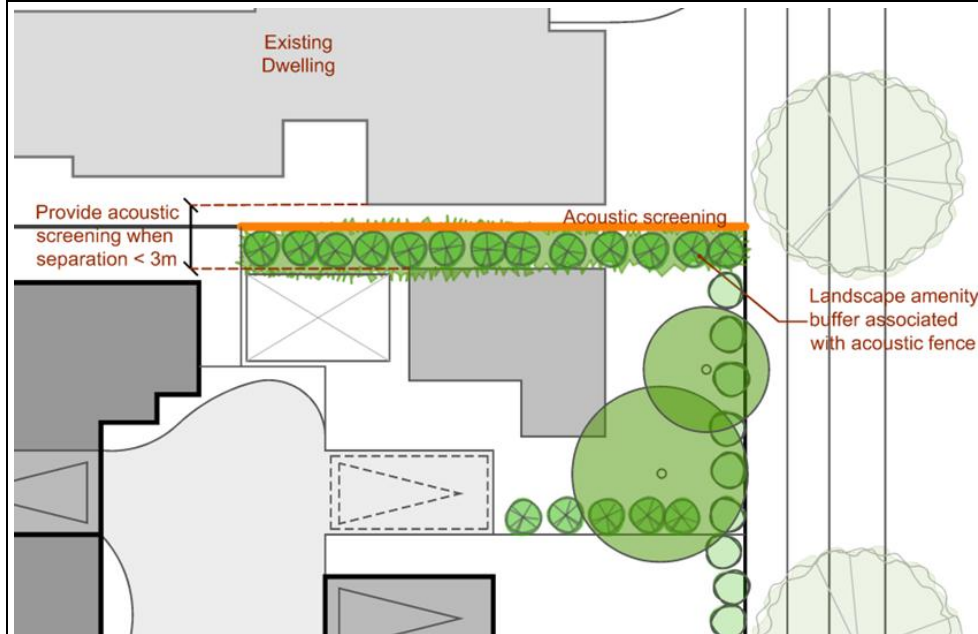


Figure 15

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO15 Where communal open space is provided, it:</p> <ul style="list-style-type: none"> <li>(a) is positioned on site to enjoy a good solar orientation;</li> <li>(b) includes landscaping and screening for privacy to provide amenity;</li> <li>(c) is easily accessible and conveniently located for the use of all occupants;</li> <li>(d) is useable and safe;</li> <li>(e) is designed and located to minimise internal and external impacts on the amenity of residents and neighbouring premises; and</li> <li>(f) contributes positively to the streetscape where located in a frontage area.</li> </ul>	<p>AO15.1 Communal open space is oriented to receive at least three hours of sun between 9am and 5pm on 21 June.</p> <p>AO15.2 Communal open space provided at the ground level, includes a balance of soft and hard landscape features with:</p> <ul style="list-style-type: none"> <li>(a) a minimum 50% open to the sky, and</li> <li>(b) a minimum 25% shaded by trees within 5 years.</li> </ul> <p>AO 15.3 The location and design of communal open space ensures that it is:</p> <ul style="list-style-type: none"> <li>(a) able to be accessed by all residents from a common area within the site;</li> <li>(b) subject to casual surveillance; and</li> <li>(c) separated from private areas of the site.</li> </ul> <p>AO15.4 Communal open spaces may include indoor recreation facilities.</p> <p>Note: Communal open spaces do not contain:</p> <ul style="list-style-type: none"> <li>(a) vehicle driveways, parking, manoeuvring or other associated hardstand areas; or</li> <li>(b) structures such as rainwater tanks, bin storage areas, air conditioning plant, clothes hoists, etc. (See Figure 16.)</li> </ul>	<p><b>Not Applicable.</b></p> <p>The proposed development does not involve any communal open space.</p>

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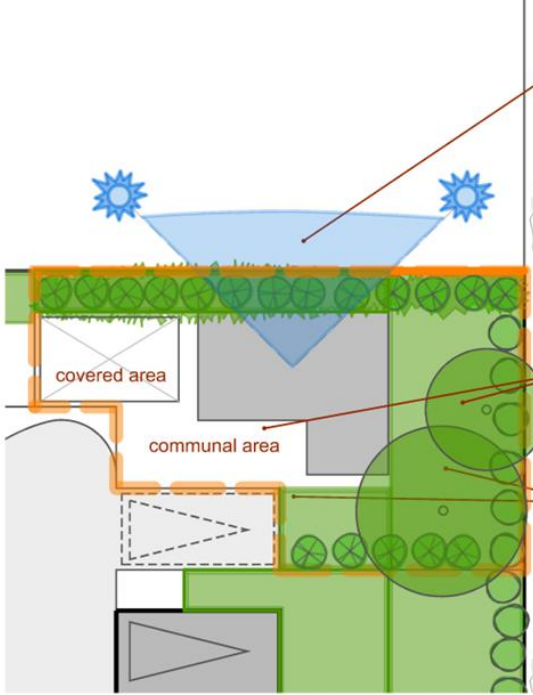
Performance outcomes	Acceptable outcomes	Compliance summary
 <p>The diagram shows a site plan with a 'covered area' on the left and a 'communal area' on the right. The communal area contains various landscape features including trees and open space. A blue shaded area at the top of the communal area is indicated by a line pointing to a text box.</p>	<p>Communal open space oriented to receive at least 3 hours of sun between 9am and 5pm on 21 June</p> <p>minimum 25% shade trees and 50% open to sky. Balance area of 25% may include covered recreation areas.</p> <p>Communal open space includes a balance of soft and hard landscape features.</p>	

Figure 16

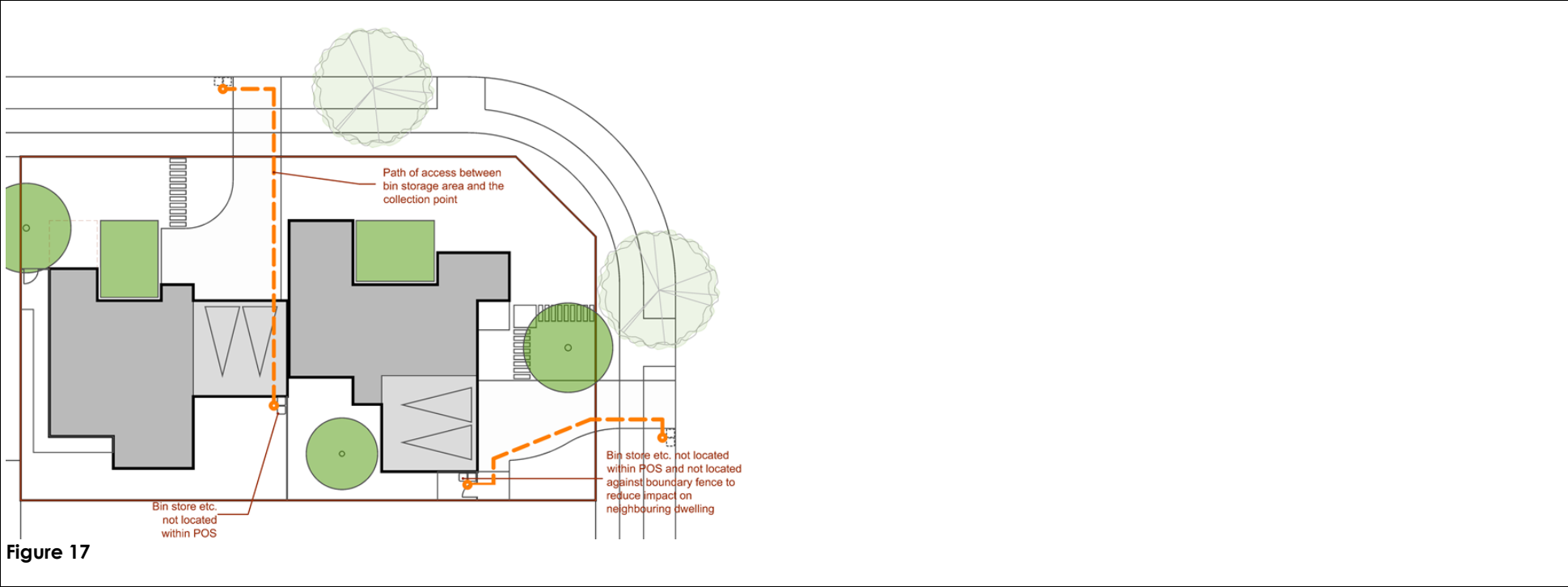
**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>16</sub> Bin storage and access to the collection point is provided in a manner which is convenient, hygienic and will not result in an amenity impact on any use onsite or adjoining the premises.</p>	<p>AO<sub>16.1</sub> Bin storage is:</p> <ul style="list-style-type: none"> <li>(a) not located within the minimum private open space area required for the development;</li> <li>(b) not located within 2m of any fresh air intake of any dwelling on the premises or adjoining premises; and</li> <li>(c) if in an area which is accessible by more than one dwelling, is clearly identified as the storage area for the relevant unit/dwelling.</li> </ul> <p>AO<sub>16.2</sub> A path of access is provided between any bin storage area and the collection point which:</p> <ul style="list-style-type: none"> <li>(a) is a minimum of 1.2m wide;</li> <li>(b) is clear of any obstruction (i.e. retaining wall, landscaping, hot water tank);</li> <li>(c) does not traverse through any habitable room (other than a garage);</li> <li>(d) provides an at grade path of access (i.e. no steps); and</li> <li>(e) is no longer than 30m.</li> </ul> <p>Note: See Figure 17.</p>	<p><b>Complies.</b></p> <p>Each dwelling will benefit from its own refuse container storage area which will be located outside of private open space areas and away from fresh air intakes of adjoining units.</p> <p>It is envisaged that residents will utilise the driveway as a means of transporting refuse containers to kerbside as per existing arrangements.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
 <p>The diagram illustrates a residential site plan with several key features:         <ul style="list-style-type: none"> <li><b>Bin storage area:</b> A green rectangular area located within the property boundary but outside the Primary Object Space (POS).</li> <li><b>Path of access:</b> A dashed orange line showing the route from the bin storage area to a collection point on the street.</li> <li><b>Driveway:</b> A long, unvaried driveway running along the side boundary of the property.</li> <li><b>Setbacks:</b> The driveway is set back from the side boundary by 1.5m for the first 6m and 750mm thereafter.</li> <li><b>Bin storage location:</b> The bin store is not located against the boundary fence to reduce impact on neighboring dwellings.</li> </ul> </p> <p><b>Figure 17</b></p>		
<p>PO<sub>17</sub> Long, unvaried driveways to property boundaries are avoided and do not detract from the local streetscape.</p>	<p>AO<sub>17.1</sub> Driveways are setback from side boundaries a minimum of 1.5m for the first 6m and 750mm thereafter incorporating a continuous densely</p>	<p><b>Performance Solution.</b> The proposed development involves the reuse of the existing driveway which is generally unvaried (noting that the existing of the driveway is to the rear of the site). Accordingly, the proposed development will not further detract from the local streetscape noting it is already impacted by existing improvements.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>planted landscape strip between the edge of the driveway and side boundary.</p> <p>AO<sub>17.2</sub> Driveways that exceed 40 metres in length must incorporate a change of alignment of a minimum of 1m.</p> <p>Note: See Figure 18.</p>	

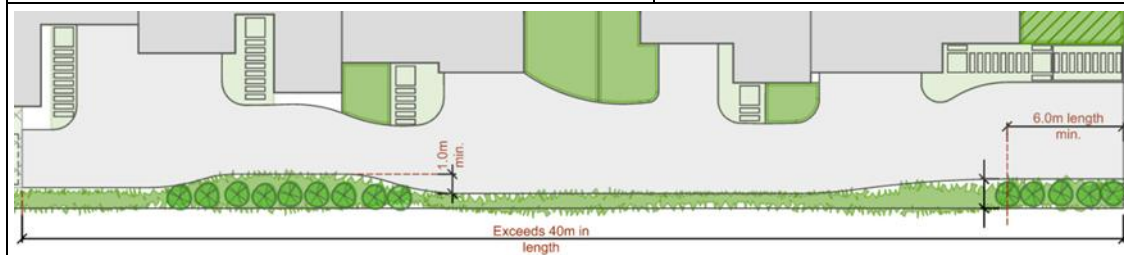


Figure 18

<p>PO<sub>18</sub> Buildings are designed to a high aesthetic standard providing:</p> <ul style="list-style-type: none"> <li>(a) prominent features towards the street frontage;</li> <li>(b) detailing and finishes appropriate to the scale, quality and character of the street and to help differentiate between dwellings;</li> <li>(c) opportunities for informal surveillance of the street and other public spaces;</li> <li>(d) clear and visible entries; and</li> <li>(e) elements that complement or enhance the character of the local neighbourhood.</li> </ul>	<p>In partial fulfilment of the performance outcome:</p> <p>AO<sub>18.1</sub> Buildings:</p> <ul style="list-style-type: none"> <li>(a) where at the front of the site, are oriented to and address the street;</li> <li>(b) have front entries that provide full weather protection and where within 10m of any street, have clearly defined front entries that are visible from the street;</li> <li>(c) where within 10m of any street; have one or more habitable rooms facing the street boundary that have windows or doors with a minimum area of clear glazing of 0.25m<sup>2</sup></li> </ul>	<p><b>Complies.</b></p> <p>The proposed development is located behind existing buildings on the site. Each unit will be provided with full weather protection with visible elements utilising variation to deliver depth at the street level.</p>
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Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>for each linear metre of street frontage; and                      (d) are designed to deliver depth in the façade facing the street or public space with a variation not less than 2m deep for a minimum 50% of the building width at ground level.</p> <p>Note: See Figure 19.</p>	

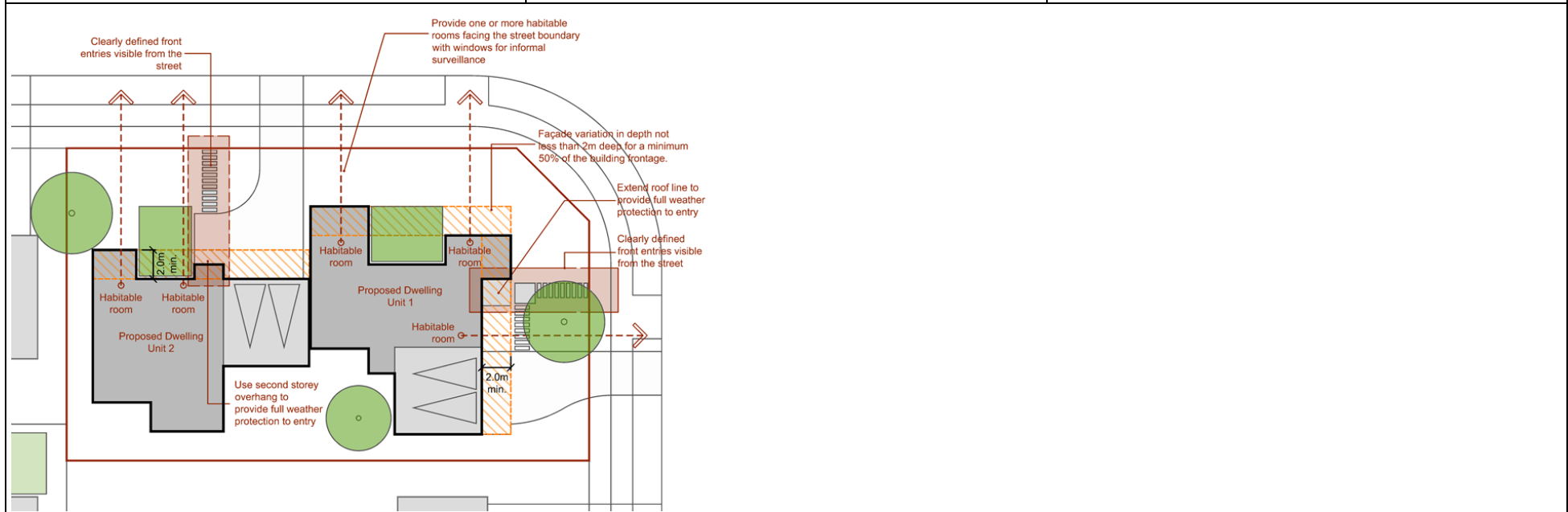


Figure 19

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>PO<sub>19</sub> Development incorporates typical domestic detailing to achieve a residential scale living environment with aesthetic standards and amenity for occupants similar to those of single dwellings in the neighbourhood.</p>	<p>In partial fulfilment of the performance outcome</p> <p>AO<sub>19.1</sub> The maximum single wall length of a building is 32m with offsets of 600mm minimum provided every 7.5m;</p> <p>Note: Building walls separated by a distance of less than 10% of their combined length are considered as one wall. (See Figure 20.)</p> <p>AO<sub>19.2</sub> Building bulk is minimised through articulation and other measures, including the provision of at least one (1) element from at least three (3) of the following element groups:</p> <ul style="list-style-type: none"> <li>(a) verandahs, porches, pergolas or balconies;</li> <li>(b) roof overhangs;</li> <li>(c) window hoods/screens;</li> <li>(d) awning and shade structures; or</li> <li>(e) 600mm vertical gutter offset to create roof articulation.</li> </ul> <p>AO<sub>19.3</sub> Townhouses or attached dwellings have:</p> <ul style="list-style-type: none"> <li>(a) no more than three (3) dwellings contained within a single building;</li> <li>(b) a minimum of 4.5m separation between buildings.</li> </ul>	<p><b>Complies.</b> The proposed development does not involve a single wall length exceeding 32m in length.</p> <p><b>Complies.</b> The proposed development has been designed to incorporate building articulation and variations in building materials to reduce building bulk.</p> <p><b>Complies.</b> The proposed development involves three (3) dwelling units.</p>

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Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
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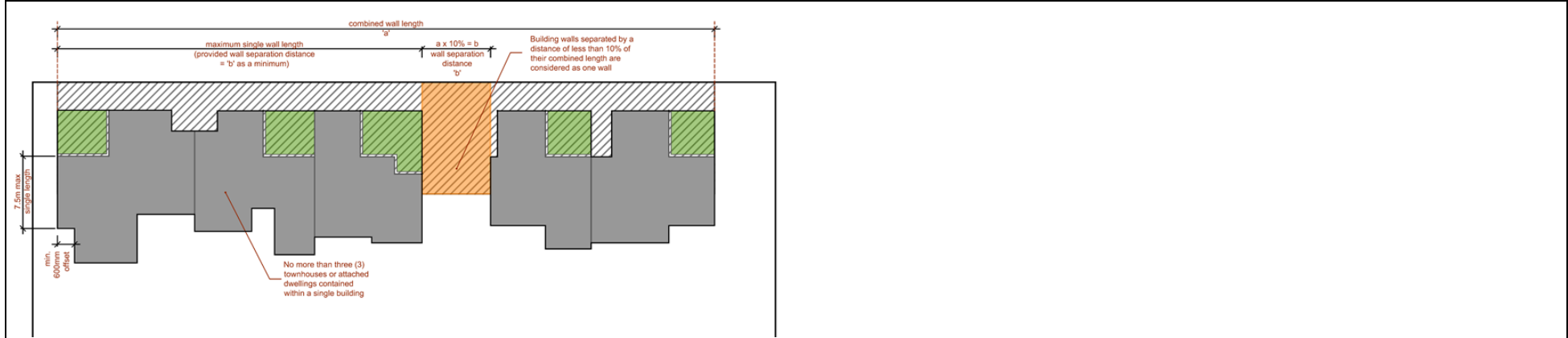


Figure 20

<p>PO<sub>20</sub> Facades include a variety of colours, finishes and textures based on those found in houses within the surrounding streetscape and appropriate to the desired character of the area.</p>	<p>In partial fulfilment of the performance outcome: AO<sub>20.1</sub> Building facades incorporate:</p> <ul style="list-style-type: none"> <li>(a) external elements and treatments that are of a high quality finish, durable and need minimal maintenance;</li> <li>(b) at least three (3) distinct textures to finishes or surfaces not including roof, rainwater goods, glazing, joinery, doors, garage doors or soffits; and</li> <li>(c) colours and textures that are similar with the colours and textures of adjoining buildings.</li> </ul>	<p><b>Complies.</b> The proposed building facades incorporate:</p> <ul style="list-style-type: none"> <li>• high quality and contemporary materials which are durable and need minimal maintenance;</li> <li>• colours and textures which are consistent with residential development with the surrounding locality.</li> </ul>
<p>PO<sub>21</sub> Development provides landscaping that contributes to the landscape character, shade and the micro-climate of the neighbourhood and site through the retention of existing significant trees on the site.</p>	<p>In partial fulfilment of the performance outcome: AO<sub>21.1</sub> Landscape design plans for the subject site are prepared by a Registered Landscape Architect</p>	<p><b>Performance Solution.</b> The proposed development provides landscaping to contribute to enhancing streetscape character and improving micro-climate. Species selected will reflect those used in existing landscaping onsite. The scale of</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>and lodged with the application for development approval.</p> <p>AO<sub>21.2</sub> Existing mature trees:</p> <ul style="list-style-type: none"> <li>(a) are retained where assessed by a qualified arborist as healthy and sound and not a potential risk to people and property, and having a life expectancy greater than 10 years; and</li> <li>(b) are integrated into the site design and protected from disturbance.</li> </ul> <p>Note: An arborist report will be required to demonstrate assessment of mature trees and include advice for their protection from disturbance during construction.</p> <p>Note: Retained mature trees are eligible to contribute to minimum tree provision requirements within AO<sub>22.3</sub>.</p> <p>OR</p> <p>AO<sub>21.3</sub> New canopy trees capable of achieving a height of at least five (5) metres and canopy spread of at least six (6) metres at maturity are provided at the following rate:</p> <ul style="list-style-type: none"> <li>(a) one (1) per site with a frontage of less than 25 metres; and</li> <li>(b) two (2) per site with a frontage of 25m or more.</li> </ul> <p>AO<sub>21.4</sub> Plant species selection complements the local landscape character, is long lived, has non-invasive roots and suited to the site drainage, soil and climatic conditions.</p>	<p>the proposed landscaping will be consistent with that used in the surrounding locality.</p>

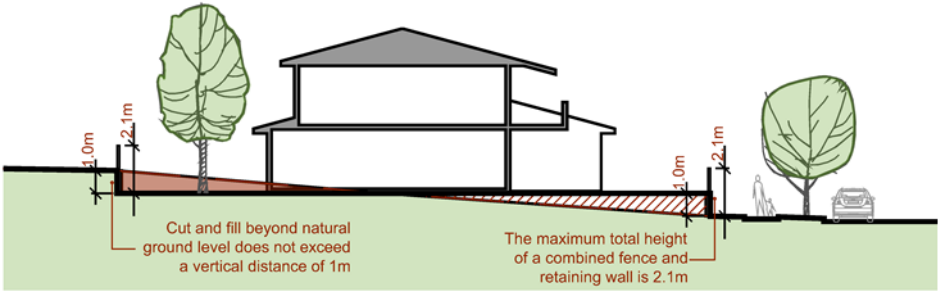
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**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	Note: New street trees should be provided in accordance with Toowoomba Regional Council Street Tree Master Plan	
<p>PO<sub>22</sub> The treatment of fencing and retaining walls in all situations reinforces the high quality, amenity and safety of the development.</p> <p>Note: Details demonstrating compliance with the performance outcome should be included in the landscape design drawings submitted with any development application.</p>	<p>AO<sub>22.1</sub> The maximum total height of a combined fence and retaining wall is 2.1m, as measured from the finished surface level.</p> <p>AO<sub>22.2</sub> Where a combined fence and retaining wall is located on the front boundary, the fence has a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>AO<sub>22.3</sub> Retaining walls and fences are constructed of high quality materials and contribute to residential amenity.</p> <p>AO<sub>22.4</sub> Any fence on the front boundary has a minimum 30% transparency when viewed from in front of the fence (at 90 degree angle to the fence).</p> <p>Note: Where retaining walls are set off the property boundary by a minimum of 600mm, they are not considered to be combined with the fencing. (See Figure 21.)</p>	<p><b>Complies.</b> Any new combined fence and retaining walls will be a maximum height of 2.1m.</p> <p><b>Not Applicable.</b> The proposed development does not involve any new fences or retaining walls along the front boundary.</p> <p><b>Complies.</b> Retaining walls and fences will be constructed of high quality materials to contribute to enhancing residential amenity in the locality.</p> <p><b>Not Applicable.</b> The proposed development does not involve any new fencing along the front boundary.</p>

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Medium Density Residential Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
		
<p><b>Figure 21</b></p> <p>PO<sub>23</sub> Front fencing contributes to privacy, screening, security, and street character while maintaining a visual relationship between dwellings and public spaces.</p> <p>Note: Details demonstrating compliance with the performance outcome should be included in the landscape design drawings submitted with any development application.</p>	<p>AO<sub>23.1</sub> Fences located on a front property boundary or a common boundary with an adjacent park or within the front setback area:</p> <ul style="list-style-type: none"> <li>(a) have a maximum height of 1.2 metres; or</li> <li>(b) have a maximum height of 1.5 metres and a minimum transparency of 30% when viewed from in front of the fence (at 90 degree angle to the fence).</li> </ul> <p>Note: In instances where the fence is located in conjunction with a retaining wall refer PO23.</p>	<p><b>Not Applicable.</b> See response to AO<sub>22.4</sub>.</p>
<p>PO<sub>24</sub> Stormwater discharge from the site is to a lawful point of discharge as defined in the Queensland Urban Drainage Manual (QUDM), without the use of pumped or charged pipe systems, and not to private land other than to an easement for stormwater purposes befitting the site and</p>	<p>AO<sub>24.1</sub> The site:</p> <ul style="list-style-type: none"> <li>(a) has a natural surface with an elevation that is higher than the abutting road and enables stormwater to drain gravitationally</li> </ul>	<p><b>Complies.</b> Stormwater generated by the proposed development will be discharged to a lawful point of discharge. Reference is made to the Conceptual Stormwater Report attached at <b>Appendix D</b>.</p>

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**Medium Density Residential Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>allowing discharge to a lawful point of discharge in land over which Council has tenure or control.</p> <p>Note: Land over which Council has tenure or control does not include Council's open space network.</p>	<p>to the abutting road via subsurface pipes; or</p> <p>(b) has a natural surface with an elevation that is higher than Council's stormwater drainage network in the abutting road and enables stormwater to drain gravitationally to the stormwater drainage network in the abutting road via subsurface pipes; or</p> <p>(c) has lawful access to an inter allotment drainage network with available capacity to meet the requirements of the development.</p>	
<p>PO<sub>25</sub> Where a Relocatable Home Park or Tourist Park the development site has an appropriate area and configuration to accommodate home or van sites, as well as adequate landscape and recreation areas, ancillary amenities and facilities, car parking and service areas.</p>	<p>No acceptable outcome is nominated.</p>	<p><b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.</p>

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**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Outdoor Lighting</b>		
<p>PO<sub>1</sub> Development does not unacceptably reduce the amenity and environmental quality of environs, especially of any nearby residential premises or public spaces as a result of light spill.</p>	<p>AO<sub>1.1</sub> No outdoor lighting is proposed as part of the development.</p> <p>OR</p> <p>AO<sub>1.2</sub> Technical parameters, design, installation, operation and maintenance of outdoor lighting comply with the requirements of Australian Standard 'AS4282-1997 control of the obtrusive effects of outdoor lighting'.</p> <p>AO<sub>1.3</sub> For sporting fields and sporting courts the technical parameters, design, installation, operation and maintenance comply with the requirements of Australian Standard AS4282-1997 – Control of the obtrusive Effects of Outdoor Lighting and a compliance statement by a lighting designer has been provided in accordance with the Australian Standard (Section 4).</p> <p>AO<sub>1.4</sub> Where light spillage outside of the property boundary is likely to result in levels above those mentioned in AO<sub>1.3</sub> the applicant has provided a lighting proposal and impact assessment (environmental and amenity) as part of the application which has demonstrated that the lighting will not create nuisance issues for surrounding sensitive receptors.</p> <p>AO<sub>1.5</sub> For private sporting courts the lighting system: (a) is baffled or shielded to ensure that a light source is not directly visible from a</p>	<p><b>Not Applicable.</b> Refer to responses at AO<sub>1.2</sub> – AO<sub>1.6</sub>.</p> <p><b>Complies.</b> Outdoor lighting associated with the proposed development will be designed, installed and operated in accordance with the relevant requirements.</p> <p><b>Not Applicable.</b> The proposed development does not involve sporting fields or sporting courts.</p> <p><b>Not Applicable.</b> The proposed development does not involve sporting fields or sporting courts.</p> <p><b>Not Applicable.</b> The proposed development does not involve private sporting courts.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>Habitable Room window of an adjoining Dwelling; and</p> <p>(b) the luminaire does not exceed a height of 8m above the court surface.</p> <p>AO<sub>1.6</sub> The alignment of streets, driveways and servicing areas avoid vehicle headlight impacts on adjacent residential dwellings.</p>	<p><b>Not Applicable.</b></p> <p>The proposed development will involve low level security lighting only.</p>
<p>PO<sub>2</sub> Outdoor lighting (excluding street lighting, normal residential lighting and low level security lighting) situated in excess of 4m above ground level does not jeopardise the safety or well-being of any pedestrian, cyclist or motorist. Light emissions do not reduce the ability of transport system users to see essential details of the route ahead, including signalling systems and signage.</p>	<p>AO<sub>2.1</sub> Outdoor lighting situated in excess of 4m above ground level is provided in accordance with Australian Standard AS1158.1.1:2005 – <i>Road Lighting – Vehicular Traffic (Category V) Lighting – Performance and Installation Design Requirements</i>.</p>	<p><b>Not Applicable.</b></p> <p>The proposed development will involve low level security lighting only.</p>
<p>PO<sub>3</sub> Outdoor Lighting does not cause unreasonable disturbance or cause detrimental impacts to any significant natural environment.</p>	<p>AO<sub>3.1</sub> The vertical illumination emanating from the outdoor lighting does not exceed one (1) lx on land within the Environmental Significance Overlay.</p>	<p><b>Not Applicable.</b></p> <p>The site is not located within the Environmental Significance Overlay,</p>
<p>PO<sub>4</sub> Proposed sensitive land uses adjoining existing lawful non-residential uses with significant lighting for community purposes, security or safety reasons are designed to proactively address possible obtrusive light nuisance.</p>	<p>AO<sub>4.1</sub> Proposed sensitive land uses adjoining existing lawful non-residential uses with significant lighting for community purposes, security or safety reasons are designed in a manner to mitigate any light nuisance impacts from the existing lawful use by establishing:</p>	<p><b>Not Applicable.</b></p> <p>The site does not adjoin a non-residential land use.</p>

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**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Odour</b>		
<p>PO<sub>5</sub> Development does not unreasonably affect the amenity and environmental quality of environs, especially of any nearby residential premises or public spaces due to odour impacts.</p>	<p>AO<sub>5.1</sub> The development does not involve activities that create odorous air emissions.</p> <p>OR</p> <p>AO<sub>5.2</sub> The development does not result in air emissions that exceed any of the acceptable levels specified within the <i>Environmental Protection (Air) Policy 2008</i>.</p> <p>AO<sub>5.3</sub> The development will not result in the release of noxious or offensive odours beyond the boundary of the site that cause environmental nuisance at any odour sensitive place, i.e. sufficient buffering is available within the development site itself to dissipate odour issues.</p> <p><i>Note: An Odour Assessment Report provided with the application may be necessary to demonstrate compliance with AO5.3.</i></p>	<p><b>Complies.</b> The proposed development is not of a nature that will result in the generation of odorous air emissions.</p>
<p>PO<sub>6</sub> Lot reconfigurations for residential or other environmentally sensitive land uses do not encroach upon existing or approved uses that may detrimentally impact upon the amenity of those proposed uses in terms of odour nuisance.</p>	<p>AO<sub>6.1</sub> Lots for residential or other environmentally sensitive land uses are not located within the distances from specific uses outlined in Table 9.4.2:2 at the end of this code.</p>	<p><b>Complies.</b> The proposed development is not located within the distances to any of the uses listed in Table 9.4.2:2.</p>

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**Environmental Standards Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary					
	<p>AO<sub>6.2</sub> Where lots for residential or other environmentally sensitive land uses are located within the distances from specific existing uses outlined in Table 9.4.2:2, an Odour Assessment Report has been provided to demonstrate that the development will achieve the following thresholds therefore minimising odour nuisance.</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="background-color: #cccccc;">Existing Use/Activity</th> <th style="background-color: #cccccc;">Odour Level at Sensitive Receiving Environment.</th> </tr> </thead> <tbody> <tr> <td rowspan="2" style="text-align: center;">All Activities</td> <td>2OU/m<sup>3</sup> 3 minute average, 99.5th percentile.</td> </tr> <tr> <td>4OU/m<sup>3</sup> 3 minute average, 99.9th percentile.</td> </tr> </tbody> </table>	Existing Use/Activity	Odour Level at Sensitive Receiving Environment.	All Activities	2OU/m <sup>3</sup> 3 minute average, 99.5th percentile.	4OU/m <sup>3</sup> 3 minute average, 99.9th percentile.	<p><b>Not Applicable.</b> See response to AO<sub>6.1</sub>.</p>
Existing Use/Activity	Odour Level at Sensitive Receiving Environment.						
All Activities	2OU/m <sup>3</sup> 3 minute average, 99.5th percentile.						
	4OU/m <sup>3</sup> 3 minute average, 99.9th percentile.						
<p>PO<sub>7</sub> Putrescibles waste generated as a result of the development does not cause odour nuisance issues for adjoining land uses.</p>	<p>AO<sub>7.1</sub> The development ensures that all putrescibles waste will be stored in a manner that prevents odour nuisance and fly breeding and will be disposed of at intervals not exceeding seven (7) days.</p>	<p><b>Complies.</b> All general waste will be stored in a manner that ensures that fly breeding and odour nuisance does not occur. Waste will be disposed at regular intervals via Council's regular waste collection program.</p>					
<b>Noise</b>							
<p>PO<sub>8</sub> The generation of noise from the premises does not cause Environmental Harm or Nuisance to adjoining properties or other noise sensitive land uses. (a) Development: (i) is located in an appropriate zone; (ii) proposes best practice design and construction materials (in relation to noise attenuation); and</p>	<p>AO<sub>8.1</sub> The development will achieve the following noise levels (when measured at the nearest sensitive receiver): (a) Background (L<sub>90</sub>) + 5dB(A) for variable noise between the hours of 7:00 am to 10:00 pm (measured at the facade of the sensitive land use); (b) Background (L<sub>90</sub>) + 3dB(A) for variable noise between the hours of 10:00 pm and 7:00 am</p>	<p><b>Not Applicable.</b> The proposed development is not of a nature that will result in adverse noise impacts.</p>					

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>(iii) proposes operational practices that will minimise noise nuisance for adjoining sensitive land uses.</p>	<p>(measured within bedrooms assuming open windows);</p> <p>(c) Background (<math>L_{90}</math>) for continuous noise sources (measured at the facade of the sensitive land use between 7:00 am and 10:00 pm and within bedrooms assuming open windows from 10:00 pm – 7:00 am); and</p> <p>(d) maximum limit <math>L_{Amax}</math> 45dB(A) inside dwellings; and</p> <p>The development will achieve the Acoustic Quality Objectives listed within the <i>Environmental Protection (Noise) Policy 2008</i>.</p> <p>HOWEVER</p> <p>AO<sub>8.2</sub> Where a development is unable to meet noise levels specified in AO<sub>8.1</sub> an acoustic assessment has been undertaken by a suitably qualified and skilled person which demonstrates that the development will not result in environmental nuisance at any existing or likely future residential premises (within a 10 year planning horizon).</p>	
<p>PO<sub>9</sub> Development (other than licensed premises operating under a Liquor Licensing Approval) proposing the use of amplified sound equipment is designed, constructed and operated in a manner that is sensitive to the impacts of high and low frequency noise on adjoining sensitive land uses.</p>	<p>AO<sub>9.1</sub> Where development (other than licensed premises operating under a Liquor Licensing Approval) proposes the use of amplified sound equipment, existing background octave band centre frequencies have been assessed and the development proposes the following maximum sound pressure criterion:</p>	<p><b>Not Applicable.</b></p> <p>The proposed development will not involve the use of amplified sound equipment.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary																											
	<table border="1" style="margin-left: auto; margin-right: auto;"> <thead> <tr style="background-color: #cccccc;"> <th colspan="9">Frequency - Hz - "A" Weighted</th> </tr> <tr style="background-color: #cccccc;"> <th></th> <th>31</th> <th>63</th> <th>125</th> <th>250</th> <th>500</th> <th>1K</th> <th>2K</th> <th>4K</th> </tr> </thead> <tbody> <tr style="background-color: #cccccc;"> <th style="writing-mode: vertical-rl; transform: rotate(180deg);">Background Level SPL dB(A)</th> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> <td>L<sub>90</sub> + 5dB(A)</td> </tr> </tbody> </table>	Frequency - Hz - "A" Weighted										31	63	125	250	500	1K	2K	4K	Background Level SPL dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	L <sub>90</sub> + 5dB(A)	
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<p>PO<sub>10</sub> Proposed sensitive land uses in close proximity to existing lawful land uses involving significant noise emissions such as entertainment venues, child care centres, industrial zones or other commercial premises are designed and constructed in a manner that achieves acoustic amenity for the users of the development.</p>	<p>AO<sub>10.1</sub> The development is designed to achieve the internal noise criterion (Acoustic Quality Objectives) for the particular use as specified within the <i>Environmental Protection (Noise) Policy 2008</i>.</p> <p>AO<sub>10.2</sub> Where the proposed sensitive land use is not listed in the Environmental Protection (Noise) Policy 2008, the development is designed and constructed to meet the internal sound level design criterion contained in Australian Standard AS2107:2000 <i>Acoustics – Recommended design sound levels and reverberation times for building interiors</i>.</p> <p>AO<sub>10.3</sub> Where the sensitive land use is within or adjoining entertainment venues utilising amplified music the applicant has undertaken an acoustic assessment of existing background octave levels and designed the sensitive land use component to mitigate the impacts of low frequency noise (particularly between 31.5Hz and 125Hz).</p>	<p><b>Not Applicable.</b> The proposed development is not located within proximity of a use that will result in significant noise emissions.</p>																											

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PO <sub>11</sub> Proposed sensitive land uses adjoining Council controlled arterial roads (other than designated 'Transport Noise Corridors') are designed and constructed in a manner that provides acoustic amenity for users/residents of the development.	<p>Where development involves a sensitive land use adjoining a Council controlled arterial road (other than designated 'Transport Noise Corridors':</p> <p>AO<sub>11.1</sub> The development is designed and constructed in a manner that achieves the internal noise Acoustic Quality Objectives listed within the <i>Environmental Protection (Noise) Policy 2008</i>.</p> <p>AO<sub>11.2</sub> The siting of buildings and selection of construction materials complies with the specifications of Australian Standard AS3671-1989 <i>Acoustics – Road traffic noise intrusion – Building siting and construction</i>.</p>	<p><b>Not Applicable.</b> The site does not adjoin a Council controlled Arterial Road.</p>
PO <sub>12</sub> Proposed sensitive land uses adjoining 'Transport Noise Corridors' as designated by State or Local Government are designed and constructed in a manner that provides acoustic amenity for users/residents of the development.	AO <sub>12.1</sub> Proposed sensitive land uses adjoining 'Transport Noise Corridors' as designated by State or Local Government comply with the <i>Queensland Development Code Mandatory Part (MP) 4.4 'Buildings in a Transport Noise Corridor'</i> for all habitable rooms adjoining the corridor.	<p><b>Not Applicable.</b> The site does not adjoin a Transport Noise Corridor.</p>
PO <sub>13</sub> Air conditioning units, refrigeration units and any other form of mechanical ventilation or extraction systems do not adversely impact on the acoustic amenity of surrounding sensitive land uses.	<p>AO<sub>13.1</sub> Plant of this nature is not elevated, is acoustically shielded (if necessary) and will not be audible at adjoining sensitive receivers.</p> <p>AO<sub>13.2</sub> Roof-top mounted plant and equipment is located away from surrounding sensitive land uses and is acoustically shielded to achieve a nil increase in background noise levels (L<sub>90</sub>) at the nearest sensitive receiver.</p>	<p><b>Complies.</b> Mechanical plants will not be elevated and will be acoustically shielded as required to ensure they do not adversely affect nearby premises.</p> <p><b>Not Applicable.</b> The proposed development will not involve roof-top mounted mechanical plants.</p>
PO <sub>14</sub> The construction phase of the development does not cause adverse acoustic impacts on surrounding sensitive receivers.	AO <sub>14.1</sub> Building work (including excavation and filling) is only conducted between the hours of 6:30 am and 6:30 pm Monday to Saturday (excluding public holidays).	<p><b>Complies.</b> Building work will be completed between the hours of 6:30am and 6:30pm from Monday to Sunday.</p>

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	AO <sub>14.2</sub> Where building work is proposed outside of the acceptable timeframe of 6:30 am to 6:30 pm (Monday – Saturday) the applicant has supplied a 'construction management plan' which adequately addresses noise mitigation measures.	<b>Not Applicable.</b> See response to AO <sub>14.1</sub> .
PO <sub>15</sub> Private sporting courts do not create acoustic amenity issues for surrounding sensitive receivers.	AO <sub>15.1</sub> Private sporting courts are not used between 10:00 pm and 7:00 am. AO <sub>15.2</sub> Mechanical equipment such as ball throwing machines which create audible noise at the nearest sensitive receiver is not used between 7:00 pm and 7:00 am.	<b>Not Applicable.</b> The proposed development will not involve a private sporting court.
PO <sub>16</sub> Vibration from the development does not affect the amenity of surrounding sensitive land uses or cause environmental harm or nuisance.	AO <sub>16.1</sub> The development does not result in vibration impacts outside of the development site.  AO <sub>16.2</sub> Where vibration may impact on surrounding sensitive land uses, the proponent has provided a vibration impact assessment or alternatively included vibration within an environmental impact report for the site which demonstrates that the level of vibration will not cause adverse amenity impacts or cause environmental harm or nuisance at any sensitive land use surrounding the development.	<b>Complies.</b> The proposed development is not of a nature that will result on offsite vibration impacts.  <b>Not Applicable.</b> The proposed development is not of a nature that will result on offsite vibration impacts.

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<b>Dust</b>		
<p>PO<sub>17</sub> The construction phase of the development prevents or mitigates (to an acceptable level) the release of dust particles which have potential to cause environmental nuisance to adjoining sensitive receivers (including sensitive receivers along haulage routes during excavation and filling operations).</p>	<p>AO<sub>17.1</sub> Off-site release of dust particles will be strictly managed to ensure that dust emissions do not travel beyond the property boundary and environmental nuisance does not occur.</p> <p>AO<sub>17.2</sub> Areas of exposed fill, excavation and unsealed accesses on the site are watered regularly (particularly during periods of high or constant wind) to reduce dust generation.</p> <p>AO<sub>17.3</sub> Areas of fill and excavation are graded, compacted and planted and/or mulched immediately after the dumping operation is complete.</p> <p>AO<sub>17.4</sub> Stockpiles of aggregate, sand or other materials brought onto the site are sprayed with water (or treated with an alternative method) to minimise dust nuisance. The frequency of water spraying is increased during hot, dry periods or where wind conditions are such that a dust nuisance is likely to occur. Stockpiles are located away from adjoining sensitive land uses.</p> <p><i>Note: Where excavation and filling exceeds 1,000 cubic metres the development has submitted a 'construction management plan' which adequately addresses dust mitigation measures. Measures must include strategies such as progressive rehabilitation and complaints processes.</i></p>	<p><b>Complies.</b> Construction activities will be managed to ensure that off-site release of dust particles does not occur.</p> <p><b>Complies.</b> Exposed areas will be regularly watered to minimise dust generation.</p> <p><b>Complies.</b> Areas of fill and excavation will be treated as required following a dumping operation.</p> <p><b>Complies.</b> Stockpiles will be sprayed with water as required to avoid dust nuisance.</p>

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PO <sub>18</sub> Haulage activities associated with excavation and filling are managed to prevent environmental nuisance issues.	AO <sub>18.1</sub> Haulage routes are selected on the basis of using the most suitable road surface to prevent dust generation and minimising the number of dwellings or other sensitive land uses affected by potential dust nuisance.	<b>Complies.</b> Haulage routes will be selected to reduce the number of people impacted by dust generation.
PO <sub>19</sub> Water used for dust suppression activities does not itself create environmental harm.	AO <sub>19.1</sub> Water approved as a method for controlling dust emissions must not be used in a manner that enables contaminated water to enter any stormwater system or natural drainage corridor outside of the site boundaries.	<b>Complies.</b> Water used for spraying will be used in a manner that ensures receiving waters are not contaminated.
PO <sub>20</sub> The ongoing operation of the development site does not create dust nuisance for adjoining landholders.	<p>AO<sub>20.1</sub> Areas within the site that are frequently used for vehicular purposes are imperviously sealed.</p> <p>AO<sub>20.2</sub> Industry-specific activities undertaken on site that create dust are performed in an enclosed shed or other structure with suitable dust extraction and filtration systems.</p> <p>AO<sub>20.3</sub> Grain facilities are equipped with semi enclosed grain receival hoppers fitted with dust extraction and filtration systems. All conveyor belts and bulk grain processing equipment are enclosed to prevent dust emission. Bunker storage without dust extraction is only permitted whereby the release of dust will not impact on surrounding sensitive receivers.</p> <p>AO<sub>20.4</sub> All development likely to generate any significant amount of dust must have an adequate water supply available at all times in order to undertake proactive dust reduction measures e.g. watering of access roads.</p>	<p><b>Complies.</b> Areas used for vehicular access will be imperviously sealed.</p> <p><b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.</p> <p><b>Not Applicable.</b> The proposed development is for a Multiple Dwelling.</p> <p><b>Not Applicable.</b> The proposed development is not of a nature that will generate dust emissions.</p>

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	AO <sub>20.5</sub> Development that is likely to create ongoing significant dust issues has submitted a 'site based management plan' which adequately addresses dust mitigation measures.	<b>Not Applicable.</b> The proposed development is not of a nature that will generate dust emissions.
PO <sub>21</sub> Proposed sensitive land uses are adequately separated from existing lawful land uses likely to generate dust emissions such as landfill sites, quarries, cropping land, motor sport facilities and other similar dust generating activities.	AO <sub>21.1</sub> Sensitive land uses achieve the separation distances from the nominated uses specified in Table 9.4.2:2.	<b>Complies.</b> The proposed development achieves the setbacks in Table 9.4.2:2.
PO <sub>22</sub> Development does not result in dustfall quantities that are likely to impact on the health of surrounding sensitive receivers.	AO <sub>22.1</sub> Dustfall averaged over an annual period of time does not exceed 133mg/m <sup>2</sup> /day when measured at the nearest sensitive receiver.	<b>Not Applicable.</b> The proposed development is not of a nature that will result in the generation of dust.
<b>General Emissions</b>		
PO <sub>23</sub> Air emissions resulting from development do not cause environmental harm (including environmental nuisance).	AO <sub>23.1</sub> The development does not result in air emissions that exceed any of the acceptable levels specified within the <i>Environmental Protection (Air) Policy 2008</i> .	<b>Not Applicable.</b> The proposed development is not of a nature that will result in the generation of adverse air emissions.
	AO <sub>23.2</sub> Where a type of air emission is not listed within the <i>Environmental Protection (Air) Policy 2008</i> the proponent can demonstrate that the level of emission is in compliance with Australian ambient air quality standards; or  If Australian standards do not exist, an ambient air quality standard from another country or organisation may be used with appropriate justification.	<b>Not Applicable.</b> The proposed development is not of a nature that will result in the generation of adverse air emissions.
	AO <sub>23.3</sub> Where a development is proposing to generate and release air emissions in excess of current air quality emission standards the proponent will provide an 'air quality impact assessment'	<b>Not Applicable.</b> The proposed development is not of a nature that will result in the generation of adverse air emissions.

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	which adequately addresses the impact of the release and provides justification as to why the industry cannot mitigate the levels further.	
PO <sub>24</sub> Child Care Centres are well located to avoid any harmful impacts from air pollution.	AO <sub>24.1</sub> Maximum concentrations of air pollutants do not exceed those recommended by the National Health and Medical Research Council.	<b>Not Applicable.</b> The proposed development is not of a nature that will result in the generation of adverse air emissions.
PO <sub>25</sub> Proposed sensitive land uses are adequately separated from existing lawful land uses that produce air emissions.	AO <sub>25.1</sub> Sensitive land uses in relation to air emissions are not located within the separation distances specified in Table 9.4.2:2.	<b>Complies.</b> The proposed development achieves the setbacks in Table 9.4.2:2.
PO <sub>26</sub> Electromagnetic radiation levels from telecommunications and other facilities do not pose health risks to the community.	AO <sub>26.1</sub> Emission levels from equipment and infrastructure comply with the relevant industry standards as demonstrated through an approved written statement or certification provided by the carrier to council i.e. Electromagnetic Energy report.	<b>Not Applicable.</b> The proposed development will not involve electromagnetic radiation.
PO <sub>27</sub> Air emission vents or stacks are sited appropriately to ensure that surrounding land uses are not exposed to concentrated levels of air contaminants.	<p>AO<sub>27.1</sub> Car park exhaust stacks are located away from adjoining sensitive receivers.</p> <p>AO<sub>27.2</sub> Emissions are discharged vertically and have an exit velocity of at least 10m/second.</p> <p>AO<sub>27.3</sub> Spray booth exhaust stacks are at least 8m in height or 4m higher than the adjoining ridgeline of a neighbouring building (if the building is within 40m of the emission point), whichever is the greater.</p> <p>AO<sub>27.4</sub> Tank venting for hydrocarbon fuel storage and LP Gas is located in accordance with AS1940-2004 <i>The Storage and handling of flammable and combustible liquids (for hydrocarbons) and</i></p>	<p><b>Not Applicable.</b> The proposed development does not involve carpark exhaust stacks.</p> <p><b>Not Applicable.</b> See response to AO<sub>27.1</sub>.</p> <p><b>Not Applicable.</b> The proposed development does not involve a spray booth.</p> <p><b>Not Applicable.</b> The proposed development does not involve the storage of hydrocarbon or LPG.</p>

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	<i>AS1596:2008 The Storage and Handling of LP Gas.</i>	
<b>Waste Management</b>		
PO <sub>28</sub> The development (excluding high rise buildings in excess of three (3) stories) is designed to ensure that waste storage and collection can be undertaken in a manner that complies with Council's <i>Technical Guidelines for New Developments Waste Storage and Collection Requirements</i> .	AO <sub>28.1</sub> For commercial premises and industry activities (other than those premises utilising Council's wheelie bin waste collection program): <ul style="list-style-type: none"> <li>(a) general waste and recycling containers are located within the curtilage of the property in an area that enables the waste collection truck to pick up the containers while entering and leaving the premises in a forward gear;</li> <li>(b) a container storage area is dedicated that is large enough to cater for the expected volume of general waste and recycling;</li> <li>(c) storage areas are screened either behind a building or using screening materials or landscaping to a minimum height of 1.5m;</li> <li>(d) where bulk bins (or alternative combined waste and recycling containers exceeding 2 cubic metres) are proposed the bin storage area is roofed and bunded, contains an impervious surface, is in close proximity to a hose cock and is graded and drained to either a wastewater system connection (requiring a trade waste approval) where sewer is available or in sewerred areas, storage areas are drained to an area of significant landscaping, waste water treatment device or water quality improvement system e.g. Bioretention;</li> </ul>	<b>Not Applicable.</b> The proposed development is for a residential use.

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	<p>(e) where bulk bins (or alternative combined waste and recycling containers exceeding 2 cubic metres) are proposed the bin storage area is designed to enable bins to be washed out within the storage area and drained to a sewer system (requiring trade waste approval) within sewer areas or area of significant landscaping, water treatment device or water quality improvement system e.g. Bioretention in non-sewered areas; and</p> <p>(f) bin storage areas do not pose amenity issues for surrounding sensitive receivers, including odour during storage periods or noise issues resulting from collection programs.</p>	
	<p>AO<sub>28.2</sub> For a Multiple Dwelling of three (3) – six (6) units the development satisfies one of the following criteria:</p> <p>(a) a minimum road frontage is available within the immediate road reserve adjoining the development in order to place the required number of waste and recycling containers out for collection (2 x 240L wheelie bins per tenement) when calculated at 1m/bin e.g. a development requiring eight (8) bins must have at least 8m of useable road reserve (in terms of bin collection, excluding a 1m clearance around power poles and any area below a street trees canopy where bins cannot be collected);</p> <p>OR</p>	<p><b>Complies.</b> The proposed development is for a Multiple Dwelling involving three (3) units and has suitable road frontage for the collection of bins by Council's wheelie bin collection service.</p>

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	<p>(b) the complex includes a communal bin storage area, whereby the body corporate will implement internal procedures requiring residents to progressively fill bins and only place full bins out for collection; and</p> <p>(c) each tenement has an approved bin storage area that will not create amenity issues for surrounding sensitive land uses; and</p> <p>(d) bin storage areas are screened behind buildings for amenity purposes; and</p> <p>(e) storage areas are not within dwellings (including garages) and it is not necessary to take the bins through dwellings (including garages) for collection purposes; and</p> <p>(f) a hose cock is located in close proximity to the storage location to enable bins to be cleaned; and</p> <p>(g) where a rear storage area is not possible bins are stored in a minimum 1.5m high screened area in the front of the dwelling(s);</p> <p align="center">OR</p> <p>(h) screened communal storage areas (to a minimum height of 1.5m) are proposed which contain an impervious floor, hose cock and grading/drainage towards a grassed area or other porous surface.</p>	
	<p>AO<sub>28.3</sub> For a Multiple Dwelling above six (6) units the development satisfies one of the following criteria:</p>	<p><b>Not Applicable.</b> The proposed development does not involve more than six (6) dwellings.</p>

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	<p>(a) The development incorporates 'internal collection' of either bulk bins or wheelie bins (in accordance with the waste management guideline that accompanies the environmental standard);</p> <p>OR</p> <p>(b) Communal bin storage areas contain a roof, bunding and bin 'washing' provisions in the form of either a sewer connection (requiring trade waste approval) or where no sewer is available a connection to a waste water treatment device, drain to an area of significant landscaping or drain to a water quality improvement device e.g. Bioretention system is acceptable;</p> <p>OR</p> <p>(c) Where 'internal collection' is proposed the internal design complies with the waste management guideline that accompanies this environmental standard and a certification from a registered RPEQ has been provided to demonstrate that manoeuvrability is acceptable for an appropriately sized refuse vehicle.</p>	
	<p>AO<sub>28.4</sub> Commercial premises utilising Council's wheelie bin waste collection service to dispose of commercial waste:</p> <p>(a) utilise a maximum of four (4) wheelie bins i.e. less than 1 cubic metre;</p> <p>(b) store bins within the curtilage of the property in a designated area in close proximity to a hose cock, whereby any</p>	<p><b>Not Applicable.</b> The proposed development is for a residential use.</p>

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	<p>adjoining sensitive land uses will not experience amenity issues i.e. odour;</p> <p>(c) store bins on an impervious surface;</p> <p>(d) place bins on the road reserve for a maximum period of 24 hours during collection programs; and</p> <p>(e) store bins in an area that is screened from public view either in a building, behind a building or within a purpose built screened storage area within a 1.5m minimum height.</p>	
<p>PO<sub>29</sub> Development for a Community Residence, Residential Care Facility or Retirement Facility utilising communal bin storage areas ensure that residents have reasonable access to waste containers, where the development is for aged care purposes.</p>	<p>AO<sub>29.1</sub> For development involving a Community Residence, Residential Care Facility or Retirement Facility, bin storage areas are located within reasonable proximity to all units, in accordance with Council's Environmental Guideline.</p>	<p><b>Not Applicable.</b> The proposed development involves a Multiple Dwelling.</p>
<p>PO<sub>30</sub> High rise (in excess of three (3) storeys) residential developments and joint commercial and residential developments are designed to enable best practice waste management principles to be applied.</p>	<p>AO<sub>30.1</sub> The applicant has provided a waste management plan that as a minimum has addressed the following issues:</p> <p>(a) likely waste quantity and waste type to be generated on site on a weekly basis;</p> <p>(b) likely recycling quantities to be generated on a weekly basis;</p> <p>(c) waste container and recycling container (type and volume) requirements for the residential component (based on 240L of general waste and 240L of recycling per</p>	<p><b>Not Applicable.</b> The proposed development does not include a high rise.</p>

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	unit) and the commercial component (if applicable); (d) waste storage area locations; (e) dual waste chutes for general waste and recycling; (f) bin room specifications and hygiene practices for waste handling areas, chutes, waste containers and other applicable equipment; (g) collection arrangements and manoeuvring diagrams (including overhead clearances); (h) waste minimisation practices; (i) use of compactors; (j) an impact assessment of waste management practices on any surrounding sensitive land uses; (k) air extraction fans, refrigeration or associated devices for refuse storage areas to prevent odour, particularly where putrescibles are stored; and (l) clinical and related waste storage and collection issues (if applicable).	
PO <sub>31</sub> Demolition and building activities actively involve waste minimisation and waste avoidance principles including the promotion of recycling and re-use.	AO <sub>31.1</sub> The development will be carried out in accordance with the waste management hierarchy outlined in the <i>Technical Guideline for New Developments Waste Storage and Collection Requirements</i> and the applicant has nominated the quantity and type of materials that will be disposed of to landfill.	<b>Complies.</b> Building activities will be undertaken in accordance with Council's requirements.
PO <sub>32</sub> Development that involves the generation of 'clinical and related waste' as per the definition of the <i>Technical Guideline for New</i>	Where involving development that involves the generation of 'clinical and related waste' as per the definition of the <i>Technical Guideline for New</i>	<b>Not Applicable.</b> The proposed development is for Multiple Dwelling and will not generate clinical or related waste.

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<p><i>Developments Waste Storage and Collection Requirements</i> is designed to adequately cater for legislative storage and collection requirements.</p>	<p><i>Developments Waste Storage and Collection Requirements:</i></p> <p>AO<sub>32.1</sub> The storage of 'clinical and related waste' is in accordance with the <i>Technical Guideline for New Developments Waste Storage and Collection Requirements</i> with storage locations being demonstrated on submitted site/floor plans.</p> <p>AO<sub>32.2</sub> The development has proposed a method of disposing of 'clinical and related waste' and has demonstrated that an applicable waste collection vehicle is able to manoeuvre on site, while entering and leaving the premises in a forward gear.</p>	
<p>PO<sub>33</sub> Residential development involving 'internal collection' of either bulk bins or wheelie bins is designed to a standard that enables heavy vehicle access and manoeuvring whilst providing safety to residents and the protection of infrastructure.</p>	<p>AO<sub>33.1</sub> The development is designed and certified by a RPEQ and complies with the requirements outlined in Council's <i>Technical Guidelines for Waste Storage and Collection Requirements for New Developments</i>, including:</p> <ul style="list-style-type: none"> <li>(a) appropriate manoeuvring is adequate with vehicles being able to enter and exit the property in a forward gear;</li> <li>(b) overhead clearance is adequate for the applicable refuse vehicle;</li> <li>(c) road surface is appropriate for a HRV;</li> <li>(d) side clearance is appropriate for wheelie bin collection;</li> <li>(e) collection areas are appropriate for either bulk bins or wheelie bins;</li> <li>(f) minimum road width of 5.5m; and</li> </ul>	<p><b>Not Applicable.</b> The proposed development will not involve the internal collection of waste.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	(g) internal road networks enable the refuse vehicle to traverse the site without resident safety being jeopardised.	
PO <sub>34</sub> Development involving refuse storage and collection external to Council's waste contract utilise waste containers and hygiene practices that prevent odour issues and remove harbourage opportunities for vermin and mosquitoes.	AO <sub>34.1</sub> The applicant will utilise the following control measures: (a) putrescibles waste will be removed from the property at intervals not exceeding seven (7) days (putrescibles will be refrigerated where possible and appropriate); (b) tight fitting lid assemblies will be utilised on all waste containers to prevent the pooling of rainwater, thus minimising mosquito breeding opportunities; and (c) bins will be secured to ensure that vermin and pest animals do not have access to a potential food source; and (d) bins will be cleaned on an 'as needed' basis if odour is identified as an issue.	<b>Not Applicable.</b> The proposed development will not involve waste collection and storage external to Council's waste contract.

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**Integrated Water Cycle Management Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes or compliance outcomes if made applicable to compliance assessable development	Compliance summary
<b>Stormwater Management</b>		
PO <sub>1</sub> Development does not adversely impact on the quality of receiving waters by avoiding or minimising pollutants entering and being transported with stormwater.	AO <sub>1.1</sub> Stormwater quality treatment measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> . AO <sub>1.2</sub> Pollutant load reductions are achieved in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> The proposed development has been designed to maintain stormwater quality. Reference is made to the Conceptual Stormwater Report attached at <b>Appendix D</b> .
PO <sub>2</sub> Adverse impacts of construction activities on stormwater quality are avoided or minimised using best practice environmental management for erosion and sediment control.	AO <sub>2.1</sub> Sediment and erosion control measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Sediment and erosion control measures will be provided in accordance with the relevant requirements. Reference is made to the Conceptual Stormwater Report attached at <b>Appendix D</b> .
PO <sub>3</sub> Stormwater management incorporates water sensitive urban design techniques and avoids adverse impacts from water quantity, flow rates and duration and frequency in receiving waters, having regard to: (a) channel, bed and bank stability; (b) aquatic and riparian ecosystems; and (c) hydrological functions.	AO <sub>3.1</sub> Stormwater flow control measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Stormwater flow controls measures will be provided where required in accordance with the relevant requirements. Reference is made to the Conceptual Stormwater Report attached at <b>Appendix D</b> .
<b>Waste Water Management</b>		
PO <sub>4</sub> Development does not discharge waste water to a waterway or external to the site unless demonstrated to be best practice environmental management for that site and has appropriate regard for:	AO <sub>4.1</sub> Waste water management measures are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Wastewater measurement measures can be implemented in accordance with the relevant standards.

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**Integrated Water Cycle Management Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes or compliance outcomes if made applicable to compliance assessable development	Compliance summary
<ul style="list-style-type: none"> <li>(a) cumulative effects;</li> <li>(b) the applicable water quality objectives for the receiving waters;</li> <li>(c) adverse impact on ecosystem health of receiving waters; and</li> <li>(d) in waters mapped as being of high ecological value, the adverse impacts of such releases and their offset.</li> </ul>		
<b>Artificial Waterways and Water Bodies</b>		
PO <sub>5</sub> The waterway or water body is designed to integrate multiple functions, including: <ul style="list-style-type: none"> <li>(a) aesthetics, landscaping, and recreation;</li> <li>(b) flood management;</li> <li>(c) stormwater management;</li> <li>(d) water conservation and reuse;</li> <li>(e) community health; and</li> <li>(f) pest management.</li> </ul>	AO <sub>5.1</sub> Artificial waterways or water bodies are designed in accordance with <i>SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Not Applicable.</b> The proposed development does not involve artificial waterways or water bodies.
PO <sub>6</sub> The waterway is located and designed to be responsive to natural drainage features.	AO <sub>6.1</sub> Artificial waterways or water bodies are designed in accordance with <i>SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Not Applicable.</b> The proposed development does not involve artificial waterways or water bodies.
PO <sub>7</sub> The waterway or body is designed to minimise whole of life cycle costs.	AO <sub>7.1</sub> Artificial waterways or water bodies are designed in accordance with <i>SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Not Applicable.</b> The proposed development does not involve artificial waterways or water bodies.

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**Integrated Water Cycle Management Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes or compliance outcomes if made applicable to compliance assessable development	Compliance summary
<b>Flooding and Drainage</b>		
PO <sub>8</sub> Flooding and drainage characteristics upstream or downstream of the site are not worsened.	AO <sub>8.1</sub> Development is undertaken in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> The proposed development can be undertaken in accordance with the relevant standards. Refer to the Conceptual Stormwater Report at <b>Appendix D</b> .
PO <sub>9</sub> The drainage network has sufficient capacity to safely convey stormwater run-off from the site.	AO <sub>9.1</sub> Development is undertaken in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> The proposed development can be undertaken in accordance with the relevant standards. Refer to the Conceptual Stormwater Report at <b>Appendix D</b> .
PO <sub>10</sub> Stormwater resulting from roofed areas is collected and discharged in a manner that does not adversely affect the stability of buildings or the use of adjacent land.	AO <sub>10.1</sub> Roof water is collected and discharged in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Roof water can be collected and discharged in accordance with the relevant standards. Refer to the Conceptual Stormwater Report at <b>Appendix D</b> .
<b>Water Cycle Management</b>		
PO <sub>11</sub> The design and management of the development integrates water cycle elements so that:	AO <sub>11.1</sub> Integrated water management practices and infrastructure are implemented in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Integrated water management practices and infrastructure can be implemented in accordance with the relevant standards.

Toowoomba Regional Planning Scheme 2012 (v28.0)

**Integrated Water Cycle Management Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes or compliance outcomes if made applicable to compliance assessable development	Compliance summary
<ul style="list-style-type: none"> <li>(a) water is used efficiently and potable water demand is reduced;</li> <li>(b) wastewater production is minimised;</li> <li>(c) stormwater peak discharges and runoff volumes are not worsened;</li> <li>(d) natural drainage lines and hydrological regimes are maintained as far as possible;</li> <li>(e) large, uninterrupted impervious surfaces are minimised;</li> <li>(f) reuse of stormwater and grey-water is encouraged where public health and safety will not be compromised; and</li> <li>(g) water is used efficiently.</li> </ul>		

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Landscaping Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>1</sub> Landscape design is developed by a suitably qualified landscape professional and demonstrates an integrated approach to planning/development issues and documents both hard and soft works proposed for the development.	AO <sub>1.1</sub> Landscape documentation is prepared by the landscape professional identified in Table 9.4.4:2.	<b>Complies.</b> Landscaping documentation can be prepared by a suitably qualified professional prior to the construction of the development.
PO <sub>2</sub> Landscape construction is undertaken by a suitably qualified landscape professional.	AO <sub>2.1</sub> Landscape construction is carried out by a member of the Queensland Association of Landscape Industries.	<b>Complies.</b> Landscaping will be construction by a suitably qualified professional.
PO <sub>3</sub> Landscape design reflects the local context and incorporates cohesive and desirable aspects of the prevailing landscape character. (Desirable aspects are those considered necessary to maintain and enhance the character, setting and/or ambience, and ecological values of the location.)	AO <sub>3.1</sub> Where a street or locality has an identifiable character derived from existing vegetation, similar or identical plant species are used.  AO <sub>3.2</sub> Existing desirable landscape elements and treatments are incorporated into landscaping to integrate the development into the existing character of the area.  AO <sub>3.3</sub> Existing site trees are integrated into the development.  AO <sub>3.4</sub> Species selection is reflective of cool temperate species.	<b>Complies.</b> Species used in landscaping will be selected to achieve consistency with the established landscape character in the street. <b>Complies.</b> Landscape elements will be incorporated into landscape design where relevant.  <b>Not Applicable.</b> The site does not contain any existing trees. <b>Complies.</b> Species used in landscaping will be selected to be reflective of a cool temperate climate.
PO <sub>4</sub> Where the development involves the creation of a new road street tree planting is undertaken having consideration of: (a) the hierarchy and function of the street; (b) selection of appropriate species; (c) avoidance of conflict between the street tree and utilities and services within the road reserve; (d) soil conditions; (e) existing street trees;	Where the development involves the creation of a new road:  AO <sub>4.1</sub> Street planting is carried out in accordance with the requirements of <i>SC6.2 PSP No. 2 Engineering Services Infrastructure Roads and Drainage</i> . AO <sub>4.2</sub> Species and materials are used that minimise the use of potable water. AO <sub>4.3</sub> Street tree planting is in accordance with PSP No.8 – Street Trees.	<b>Not Applicable.</b> The proposed development does not involve the creation of a new road.

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Landscaping Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
(f) solar access; and (g) driveway access.		
PO <sub>5</sub> Fencing design and acoustic barriers: (a) are compatible with the existing streetscape and proposed development type; and (b) provide visual interest and address the street.	AO <sub>5.1</sub> Front fences longer than 15m and greater than 1,400mm in height are visually fragmented with recesses at least 1.2m deep and 1.2m wide at 15m intervals, planted with at least one tree and groundcovers.  AO <sub>5.2</sub> All planting and recesses along a fence are located within the property boundary and planting recesses are accessible from within the site.  AO <sub>5.3</sub> Where acoustic fencing is required by the planning scheme it is designed by an acoustic engineer and incorporates a minimum 3m vegetated buffer on either side of the fence with vegetation having a mature height equal to or above the height of the acoustic fencing.	<b>Not Applicable.</b> The proposed development does not involve a front fence.  <b>Not Applicable.</b> See response to AO <sub>5.1</sub> .  <b>Not Applicable.</b> The proposed development is not required to include acoustic fencing.
PO <sub>6</sub> Location, design and provision of planting in carparks and internal roadways achieve a high degree of shade, amenity and safety.	AO <sub>6.1</sub> Landscaping visually fragments and shades carparking areas with regular tree planting in individual planting bays evenly distributed throughout the car parking area at the rate of one planting bay per eight (8) carparking spaces.  AO <sub>6.2</sub> Individual planting bays have a minimum dimension of 1,500 x 1,500mm with permeable surface treatments and are flush with the finished surface levels of the car park.  AO <sub>6.3</sub> No raised kerbing is provided around planting bays. Wheelstops or bollards are used to delineate planting bays where necessary and finished carpark surface levels fall toward planting areas.	<b>Not Applicable.</b> The proposed development is not of a scale that warrants regular tree planting in planting bays.  <b>Not Applicable.</b> The proposed development does not include planting bays.  <b>Not Applicable.</b> The proposed development does not include planting bays.

Toowoomba Regional Planning Scheme 2012 (v28.0)

Landscaping Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>6.4</sub> Planting bays incorporate ground covers less than 1,000mm height that allow unobstructed surveillance.</p>	<p><b>Not Applicable.</b> The proposed development does not include planting bays.</p>

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**Landscaping Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>7</sub> Location and habit of tree planting must not interfere with the function and accessibility of any adjacent utility services.	AO <sub>7.1</sub> Species mature height and siting must not interfere with or compromise overhead and underground utility assets including stormwater inlet pits. AO <sub>7.2</sub> Tree planting must be a minimum of 2m from any mains water easements and offset 4m from any sewer main or inspection chamber.	<b>Not Applicable.</b> The proposed development does not involve tree planting.
PO <sub>8</sub> Maintenance access points must be considered and accommodated for in the site planning and design process.	AO <sub>8.1</sub> Access by appropriate maintenance or utility vehicles must be demonstrated with ground surface treatments that are stable and usable in all weather. AO <sub>8.2</sub> Functional maintenance vehicle circulation and access gates to be provided.	<b>Complies.</b> Maintenance vehicles can access the site via the existing driveway.
PO <sub>9</sub> On-site stormwater harvesting is to be maximised for irrigating landscaping in development with reuse measures and amelioration of stormwater impacts provided.	AO <sub>9.1</sub> Landscape design takes advantage of the flow of water along overland flow paths. AO <sub>9.2</sub> Landscaping is used to help maximise opportunities for on-site stormwater infiltration by: (a) minimising impervious surfaces and incorporating semi-permeable paving products; (b) falling hard surfaces towards pervious surfaces such as turf or mulched areas; (c) maximise opportunities for turf and planting areas; (d) align planting areas parallel to contours to slow the flow of surface water; and (e) ensure planting palette comprises canopy tree species. AO <sub>9.3</sub> Provision for drainage is incorporated through treatments such as subsurface drains, swales, ponds and infiltration cells.	<b>Not Applicable.</b> The site is not located within an overland flow path. <b>Complies.</b> The proposed development involves the provision of landscaping adjacent to impervious surfaces to maximise opportunities for onsite infiltration.  <b>Complies.</b> Additional drainage measures can be incorporated as required.

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**Landscaping Code – Assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>9.4</sub> Sediment and erosion control measures are provided.</p> <p>AO<sub>9.5</sub> Planter boxes on podiums and building forecourts are plumbed to stormwater.</p>	<p><b>Complies.</b> Sediment and erosion control measures can be incorporated as required.</p> <p><b>Not Applicable.</b> The proposed development does not involve planter boxes.</p>
<p>PO<sub>10</sub> Landscape design is integrated with any existing urban design theme within the surrounding area and coordinates paving, planting, street furniture, lighting, signage and other elements to reflect that theme and assist in the creation of a sense of place.</p>	<p>No acceptable outcome is nominated.</p>	<p><b>Complies.</b> Landscaping will be designed to integrate with the existing urban theme in the street to contribute to creating a sense of place</p>
<p>PO<sub>11</sub> Design of pedestrian paths and places reinforces the desired character of the area and/or place and includes features to enhance their use that are of universal design to ensure non-discriminatory access and use.</p>	<p>AO<sub>11.1</sub> Design complies with AS1428 parts 1, 2, 3, and 4 – <i>Design for Access and Mobility</i></p>	<p><b>Not Applicable.</b> The proposed development does not involve the provision of pedestrian paths or places.</p>
<p>PO<sub>12</sub> Risks to personal safety and the potential for crime, vandalism and fear are reduced through landscape design that has been informed by <i>Crime Prevention Through Environmental Design (CPTED)</i> principles in relation to:</p> <ul style="list-style-type: none"> <li>(a) Surveillance.</li> <li>(b) Access control.</li> <li>(c) Territorial reinforcement.</li> <li>(d) Space management.</li> </ul>	<p>Landscape design incorporates the following design measures:</p> <p>AO<sub>12.1</sub> The attractiveness of crime targets is minimised by providing opportunities for effective surveillance through: clear sight lines from private to public space, reducing concealment or entrapment opportunities, public facilities (toilets, shelters etc) located to promote use, dual access points, avoiding blind corners, and lighting where appropriate.</p> <p>AO<sub>12.2</sub> Barriers are used to attract, channel or restrict the movement of people by: clear spatial definition and legibility, optimising opportunity for public interaction, visually permeable</p>	<p><b>Complies.</b> Landscaping will be designed to ensure that clear sight lines into and within the site are created to promote safety within the site.</p> <p><b>Not Applicable.</b> The provision of barriers throughout the site is not warranted.</p>

Toowoomba Regional Planning Scheme 2012 (v28.0)

Landscaping Code – Assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>screens and fencing, appropriate use of mechanical measures that correspond to actual risk.</p> <p>AO<sub>12.3</sub> Reinforcing definition of territory and ownership of private, semi-public and public spaces through: clear design cues for use and activities, transitions and boundaries between public and private, design that encourages public interaction and ownership, legible universal signage.</p> <p>AO<sub>12.4</sub> Space Management: ensuring that public spaces are appropriately utilised and maintained by the use of vandal- and graffiti-resistant materials, easily accessed and maintained fixtures.</p>	<p><b>Complies.</b> Territorial reinforcement measures can be implemented to delineate the extent of areas useable by each unit within the site.</p> <p><b>Complies.</b> Vandal and graffiti resistant materials and treatments will be used throughout the site.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Transport, Access and Parking Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Driveway Crossovers</b>		
<p>PO<sub>1</sub> Vehicular access arrangements, including driveway crossovers:</p> <p>(a) are appropriate for:</p> <ul style="list-style-type: none"> <li>(i) the capacity of the parking area;</li> <li>(ii) the volume, frequency and type of vehicle usage; and</li> <li>(iii) the function and configuration of the access road;</li> </ul> <p>(b) minimise any potentially adverse impact on:</p> <ul style="list-style-type: none"> <li>(i) the safety and efficiency of the road and pedestrian/cycle paths;</li> <li>(ii) the safety and efficiency of the road and footpath users;</li> <li>(iii) the integrity of any infrastructure within the road reserve; and</li> <li>(iv) the safety of access to adjacent properties.</li> </ul> <p>(c) protect the amenity of premises in the vicinity by:</p> <ul style="list-style-type: none"> <li>(i) maintaining the predominant vehicular access pattern in the street, including consistent width, grade and location;</li> <li>(ii) preserving the residential amenity of the streetscape, including noise and visual impact, and consideration of existing landscaping by considering:</li> </ul>	<p>AO<sub>1.1</sub> Vehicular access and driveway crossovers are not:</p> <ul style="list-style-type: none"> <li>(a) an additional site/property access;</li> <li>(b) to a State-controlled Road or a road with bluestone kerbing;</li> <li>(c) within 25 m of a signalised road intersection;</li> <li>(d) within 20m of an unsignalised road intersection in a Commercial or Industrial Area;</li> <li>(e) within 10m of an unsignalised road intersection in a Community, Residential, Rural or Other Area;</li> <li>(f) within 1m of any infrastructure, including street signage, power poles, street lights, manholes, stormwater gully pits, or other Council/public utility asset;</li> <li>(g) within the Tree Protection Zone, as defined by Australian Standard 4970- 2009;</li> <li>(h) for a lot with a frontage of 10m or less;</li> <li>(i) greater than 4m in width when for a lot with a frontage / width of more than 10m but less than 20m; and</li> <li>(j) greater than 6m in width when for a lot with a frontage / width of greater than 20m.</li> </ul> <p><i>Note: An additional site access is considered to be more than one site access.</i></p>	<p><b>Not Applicable.</b></p> <p>The proposed development will utilise existing vehicle crossover and access arrangements.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Transport, Access and Parking Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<p>(A) use of materials which integrate with the streetscape (e.g. existing crossovers and driveways, etc);</p> <p>(B) minimising the width and grade of the access;</p> <p>(C) minimising impacts on the appearance of the streetscape by retaining existing vegetation, including approved landscaping; and</p> <p>(D) locating the access to minimise the impact of vehicle noise on neighbouring/adjoining properties.</p>	<p>AO<sub>1.2</sub> Except where in a Rural Zone, vehicular access and driveway crossovers:</p> <p>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</p> <p>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the roadway;</p> <p>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, bus stop, or other structure within the road reserve</p> <p>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</p> <p>(e) do not have access restricted by an access restriction strip or link reserve;</p> <p>(f) do not access an unformed or unkerbed road;</p> <p>(g) are constructed from reinforced concrete;</p> <p>(h) are perpendicular to the road edge; and</p> <p>(i) are provided in accordance with the <i>Australian Standard AS 2890.1 – Off Street Car Parking and Australian Standard AS 2890.2 (where relevant) and the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure.</i></p>	<p><b>Not Applicable.</b></p> <p>The proposed development will utilise existing vehicle crossover and access arrangements.</p>

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**Transport, Access and Parking Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>AO<sub>1.3</sub> Where in a Rural Zone, vehicular access and driveway crossovers:</p> <ul style="list-style-type: none"> <li>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</li> <li>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the roadway;</li> <li>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, loading bay, bus stop, or other structure within the road reserve;</li> <li>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</li> <li>(e) do not have access restricted by an access restriction strip or link reserve;</li> <li>(f) are sealed where accessing a sealed formed road;</li> <li>(g) are perpendicular to the road edge; and</li> <li>(h) are provided in accordance with the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure.</li> </ul>	<p><b>Not Applicable.</b> The proposed development is not located within the Rural Zone.</p>

**Toowoomba Regional Planning Scheme 2012 (v28.0)**

**Transport, Access and Parking Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Car Parking Provision</b>		
<p>PO<sub>2</sub> Provision is made for on-site vehicle parking to meet the demand likely to be generated by the development and to avoid on-street parking where that would adversely impact on the safety or capacity of the road network or unduly impact on local amenity.</p> <p><i>Note: Where the development does not meet the acceptable outcomes, or where no acceptable outcome is specified, a parking demand analysis report prepared by a suitably qualified person may assist in demonstrating compliance with the performance outcome.</i></p>	<p>AO<sub>2.1</sub> Where in the Principal Centre Zone or Mixed Use Zone Car parking is provided at the rate of:</p> <ul style="list-style-type: none"> <li>(a) Non-Residential Use one (1) parking space per 50m<sup>2</sup> of GFA; and</li> <li>(b) Residential Use - one (1) parking space per dwelling.</li> </ul> <p>AO<sub>2.2</sub> Where not in the Principal Centre Zone or Mixed Use Zone Car parking is provided at the rates set out in Table 9.4.6:3 to this Code.</p> <p><i>Note: Where a parking rate for a use is unspecified in Table 9.4.6:3 – no acceptable outcome is provided.</i></p> <p><i>Note: If the number of car parking spaces calculated in accordance with AO<sub>2.1</sub> and AO<sub>2.2</sub> is not a whole number, the number of parking spaces to be provided is rounded-up to next highest whole number.</i></p> <p><i>Note: Where application is made for establishment of two or more uses on the same premises, the parking demand is calculated by totalling the requirements for each use.</i></p>	<p><b>Not Applicable.</b> The proposed development is located within the Low Density Residential Zone.</p> <p><b>Performance Solution.</b> The existing dwelling units each contain a single car garage and will retain the existing visitor parking space at the front of the site. As there are no changes to how the two existing dwelling units will function, no changes are proposed to the existing car parking arrangement for these units. The proposed dwelling unit will include a single car garage and will utilise the existing parking space at the front of the site, ensuring on-site parking for at least two (2) vehicles is provided to the new dwelling unit in accordance with Table 9.4.6:3 to this Code.</p>

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Transport Network</b>		
PO <sub>1</sub> The development is located on roads that are appropriate for the nature of traffic generated, having regard to the safety and efficiency of the transport network, and the functions and characteristics identified in the transport network hierarchy contained in SC 6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	No acceptable outcome is nominated.	<b>Complies.</b> The proposed development is on a road which is sufficient for catering for the expected traffic to be generated. It is noted that Benjamin Street connects to other roads that are of a higher order and accordingly, the proposed development will not result in a compromise to the safety or efficiency of the surrounding road network.
PO <sub>2</sub> Development does not compromise the orderly provision or upgrading of the transport network.	No acceptable outcome is nominated.	<b>Complies.</b> The proposed will not compromise the orderly provision or upgrading of the transport network in the area.
PO <sub>3</sub> Onsite transport network infrastructure (including roads, parking, access and public transport, pedestrian and cyclist facilities) appropriately integrates with surrounding networks and facilitates the orderly development of adjoining land.	No acceptable outcome is nominated.	<b>Not Applicable.</b> The proposed development does not make provision for onsite transport network infrastructure.
PO <sub>4</sub> Development is designed to encourage travel by public transport, walking and cycling. This may include integrated access between adjoining sites and/or the provision of mid-block connections which are safe, functional and legible for potential users.	No acceptable outcome is nominated.	<b>Not Applicable.</b> The site is not serviced by active or public transport infrastructure.
PO <sub>5</sub> Car parking areas, pathways and other elements of transport network infrastructure are designed to enhance public safety by discouraging crime and anti-social behaviour, having regard to: (a) provision of opportunities for casual surveillance;	AO <sub>5.1</sub> Car parking areas, pathways and other elements of transport network infrastructure are designed in accordance with <i>Crime Prevention Through Environmental Design (CPTED) Guidelines</i> .	<b>Complies.</b> Carparking areas can be designed in accordance with CPTED principles.

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(b) provision of lighting;</li> <li>(c) the use of fencing to define public and private spaces, whilst allowing for appropriate sightlines;</li> <li>(d) minimising potential concealment points and assault locations;</li> <li>(e) minimising opportunities for graffiti and other vandalism; and</li> <li>(f) restricting unlawful access to buildings and between buildings.</li> </ul>		
PO <sub>6</sub> Directional signage is provided within a development site to assist legibility and way-finding, including for pedestrians and cyclists.	No acceptable outcome is nominated.	<b>Not Applicable.</b> The nature and scale of the development is such that directional signage will not be required.
<b>Access</b>		
PO <sub>7</sub> Vehicle access arrangements and queuing areas are appropriate for: <ul style="list-style-type: none"> <li>(a) the capacity of the parking area;</li> <li>(b) the volume, frequency and type of vehicle usage; and</li> <li>(c) the function and characteristics of the access road and adjoining road network.</li> </ul>	AO <sub>7.1</sub> Access driveways and queuing areas are located and designed in accordance with the provisions of <i>Australian Standard AS 2890.1 Part 1: Off Street Carparking</i> .	<b>Not Applicable.</b> The proposed development will utilise existing vehicle access arrangements.
PO <sub>8</sub> Access arrangements minimise any adverse impact on: <ul style="list-style-type: none"> <li>(a) the integrity of any infrastructure within the road reserve;</li> <li>(b) the safety and convenience of pedestrians and cyclists;</li> <li>(c) the safety and convenience of access to adjacent properties;</li> <li>(d) the amenity of premises in the vicinity; and</li> </ul>	No acceptable outcome is nominated.	<b>Complies.</b> The proposed development involves the reuse of existing access arrangements and accordingly, will not result in impacts to infrastructure or the safety of people moving within the carriageway or the convenience of access for adjoining premises. Furthermore, the reuse of existing arrangements ensures that there will be no impacts to amenity in the locality.

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
(e) street trees in the road reserve.		
PO <sub>9</sub> Where the nature of the proposed development creates a demand due to the frequency and volume of vehicle movements for the set-down and pick-up of passengers, provision is made for set-down and pick-up facilities by bus, taxis or private vehicle.	No acceptable outcome is nominated.	<b>Not Applicable.</b> The nature of the development is such that it will not generate the need for dedicated set-down and pick-up facilities.
PO <sub>10</sub> Where set-down and pick-up facilities for bus, taxis or private vehicles are provided as part of development they are: (a) safe for pedestrians, cyclists and vehicles; (b) conveniently connected to the main component of the development by pedestrian pathway; and (c) designed to provide for pedestrian priority and clear sightlines.	AO <sub>10.1</sub> Bus pick-up/set-down areas: (a) allow a bus, based on the Long Rigid Bus (12m) in Austroads/Standards Australia HB72 – Design Vehicles and Turning Path Templates, to turn and manoeuvre in and out of the area in an easy and safe manner; (b) afford maximum safety for passengers boarding or alighting buses; (c) avoid standing or queuing buses from obstructing access to car parking spaces or circulation within the Site; and (d) avoid on-street queuing or boarding/alighting of buses that would reduce traffic flow or safety on the road network. One clear traffic lane in each direction should be maintained.  AO <sub>10.2</sub> Car and taxi pick-up/set-down areas: (a) allow a car to manoeuvre in and out of the area in an easy and safe manner; (b) afford maximum safety for passengers boarding or alighting cars;	<b>Not Applicable.</b> The nature of the development is such that it will not generate the need for dedicated set-down and pick-up facilities.

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Performance outcomes	Acceptable outcomes	Compliance summary
	(c) avoid standing or queuing cars from obstructing access to car parking spaces or circulation within the site; and (d) avoid on-street queuing or boarding/alighting of cars that would reduce traffic flow or safety on the road network. One clear traffic lane in each direction should be maintained.	
<b>Pedestrian and Cycle Facilities</b>		
PO <sub>11</sub> Provision is made for the safe and convenient movement of pedestrians on site and external to the site, having regard to desire lines, legibility, weather protection and the needs of people with disabilities.	AO <sub>11.1</sub> Pedestrian pathways and crossings are provided in accordance with SC6.2 PSP No.2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .  AO <sub>11.2</sub> Access for cyclists and pedestrians is clearly distinguished from vehicle access.  AO <sub>11.3</sub> Pedestrian paths of a minimum width of 1.5m are provided through each car parking row and connect to the main entrance(s) to the building(s).	<b>Not Applicable.</b> The proposed development does not involve pedestrian or cyclist facilities.
PO <sub>12</sub> Provision is made for safe and convenient cycle movement to the site and within the site having regard to desire lines, users' needs and legibility.	AO <sub>12.1</sub> Shared paths and on-road cycle lane facilities are provided in accordance with SC6.2 PSP No.2 – <i>Engineering Standards Roads and Drainage Infrastructure</i> .	<b>Not Applicable.</b> The proposed development does not involve pedestrian or cyclist facilities.
<b>Parking and Circulation</b>		
PO <sub>13</sub> Car parking areas are designed to be: (a) clearly defined, marked and signed; (b) convenient, safe and accessible; and	AO <sub>13.1</sub> The entry to the car park is clearly signposted.	<b>Not Applicable.</b> The proposed development is not of a nature that warrants signposting at the carpark entry

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>(c) safe for vehicles, pedestrians and cyclists and minimise vehicle/pedestrian conflicts by providing clear access lines for pedestrians movement within car park areas.</p>	<p>AO<sub>13.2</sub> Parking spaces are freely available for use by the development's occupants and visitors during the business hours of the use.</p> <p>AO<sub>13.3</sub> Visitor or customer parking spaces are located in the most accessible position to the main entrance of the building and signed as such.</p> <p>AO<sub>13.4</sub> Unless otherwise specified in another code relevant to the development, 60% of the parking spaces for non-residential development are clearly visible from the street.</p> <p>AO<sub>13.5</sub> Public Safety:</p> <p style="margin-left: 20px;">(a) The car park is located where it can be monitored by passers-by or the occupiers of the development.</p> <p style="margin-left: 20px;">(b) Where the car park is open to the public at night, lighting is provided throughout the car park and along pedestrian access paths in compliance with Australian Standard AS 1158.3.1 – Road Lighting – Pedestrian Area (Category P) Lighting – Performance and Installation Design Requirements.</p> <p style="margin-left: 20px;">(c) Except in the case of residential development:</p>	<p><b>Complies.</b> Carparking spaces will be available for use by the respective occupants of each dwelling with the existing visitor parking space being available to any visitor.</p> <p><b>Complies.</b> Existing visitor carparking space has been positioned at the entrance to site and can be signed if required.</p> <p><b>Not Applicable.</b> The proposed development does not involve a non-residential use.</p> <p><b>Complies.</b> The proposed carparking area has been designed to ensure that it can be monitored from the street. Carparking will be accessible by occupants at all hours and low-level security lighting can be provide in accordance with the relevant standards.</p>

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(i) where the car park is not required at night, entry to the car park is physically restricted; and</li> <li>(ii) where the car park is enclosed, the walls are finished in a light coloured material that reflects light.</li> <li>(d) Landscaping throughout the car park is provided in a manner, as indicated in the Landscaping Code that allows surveillance and minimises the risk of crime.</li> </ul>	
<p>PO<sub>14</sub> Car parking areas are designed to provide spaces which meet the needs of people with disabilities.</p>	<p>AO<sub>14.1</sub> Parking spaces for people with disabilities are provided at the rates specified in Appendix C of Australian Standard <i>AS2890.1 Part 1: Off Street Carparking</i>.</p> <p>AO<sub>14.2</sub> Car parking spaces for people with disabilities are located as near as possible to the entrance or entrances of the facility or use they serve.</p> <p>AO<sub>14.3</sub> Parking spaces for people with disabilities are designed in accordance with the provisions of Australian Standard <i>AS2890.1 Part 1: Off Street Carparking</i>.</p> <p>AO<sub>14.4</sub> Pathways and ramps between parking areas and the entrances to buildings are designed in accordance with the provisions of Australian Standard <i>AS1428.1: Design for Access and Mobility</i>.</p> <p>AO<sub>14.5</sub> Parking spaces for people with disabilities are identified by a sign incorporating the International Symbol specified in Australian</p>	<p><b>Not Applicable.</b></p> <p>The nature of the development is such that parking for separate PWD is not required. The proposed and existing garages are designed such that the dwellings are provided with suitable PWD access.</p>

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
	<p>Standard <i>AS1428.1: Design for Access and Mobility</i>.</p> <p>AO<sub>14.6</sub> The sign is readily visible from a vehicle at the entrance to the carpark, or guide signs are provided to indicate the direction of the disabled parking spaces.</p>	
<p>PO<sub>15</sub> Car parking areas for non-residential development on a site in, or adjoining, a residential zone, are designed to minimise any adverse impact on the amenity of premises in the vicinity.</p>	<p>For non-residential development on a site in, or adjoining, a residential zone:</p> <p>AO<sub>15.1</sub> Car parking and driveway areas are setback a minimum distance of 3m from a side boundary that is common with a residential use in a residential zone.</p> <p>AO<sub>15.2</sub> Landscape planting is used between the car park and driveway areas and the side boundary to soften the visual impacts of car park areas and to provide shade.</p> <p>AO<sub>15.3</sub> An acoustic fence of 1.8m height is provided along the property boundary that is common with a residential use in a residential zone.</p>	<p><b>Not Applicable.</b></p> <p>The proposed development involves a residential use.</p>
<p>PO<sub>16</sub> Car parking and associated access arrangements are located and designed to avoid dominating the road frontage of the site or otherwise detracting from streetscape character.</p>	<p>AO<sub>16.1</sub> Car parking is provided either at the rear of the development or beneath buildings.</p>	<p><b>Performance Solution.</b></p> <p>The carparking for each dwelling unit is within a garage and is not visible from the street. Accordingly, the overall impact of visible carparking spaces is reduced.</p>
<p>PO<sub>17</sub> Above ground or multi-level car parking areas are designed, articulated and finished to make a positive contribution to the local streetscape character.</p>	<p>AO<sub>17.1</sub> Above ground or multi-level parking areas are designed, articulated and finished to a quality equal to or better than adjoining buildings.</p>	<p><b>Not Applicable.</b></p> <p>The proposed development does not involve above ground or multi-level carparking.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>18</sub> Landscaping is provided to soften the visual impact of car parking areas and to provide shading and protection from glare.	AO <sub>18.1</sub> Aesthetics, glare, heat absorption and re-radiation. (a) Landscaping is provided throughout the car park in the manner and at the rate indicated in the Landscaping Code; and (b) Unless otherwise specified in a zone, precinct or use code, where the car park adjoins a street frontage, or a boundary with a Residential or other sensitive land use, a landscaped strip of minimum 3 m width is provided along the frontage/boundary.	<b>Complies.</b> Landscaping will be provided throughout the carparking/access area and generally in accordance with the Landscaping Code.
PO <sub>19</sub> Any parking, access and any other vehicle access/manoeuvring areas incorporate design measures to avoid dust nuisance to surrounding properties.	AO <sub>19.1</sub> Car parking, access and any other vehicle access/manoeuvring areas vehicle manoeuvring areas are imperviously sealed.	<b>Complies.</b> Carparking and access areas will be imperviously sealed.
PO <sub>20</sub> Noise impacts from vehicle movement areas on any adjoining residential or other sensitive land use are mitigated.	AO <sub>20.1</sub> A solid, good quality brick, timber or masonry fence of a minimum 1.8m height is constructed between any vehicle movement areas and a boundary to an adjoining residential or other sensitive land use.	<b>Complies.</b> A 1.8m high fence is already provided adjacent to the access driveway for the extent of the movement area.
PO <sub>21</sub> Any part of the parking area designated as a vehicle cleaning or repair area is designed and constructed to avoid adverse impact on water quality or Council's wastewater or stormwater infrastructure.	AO <sub>21.1</sub> The development is capable of meeting the requirements of Council's <i>Trade Waste Policy</i> and the Trade Waste Environmental Management Plan.	<b>Not Applicable.</b> The proposed development does not involve a dedicated vehicle cleaning or repair area.

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**Transport, Access and Parking Code – assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Servicing</b>		
<p>PO<sub>22</sub> Provision is made for the on-site loading, unloading, manoeuvring and access by service vehicles that:</p> <p>(a) is adequate to meet the demands generated by the development;</p> <p>(b) is able to accommodate the design service vehicle requirements; and</p> <p>(c) does not unduly impede vehicular, cyclist and pedestrian safety and convenience within the site.</p>	<p>AO<sub>22.1</sub> The service bays provided and access to them, can accommodate, at any one time, the types and numbers of service vehicles detailed in Table 9.4.6:3.</p> <p>AO<sub>22.2</sub> Service bays provided wholly or partly within a building are physically separated from the rest of the buildings floor space in manner that makes it impractical to use them as storage or work areas.</p> <p>AO<sub>22.3</sub> The design and provision of access driveways, manoeuvring areas and loading and unloading facilities for service vehicles complies with Australian Standard AS 2890.2 – 1989 – <i>Off Street Parking – Commercial Vehicle Facilities</i>.</p> <p>AO<sub>22.4</sub> Vehicles being loaded or unloaded with goods stand completely on-site and do not impede access to more than 6 parking spaces or 50% of the on-site parking spaces (whichever is the lesser) while doing so.</p> <p>AO<sub>22.5</sub> Service vehicles can enter and leave the site in a forward gear.</p>	<p><b>Not Applicable.</b></p> <p>The proposed development is such that service bays will not be provided.</p>
<p>PO<sub>23</sub> Refuse collection vehicles are able to access on-site refuse collection facilities.</p>	<p>AO<sub>23.1</sub> Where an on-site refuse area is provided, access and manoeuvring areas are designed and provided to enable access by refuse collection vehicle based on the Design Service Vehicle in Austroads/Standards Australia HB72 – Design Vehicles and Turning Path Templates.</p>	<p><b>Not Applicable.</b></p> <p>The proposed development does not involve the on-site refuse collection.</p>

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Transport, Access and Parking Code – assessment benchmarks for assessable development

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>24</sub> Servicing arrangements minimise any adverse impact the amenity of premises in the vicinity.	No acceptable outcome is nominated.	<b>Not Applicable.</b> The proposed development does not involve the on-site refuse collection.
PO <sub>25</sub> Servicing arrangements are located and designed to avoid dominating the road frontage of the site or otherwise detracting from streetscape character.	AO <sub>25.1</sub> Areas used for servicing are not located at the front of developments, or are otherwise screened to minimise visual intrusion in the streetscape.	<b>Not Applicable.</b> The proposed development does not involve the on-site refuse collection.

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**Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Utilities</b>		
<p>PO<sub>1</sub> A water supply is provided that is adequate for the current and future needs of the intended use.</p>	<p>AO<sub>1.1</sub> Where within a water supply area, the development is connected to Council's reticulated water supply system in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>OR</p> <p>AO<sub>1.2</sub> Where not in a water supply area, the development is provided with an on site water supply in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>AO<sub>1.3</sub> Water supply systems and connections are designed and constructed in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p>	<p><b>Complies.</b> The proposed development will be connected to Council's reticulated water supply in accordance with the relevant standards.</p>
<p>PO<sub>2</sub> Wastewater treatment and disposal is provided that is appropriate for the level of demand generated, protects public health and avoids environmental harm.</p>	<p>AO<sub>2.1</sub> Where within a wastewater area, the development is connected to the Council's reticulated wastewater system in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>OR</p> <p>AO<sub>2.2</sub> Where not within a wastewater area, on-site wastewater treatment and disposal is provided which complies with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p> <p>AO<sub>2.3</sub> Waste water systems and connections are designed and constructed in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i>.</p>	<p><b>Complies.</b> The proposed development will be connected to Council's sewerage system in accordance with the relevant standards.</p>

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**Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>3</sub> The development is equipped with an adequate energy supply approved by and installed in accordance with the standards of the relevant energy regulatory authority.	AO <sub>3.1</sub> Premises are connected to an electricity supply approved by the relevant energy regulatory authority.	<b>Complies.</b> The proposed development will be connected to an electricity supply.
PO <sub>4</sub> Premises are connected to a telecommunications service approved by the relevant telecommunication regulatory authority.	AO <sub>4.1</sub> The development is connected to telecommunications infrastructure in accordance with the standards of the relevant regulatory authority.	<b>Complies.</b> The proposed development will be connected to telecommunications infrastructure in accordance with the relevant standards.
PO <sub>5</sub> Provision is made for future telecommunications services (e.g. fibre optic cable).	AO <sub>5.1</sub> Conduits are provided in accordance with <i>SC6.2 PSP No.2 Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Where required, conduits can be provided in accordance with the relevant standards.
PO <sub>6</sub> Development near utility services does not: (a) adversely affect the function of the service; or (b) place an additional load on the service; and (c) protects the infrastructure form physical damage; and (d) allows ongoing necessary access for maintenance purposes.	AO <sub>6.1</sub> Setbacks and loadings comply with <i>the Queensland Development Code QDC MP1.4</i> .	<b>Complies.</b> Setbacks have been provided in accordance with <i>QDC MP1.4</i> .
PO <sub>7</sub> Infrastructure is integrated with and efficiently extends existing networks.	No acceptable outcome is nominated.	<b>Complies.</b> The proposed development ensures that any additional infrastructure required to service the development will be integrated with and efficiently extend existing networks.
PO <sub>8</sub> Water meter/s are installed and located for easy access by the relevant authority.	AO <sub>8.1</sub> Water meter/s are installed in accordance with <i>SC6.3 PSP No. 3 Engineering Standards – Water and Wastewater Infrastructure</i> .	<b>Complies.</b> Water meters will be installed in accordance with Council's requirements.

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**Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Movement Networks</b>		
PO <sub>9</sub> Premises are provided with the following works along the full extent of the road frontage and to a standard that is appropriate to the function of the road and the character of the locality: (a) appropriate roadway treatment; (b) appropriate pavement edging (including kerb and channel); (c) safe vehicular access; (d) safe footpaths and bikeways; (e) street scaping or street tree planting; (f) stormwater drainage; and (g) street lighting systems.	AO <sub>9.1</sub> Design and construction of external road works are undertaken in accordance with <i>SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure</i> .  AO <sub>9.2</sub> Footpaths and bikeways are provided in accordance with the <i>Austrroads Guide to Road Design – Part 6A: Pedestrian and Cyclist Paths (Austrroads 2009m)</i> .	<b>Not Applicable.</b> The proposed development does not involve external roadworks.
PO <sub>10</sub> Provision is made in the road reserve for street scaping, pedestrians and cyclists in a manner consistent with: (a) the current and projected level of usage; (b) the desired streetscape character; and (c) activities which are anticipated to occur within the verge.	AO <sub>10.1</sub> Street scaping works, footpaths and cycle paths are provided in accordance with <i>PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure</i> .  AO <sub>10.2</sub> Footpaths and bikeways are provided in accordance with the <i>Austrroads Guide to Road Design – Part 6A: Pedestrian and Cyclist Paths (Austrroads 2009m)</i> .	<b>Not Applicable.</b> The proposed development does not involve street scaping works, footpaths or cycle paths.  <b>Not Applicable.</b> The proposed development does not propose new footpaths or bikeways.
PO <sub>11</sub> Parking areas are constructed in a manner that is sufficiently durable for the intended function, maintains all-weather access and ensures the safe passage of vehicles, pedestrians and cyclists.	AO <sub>11.1</sub> Parking area design and construction is undertaken in accordance with the Transport, Access and Parking Code.	<b>Complies.</b> Car parking associated with the proposed development will be designed and constructed in accordance with the Transport, Access and Parking Code.
PO <sub>12</sub> Movement networks can be easily and efficiently maintained.	AO <sub>12.1</sub> Infrastructure is provided in accordance with <i>SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Infrastructure will be provided in accordance with the relevant requirements.

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**Works and Services Code – Requirements for accepted development and assessment benchmarks for assessable development**

Performance outcomes	Acceptable outcomes	Compliance summary
<b>Vehicular Access – Non-residential</b>		
<p>PO<sub>13</sub> Non-residential vehicular access arrangements to a public roadway, including driveway crossovers:</p> <ul style="list-style-type: none"> <li>(a) are safe and do not compromise the efficiency, function, convenience of use or capacity of the road network;</li> <li>(b) are located and designed to:               <ul style="list-style-type: none"> <li>(i) avoid damage to utility services, pathways, krebs, road pavement and seal and other council/public infrastructure;</li> <li>(ii) prevent conflict between vehicles and cyclists and pedestrians; and</li> <li>(iii) avoid damage to existing street trees and retain space for the future planting of street trees within the verge.</li> </ul> </li> <li>(c) minimise the number of vehicular access per site to avoid the loss of on-street carparking spaces, and are appropriately separated from other vehicular accesses and side property boundaries to prevent interference with:               <ul style="list-style-type: none"> <li>(i) the safety, capacity and operations of the existing or planned road network;</li> <li>(ii) any existing vehicular accesses;</li> <li>(iii) adjoining properties; and</li> <li>(iv) cycleways or pedestrian footpaths and their users.</li> </ul> </li> <li>(d) protect the amenity of premises in the vicinity and surrounding streetscape by:               <ul style="list-style-type: none"> <li>(i) use of materials which integrate with the streetscape (e.g. existing crossovers and driveways, etc);</li> </ul> </li> </ul>	<p>AO<sub>13.1</sub> Non-residential vehicular access and driveway crossovers to a public road are not:</p> <ul style="list-style-type: none"> <li>(a) an additional site/property access;</li> <li>(b) to a State-controlled Road or a road with bluestone kerbing;</li> <li>(c) within 25m of a signalised road intersection;</li> <li>(d) within 20m of an unsignalised road intersection in a Commercial or Industrial Area;</li> <li>(e) within 10m of an unsignalised road intersection in a Community, Residential, Rural or Other Area;</li> <li>(f) for a lot with a frontage of 10m or less;</li> <li>(g) within 1m of any infrastructure, including street signage, power poles, street lights, manholes, stormwater gully pits, or other Council/public utility asset; and</li> <li>(h) within the Tree Protection Zone, as defined by <i>Australian Standard 4970-2009</i>.</li> </ul> <p><i>Note: An additional site access is considered to be more than one site access.</i></p> <p>AO<sub>13.2</sub> Non-residential vehicular access and driveway crossovers:</p> <ul style="list-style-type: none"> <li>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</li> </ul>	<p><b>Not Applicable.</b> The proposed development is for a residential use.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
<ul style="list-style-type: none"> <li>(ii) minimising the width and grade of the access; and</li> <li>(iii) minimising impacts on the appearance of the streetscape by retaining existing vegetation, including approved landscaping.</li> </ul>	<ul style="list-style-type: none"> <li>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the road reserve;</li> <li>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, loading bay, bus stop, or other structure within the road reserve;</li> <li>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</li> <li>(e) do not have access restricted by an access restriction strip or link reserve;</li> <li>(f) do not access an unformed or unkerbed road;</li> <li>(g) are constructed from reinforced concrete;</li> <li>(h) are perpendicular to the road edge;</li> <li>(i) retain space for the planting of street trees at a minimum rate of one (1) per lot frontage; and</li> <li>(j) are provided in accordance with the Australian Standard AS 2890.1 – Off Street Car Parking and Australian Standard AS 2890.2 (where relevant), the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure and Australian Standard AS 4970-2009 – Protection of Trees on Development Sites.</li> </ul>	

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Performance outcomes	Acceptable outcomes	Compliance summary
<b>Vehicular Access – Residential</b>		
<p>PO<sub>14</sub> Residential vehicular access arrangements, including driveway crossovers:</p> <ul style="list-style-type: none"> <li>(a) are appropriate for:               <ul style="list-style-type: none"> <li>(i) the capacity of the parking area;</li> <li>(ii) the volume, frequency and type of vehicle useage; and</li> <li>(iii) the function and configuration of the access road.</li> </ul> </li> <li>(b) minimise any potentially adverse impact on:               <ul style="list-style-type: none"> <li>(i) the safety and efficiency of the road and pedestrian/cycle paths;</li> <li>(ii) the safety and efficiency of the road and footpath users;</li> <li>(iii) the integrity of any infrastructure within the road reserve; and</li> <li>(iv) the safety of access to adjacent properties.</li> </ul> </li> <li>(c) protect the amenity of premises in the vicinity by:               <ul style="list-style-type: none"> <li>(i) maintaining the predominant vehicular access pattern in the street, including consistent width, grade and location;</li> <li>(ii) preserving the residential amenity of the streetscape, including noise and visual impact, and consideration of existing landscaping by considering:                   <ul style="list-style-type: none"> <li>(A) use of materials which integrate with the streetscape (e.g. existing crossovers and driveways, etc);</li> <li>(B) minimising the width and grade of the access;</li> </ul> </li> </ul> </li> </ul>	<p>AO<sub>14.1</sub> Residential vehicular access and driveway crossovers are not:</p> <ul style="list-style-type: none"> <li>(a) an additional site/property access;</li> <li>(b) to a State-controlled Road or a road with bluestone kerbing;</li> <li>(c) within 25 m of a signalised road intersection;</li> <li>(d) within 20m of an unsignalised road intersection in a Commercial or Industrial Area;</li> <li>(e) within 10m of an unsignalised road intersection in a Community, Residential, Rural or Other Area;</li> <li>(f) within 1m of any infrastructure, including street signage, power poles, street lights, manholes, stormwater gully pits, or other Council/public utility asset;</li> <li>(g) within the Tree Protection Zone, as defined by Australian Standard 4970-2009;</li> <li>(h) for a lot with a frontage of 10m or less;</li> <li>(i) greater than 4m in width when for a lot with a frontage/width of more than 10m but less than 20m; and</li> <li>(j) greater than 6m in width when for a lot with a frontage/width of greater than 20m.</li> </ul> <p><i>Note: An additional site access is considered to be more than one site access.</i></p> <p>AO<sub>14.2</sub> Except where in a Rural Zone, residential vehicular access and driveway crossovers:</p>	<p><b>Complies.</b></p> <p>The proposed development involves the reuse of existing access arrangements which has sufficient capacity to cater for the expected vehicle volume, frequency and type whilst remaining appropriate for the function and configuration of the abutting road. Changes also ensure that the safety of vehicles and pedestrians in the carriageway is not compromised. The amenity of the surrounding locality is protected by ensuring the design and scale of the access is not uncharacteristic of the surrounding area. Visual amenity will be further protected by utilising landscaping within the front setback.</p> <p><b>Complies.</b></p> <p>Refer to response to AO14.1.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
<p>(C) minimising impacts on the appearance of the streetscape by retaining existing vegetation, including approved landscaping; and</p> <p>(D) locating the access to minimise the impact of vehicle noise on neighbouring/adjoining properties.</p>	<p>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</p> <p>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the roadway;</p> <p>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, bus stop, or other structure within the road reserve;</p> <p>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</p> <p>(e) do not have access restricted by an access restriction strip or link reserve; or</p> <p>(f) do not access an unformed or unkerbed road;</p> <p>(g) are constructed from reinforced concrete;</p> <p>(h) are perpendicular to the road edge; and</p> <p>(i) are provided in accordance with the <i>Australian Standard AS 2890.1 – Off Street Car Parking and Australian Standard AS 2890.2 (where relevant), the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure and Australian Standard AS 4970-2009 –Protection of Trees on Development Sites.</i></p> <p>AO<sub>14.3</sub> Where in a Rural Zone, residential vehicular access and driveway crossovers:</p>	<p><b>Not Applicable.</b> The site is not located in the Rural Zone.</p>

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Performance outcomes	Acceptable outcomes	Compliance summary
	<ul style="list-style-type: none"> <li>(a) do not require the modification, relocation, or removal of any existing infrastructure (e.g. street trees, fire hydrants, water meters, manholes or stormwater gully pits);</li> <li>(b) do not affect or are not adjacent to a traffic island, speed control device, car parking bay, bus stop, or other structure within the roadway;</li> <li>(c) do not require removal or modification of any existing kerbing, traffic island, speed control device, car parking bay, loading bay, bus stop, or other structure within the road reserve;</li> <li>(d) do not require any change to existing footpath/verge profiles, including table drains (where relevant);</li> <li>(e) do not have access restricted by an access restriction strip or link reserve;</li> <li>(f) are sealed where accessing a sealed formed road;</li> <li>(g) are perpendicular to the road edge; and</li> <li>(h) are provided in accordance with the relevant standard drawing in SC6.2 PSP No. 2 Engineering Standards – Roads and Drainage Infrastructure.</li> </ul>	
<b>Earthworks and Retaining Walls</b>		
PO <sub>15</sub> Earthworks result in stable landforms and structures.	AO <sub>15.1</sub> Earthworks and the construction of retaining walls and batters are undertaken in accordance with SC6.2 PSP No. 2 – Engineering Standards – Roads and Drainage Infrastructure.	<b>Complies.</b> Any earthworks or retaining walls associated with the development will be undertaken in accordance with Council's standards.

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Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>16</sub> Earthworks do not result in the contamination of land or water and avoid risk to people and property.	AO <sub>16.1</sub> Earthworks are undertaken in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Any earthworks associated with the development will be undertaken in accordance with Council's standards.
PO <sub>17</sub> Earthworks are undertaken in a manner that: (a) maintains natural landforms; (b) minimises height of retaining walls and batter faces; (c) does not unduly impact on the amenity or privacy for occupants of the site or on adjoining land; and (d) does not unduly impact on the amenity of the streetscape.	AO <sub>17.1</sub> Earthworks and the construction of retaining walls and batters are undertaken in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Any earthworks or retaining walls associated with the development will be undertaken in accordance with Council's standards.
PO <sub>18</sub> Earthworks do not create or worsen any flooding or drainage problems on the site or on neighbouring properties.	AO <sub>18.1</sub> Earthworks and the construction of retaining walls and batters are undertaken in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Any earthworks or retaining walls associated with the development will be undertaken in accordance with Council's standards.
PO <sub>19</sub> Earthworks do not prevent access or create difficult access to the property.	AO <sub>19.1</sub> Earthworks and the construction of retaining walls and batters ensure driveways can be provided in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Any earthworks or retaining walls associated with the development will be undertaken in accordance with Council's standards.
PO <sub>20</sub> Earthworks do not cause a significant impact on the amenity of the locality or along routes taken to transport material as a result of truck volumes, dust or noise.	AO <sub>20.1</sub> Earthworks are undertaken in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Any earthworks associated with the development will be undertaken in accordance with Council's standards.
PO <sub>21</sub> The transportation of material minimises adverse impact on the road system.	AO <sub>21.1</sub> Material is transported in accordance with SC6.2 PSP No. 2 – <i>Engineering Standards – Roads and Drainage Infrastructure</i> .	<b>Complies.</b> Any material transport to the site during construction will be undertaken in accordance with Council's requirements.

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Performance outcomes	Acceptable outcomes	Compliance summary
<b>Waste Management</b>		
PO <sub>22</sub> Where relevant, the development is capable of providing for the storage, collection, treatment and disposal of trade waste such that: <ul style="list-style-type: none"> <li>(a) off-site releases of contaminants do not occur;</li> <li>(b) the health and safety of people and the environment are protected; and</li> <li>(c) the performance of the wastewater system is not put at risk.</li> </ul>	No acceptable outcome is nominated.	<b>Not Applicable.</b> The proposed development does not involve the production of trade waste.
PO <sub>23</sub> Appropriate refuse container storage areas are provided which are: <ul style="list-style-type: none"> <li>(a) in a building or enclosing structure or screened from public view;</li> <li>(b) of adequate size to accommodate the expected amount of refuse to be generated by the use;</li> <li>(c) in a position that is conveniently accessible for collection; and</li> <li>(d) able to be kept in a clean state at all times.</li> </ul>	AO <sub>23.1</sub> Refuse container storage areas are provided which: <ul style="list-style-type: none"> <li>(a) are in a building, outbuilding or other enclosed structure, or otherwise screened from public view, by a minimum 1.5 m high solid fence or wall or dense vegetation;</li> <li>(b) are provided with an imperviously sealed pad, on which to stand the bin(s), that is drained to an approved waste disposal system;</li> <li>(c) are within normal hose length of a hose cock;</li> <li>(d) are large enough to accommodate at least one (1) standard sized container per dwelling and, in commercial and industrial premises, one (1) or more industrial bins of a</li> </ul>	<b>Complies.</b> Refuse bins will be appropriately stored in accordance with the relevant requirements. Reference is made to the Environmental Standards Code.

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Performance outcomes	Acceptable outcomes	Compliance summary
	<p>size appropriate to the nature and scale of use; and</p> <p>(e) are situated not closer than 6m to a road or 2m to any site boundary.</p> <p>AO<sub>23.2</sub> On sites greater than 2,000m<sup>2</sup> in area, provision is made for refuse collection vehicles to access the collection area and to enter and leave the site in a forward direction without having to make more than a 3-point turn.</p> <p>AO<sub>23.3</sub> For multiple dwelling and retirement facility, container storage areas are located not more than 50m from any dwelling.</p>	<p><b>Not Applicable.</b> The site has an area less than 2,000m<sup>2</sup>.</p> <p><b>Complies.</b> A container storage area will be provided adjacent to each dwelling.</p>
PO <sub>24</sub> Where the use is non-residential and generates recyclable waste, provision is made for conveniently located recycling bins on the premises, including in the refuse container storage area.	No acceptable outcome is nominated.	<b>Not Applicable.</b> The proposed development involves a residential use.
<b>Construction Management</b>		
PO <sub>25</sub> Work is undertaken in a manner which minimises adverse impacts on vegetation that is to be retained.	AO <sub>25.1</sub> Works include, at a minimum: <ul style="list-style-type: none"> <li>(a) installation of protective fencing around retained vegetation during construction;</li> <li>(b) erection of advisory signage;</li> <li>(c) no disturbance, due to earthworks or storage of plant, materials and equipment, of ground level and soils below the canopy of any retained vegetation; and</li> <li>(d) removal from the site of all declared noxious weeds and environmental weeds.</li> </ul>	<b>Complies.</b> Construction works for the proposed development will be undertaken in accordance with Council's requirements.

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Performance outcomes	Acceptable outcomes	Compliance summary
PO <sub>26</sub> Work is undertaken in a manner which does not cause unacceptable impacts on surrounding areas as a result of dust, odour, noise or lighting.	AO <sub>26.1</sub> Construction is undertaken in accordance with the Environmental Standards Code.	<b>Complies.</b> Construction works for the proposed development will be undertaken in accordance with Council's requirements.
PO <sub>27</sub> While undertaking development works, the site and adjoining road are maintained in a tidy, safe and hygienic manner.	AO <sub>27.1</sub> Construction is undertaken in accordance with the Environmental Standards Code.	<b>Complies.</b> Construction works for the proposed development will be undertaken in accordance with Council's requirements.
PO <sub>28</sub> Traffic, parking and delivery of construction materials generated during construction are managed to minimise impact on the amenity of the surrounding area and to manage the safety of pedestrians, cyclists and motorists.	AO <sub>28.1</sub> Construction is undertaken in accordance with the Environmental Standards Code.	<b>Complies.</b> Construction works for the proposed development will be undertaken in accordance with Council's requirements.
PO <sub>29</sub> Council and state infrastructure is not damaged by construction activities.	AO <sub>29.1</sub> Construction, alterations and any repairs to infrastructure is undertaken in accordance with the <i>SC6.2 PSP No.2 Engineering Standards – Roads and Drainage Infrastructure, Queensland Development Code QDC MP1.4, and, where applicable, in consultation with the relevant service authority.</i>  AO <sub>29.2</sub> Construction, alterations and any repairs to State-controlled roads and rail corridors are undertaken in accordance with the <i>Transport Infrastructure Act 1994.</i>	<b>Complies.</b> Construction works for the proposed development will be undertaken in accordance with Council's requirements.